

MOVING MOLINE FORWARD

URBAN DESIGN & TRANSPORTATION
UPDATE TO THE 2014 COMPREHENSIVE RIVERFRONT PLAN

JUNE 20, 2016



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ACKNOWLEDGEMENTS

PREPARED FOR:



CONSULTANT TEAM:



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1.0 INTRODUCTION

MOVING MOLINE FORWARD: URBAN DESIGN & TRANSPORTATION UPDATE TO
THE 2014 COMPREHENSIVE RIVERFRONT PLAN

JUNE 20, 2016

1.0 PLAN OVERVIEW



Plan Intent

This Plan is the continuum of many ambitious efforts by the City of Moline and local stakeholders to transform the Florencia, Moline Centre and Edgewater neighborhoods into vibrant places for working, living, learning and recreating. This Plan builds on the recommendations of previous studies and plans that were the foundation of public and private sector decision-making and strategic capital investments. Therefore, the objective of this Plan is to update prior plans and studies by providing compelling and realistic visions for the continued revitalization and transformation of the Riverfront neighborhoods.

The City engaged a multidisciplinary team led by Solomon Cordwell Buenz (SCB) to provide transportation focused updates to the 2014 Comprehensive Riverfront Plan by reviewing and addressing critical transportation, land use and urban design issues associated with the new Interstate 74 bridge, the growing Western Illinois University Quad Cities campus, The Q Multi-Modal Station, and the ongoing downtown residential and retail development. This Illinois-based consultant team included SCB for community planning and urban design, Sam Schwartz Transportation Consultants for transportation planning, Valerie S. Kretchmer Associates (VSKA) for market analysis, and Upland Design Ltd. for landscape design. This update was completed in coordination with on-going neighborhood revitalization efforts within Florencia

and Edgewater. The community planning process and preparation of the Comprehensive Plan Update involved the following components and tasks:

- Review of existing conditions including: residential, historic and commercial building stock; vacant and under-utilized properties; roadway right of ways, urban design and streetscape; retail, office and industrial activity; riverfront, flood plain and landscape issues; and transportation infrastructure systems.
- Prepare a market study to identify economic opportunities, development trends, and strategies for business attraction to generate economic development for the City of Moline.
- Conduct discussions with key stakeholders including neighborhood residents, property and business owners, local industries, civic and business organizations, brokers and developers, Western Illinois University-Quad Cities Campus, City of Moline staff and commission members, and representatives from the Bi-State Regional Planning Commission, MetroLINK, the Illinois Department of Transportation (IDOT) and Renew Moline.
- Develop preliminary transportation planning strategies and revitalized vision for Florencia, Moline Centre and Edgewater neighborhoods that include collaboration with stakeholders, analysis of existing conditions and understanding the local market demand.



5th Avenue & 13th Street in Moline Centre Neighborhood



4th Avenue in Florencia Neighborhood

1.0 PLAN OVERVIEW



Moline Riverfront Neighborhoods: Florenciente, Moline Centre, Edgewater

- Identify current and future redevelopment sites and areas poised for significant change, with a focus on the land within and around key catalyst sites including the Interstate 74 Bridge Realignment Zone, WIU-QC campus and The Q Multi-Modal Station.
- Identify implementation strategies and action steps towards achieving the desired multi-modal transportation results, in coordination with the revitalization of the riverfront neighborhoods into vibrant destinations.

Moline Centre, Florenciente and Edgewater neighborhoods encompass approximately 700 acres of land located along four miles of the Mississippi River. As described in detail in the Urban Design and Transportation chapters that follow, these three distinct neighborhoods have a wide range of land uses and physical conditions that create opportunities for shared amenities and resources, as well as inherent contradictions in land use and transportation policy. As a result, this Plan Update takes a balanced and focused approach to providing strategies and initiatives that will support the City of Moline’s priorities, catalyze key investments, and seed revitalization efforts in both the short and long-term.

The many detailed recommendations and concepts for each neighborhood area were vetted with stakeholders at community meetings and via web based surveys. The recommendations in this Plan received strong local support and are based on policies crafted to preserve existing businesses. The Plan demonstrates how changes in multi-modal transportation design, land use analysis and urban design can support community development goals and long-term vision.



River Drive in Edgewater

1.1 COMPREHENSIVE PLAN UPDATE



The Basis of this Comprehensive Plan Update

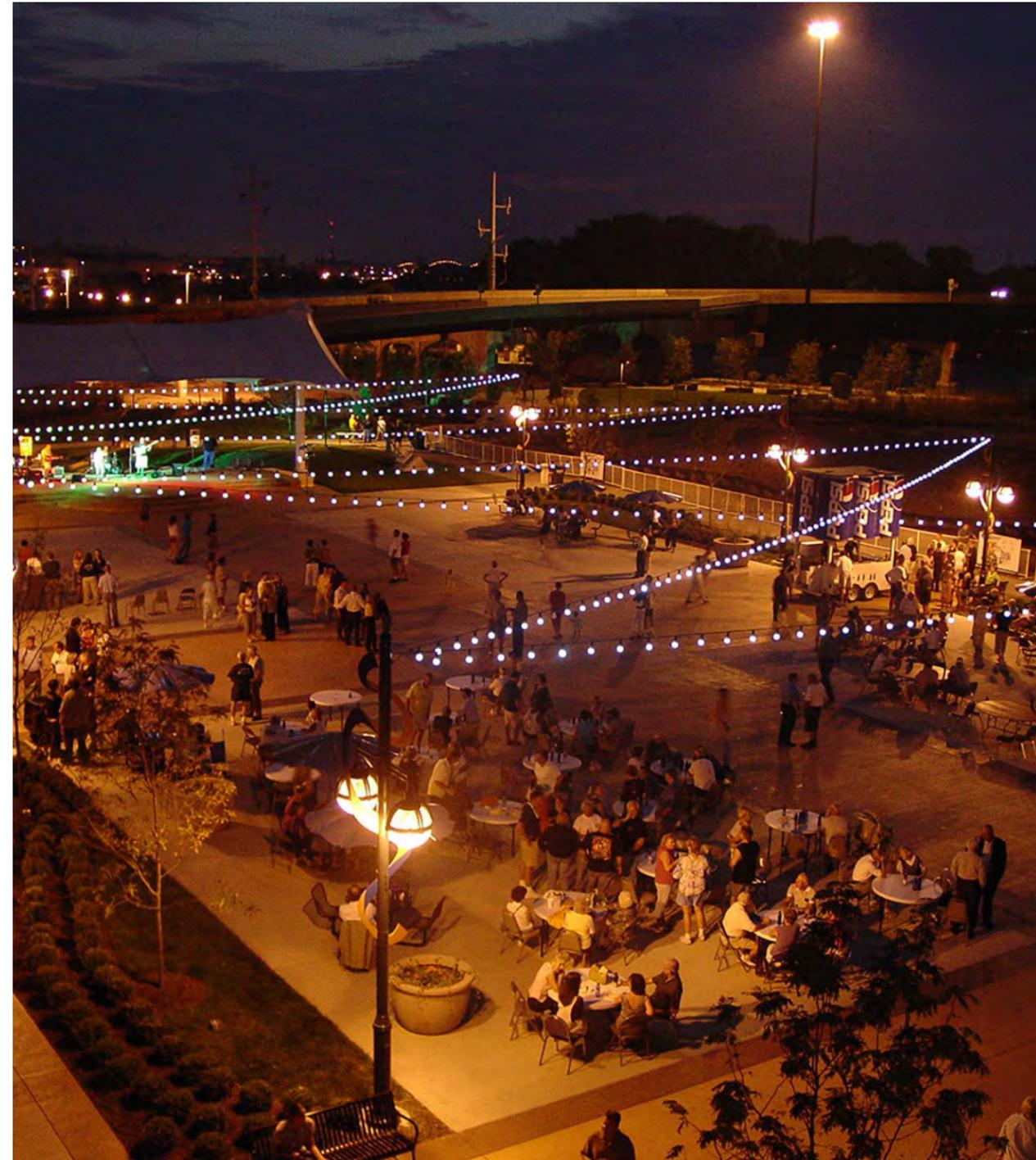
This 2016 Plan is focused on urban design and transportation related issues. It supplements and advances the recommendations from previous plans for the three riverfront communities with a special emphasis on movement, connectivity between community assets, and neighborhood accessibility.

This document is intended to provide a transportation supplement to the 2014 Comprehensive Plan for Floreciente and Edgewater, and update the Moline Centre Master Plan. For the most part, the recommendations for Moline Centre, Floreciente and Edgewater presented in this document build-on and refine recommendations in the 2014 Plan. However, where new transportation analysis and community input led to a change in the recommendations, this 2016 Comprehensive Plan Update will take precedence over the prior comprehensive plan.

Previous Plans

Below are resources and references for previous plans taken into consideration while developing this Plan:

- Moline Comprehension Plan (2001)
- Comprehensive Plan Updates: Moline Centre; Floreciente; Edgewater (2014)
- Q2030: A Regional Action Plan
- 2045 Quad Cities Long Range Transportation Plan Update (2016)
- Bi-State Region Freight Plan -Draft (2015)
- Competitive Assessment Quad Cities Regional Vision Strategy (2015)
- Downtown Streetscape Master Plan (update 2009)
- Edgewater Neighborhood Plan Update (1994)
- Floreciente Neighborhood Plan Update (2001)
- Illinois Highway 92 Corridor Study (2003)
- MetroLINK Quad Cities Balanced Growth Rail Corridor Assessment Report (2001)
- MetroLINK Strategic Plan (2001)
- Mississippi River Urban Technology Corridor Study (2006)
- Moline Bikeways Plan (2011)
- Moline Capital Improvement Program (2013-2017)
- Moline Centre Design Guidelines (2007)
- Moline Centre Master Plan Update (2001)
- Moline Comprehensive Parks and Recreation Master Plan (2004)
- Moline Strategic Plan (2015)
- QC Chamber RiverVision 2020: Community Visioning for the Strategic Plan (2013-2014)
- Quad Cities TOD + Intermodal Plan (2009)
- River Drive Corridor Concept Plan (2009)
- River to River Corridor Transportation and Land Use Study (2007)



Night Event at Bass Street Landing

1.2 PUBLIC ENGAGEMENT



Public Engagement Process

This Plan evolved over a year’s time and was guided by the insights and knowledge of the Advisory Committee comprised of transportation agency representatives, business owners, City leaders and residents. Initially transportation and a market analysis were prepared in combination with site tours and research and data gathering, in order to better understand the key issues impacting the riverfront neighborhoods. A wide variety of community input established the parameters for the planning approach, setting plans goals and future priorities. Through a series of focus group meetings, an on-line survey and public workshop, the Plan recommendations were outlined, debated and agreed upon.

The planning process included opportunities for residents, business owners, City staff and officials, regional and local transportation agencies, and other stakeholders to be involved in decision-making discussions. This Plan is a continuation of the 2014 Plan but with a greater focus on transportation issues and a look at how they will be impacted by new initiatives such as the I-74 Bridge, the new WIU-QC campus, The Q Multi-Modal Station, and other neighborhood projects. Therefore, the community goals and public opinion received during the 2014 planning process were incorporated into, and built on, in this planning effort. The community engagement process to develop the 2014 Plan was extensive, and as a result, the community was engaged to provide supplemental information in this planning process



through focus groups in order to further build upon, not duplicate, the previous community involvement efforts. Representatives from local neighborhoods, businesses, organizations and institutions were invited to discussions early in the process to help the planning team understand the transportation issues critical to the community, as well as learn about development challenges and opportunities. Early in the planning process the City of Moline circulated an electronic survey, as well as a printed version in both English and Spanish, to the community. The survey solicited input on key issues in the three neighborhoods. A community-wide, public open house was held to give the public the opportunity to view Plan components, review preliminary recommendations and provide feedback.



Stakeholder Discussions

Downtown businesses owners, major employers, municipal leadership, and City staff were invited to participate in focus group discussions as part of the initial data gathering exercise of the Plan. Over 60 stakeholders participated in the discussions which were an open dialogue on transportation and development challenges in the three riverfront communities. Many common themes were revealed through the discussions.

Public Open House

A public open house was held November 5th, 2015, at the Stoney Creek Hotel & Conference Center in Moline. The goal of this open house was to share

information, gather feedback and gain insights from the public on key recommendations. The meeting consisted of an overview presentation highlighting goals, key issues, market and transportation analysis, community survey results, findings from stakeholder discussions and preliminary recommendations. The presentation was followed by an interactive discussion forum with activity stations arranged by topic. The focus topics included data, maps, ideas and recommendations for resolving issues associated with transportation, parking, development, retail, landscape and streetscape design, biking, and walking. This workshop was well attended with more than 80 participants.



Community Meeting November 5th, 2015 at Stoney Creek Hotel & Conference Center



Community Meeting November 5th, 2015 at Stoney Creek Hotel & Conference Center

Survey

As one way of gathering insight into the community's perspective on a variety of topics related to transportation and urban planning ideas for the riverfront neighborhoods, a survey was distributed in via electronic and paper distribution. 185 participants responded to the survey and the results were cataloged and analyzed using a digital database. Survey results confirmed a desire for greater variety of shopping and entertainment options downtown; as well as a connected sidewalk network and, increased opportunities for bicyclists throughout.

Respondents were a mix of residents, business owners, employees and shoppers. Of the

respondents, 67% live in Moline; 42% have lived in Moline for over 20 years; and 11% live within the Riverfront Neighborhoods. More than half of the respondents work in Moline; while fewer 15% work in Iowa.

Detailed survey outcomes can be found in the Appendix.

Bilingual Engagement

In recognition of Moline's diverse community, meeting invitations, flyers, surveys, and meeting materials were distributed in Spanish as well as English. In addition, Spanish speaking representatives were available at the public open house.

Survey Outcomes

In response to the question "What is your primary mode of transportation," 91% of respondents use cars while 9% of the respondents rely on alternative modes of transportation including bus, bike, and walking. These percentages highlight the need for a complete multi-modal transportation network. The top three areas needing improvements include the condition of the streets and sidewalks, pedestrian safety at intersections, and accessibility by bicycle. These three needs ranked above parking availability.

Several questions were asked in respect to public open space and parks. Forty percent of respondents use the riverfront trail on a daily or weekly basis. Frequency of trail usage could increase through improved access and more activities along the trail. Parks in Floreciente and Moline Centre were seen as difficult to access by more than 1/3 of the respondents with more park space also seen as needed in these areas. Seventy-nine percent of respondents felt a new park along the River would be an asset.

Moline Centre is a popular destination for dining and attending events at iWireless. Based on survey outcomes, interest in Moline Centre could be improved through increasing the choices in stores (69%), improving parking (52%), improving the attractiveness of shopping areas (43%) and having more special events (40%).

Overall support for pedestrian prioritization was strong, including support to increase the number of protected pedestrian crossings (79%) and increasing the pedestrian nature of River Drive (65%). Eleven percent of respondents felt River Drive should remain a preferred truck route.

Support for conversion of one-way streets to two-way streets continues to be mixed; however, the majority of respondents believe two-way streets would be slower (60%) and better for business (52%). Fewer respondents felt two-way streets were safer for pedestrians (32%) and a fair amount of respondents (47%) felt 4th and 5th Avenues should continue to allow trucks and cars to move quickly through the neighborhoods. These mixed results indicate further study and community outreach is needed to build support for conversion of one-way streets to two-way streets.

1.3 PLANNING FRAMEWORK

The following and common themes emerged through community stakeholder conversations. These themes became the basis of determining how to address key issues within the planning area.

Planning Principles

- Asset Connectivity:** Connect destinations and amenities for residents and visitors through a complete pedestrian and vehicle network. Ensure each neighborhood is walkable and streets are safe for pedestrians and bicyclists.
- Support Existing and Future Land Uses:** Support existing industrial land uses by coordinating truck traffic to address both business needs and community concerns.
- Reinforce a Walkable Urban Fabric:** Assess and improve the sidewalk network and ensure safe pedestrian crossings and intersection designs.
- Emphasize Proximity to River:** Although immediately adjacent to the Mississippi River, access to the River is not always visible nor obvious. Strengthen walkable, bikeable and visible connections to the River and The Great River Trail.
- Establish Priority Development Areas:** Identify key sites that are well poised for development and will have a catalytic effect to attract new development.
- Respond to Current Market Dynamics:** Propose new development that is financially feasible and in tune with market trends.
- Preserve Existing Building Stock:** Promote adaptive reuse and renovation of high quality and historic building resources.
- Establish Strategic Infill and Redevelopment Strategy:** Identify key vacant sites for establishing redevelopment clusters with lasting impact that will allow the urban environment to become one cohesive environment that is easy to navigate and a pleasure to experience.
- Integrate Transportation Recommendations:** Reinforce transportation policy and incrementally implement the recommendations with any new development.
- Set a Realistic 10 Year Vision:** Identify strategies that can be implemented within a 10 year time frame.



Food Truck in Moline Centre



Mississippi River



Railroad Crossing in Moline Centre



Bass Street Landing



Florenciente Neighborhood



Edgewater Neighborhood

1.3 PLANNING FRAMEWORK



The Riverfront Communities: Benefiting from greater connectivity between strategic and important assets

Downtown Destinations



Proposed Trail Connections



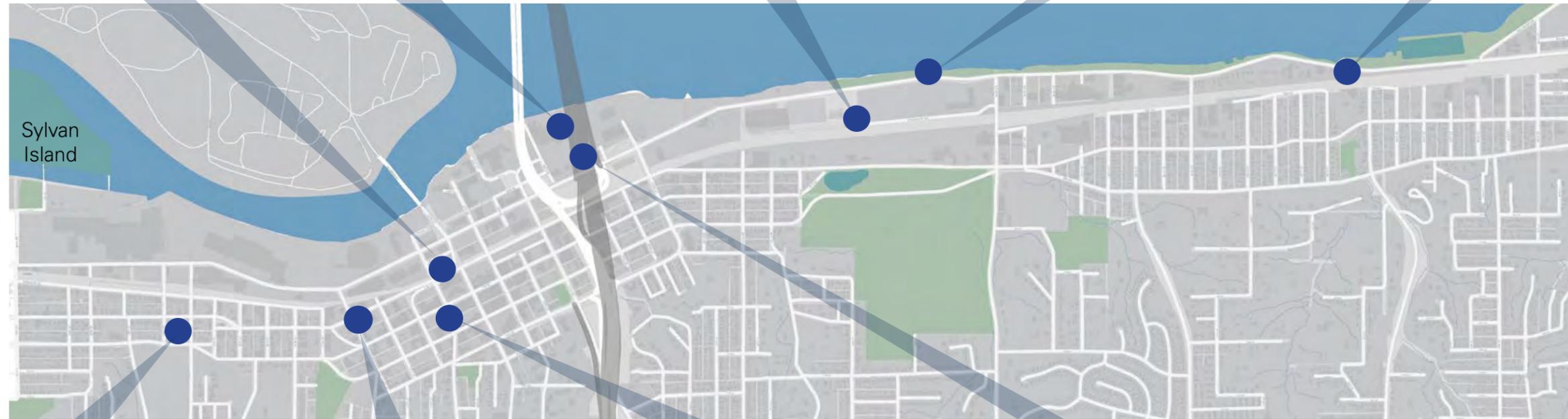
Growing WIU-QC Campus



River Transit



Great River Trail



Unique Neighborhood Character



The Q Multi-Modal Station



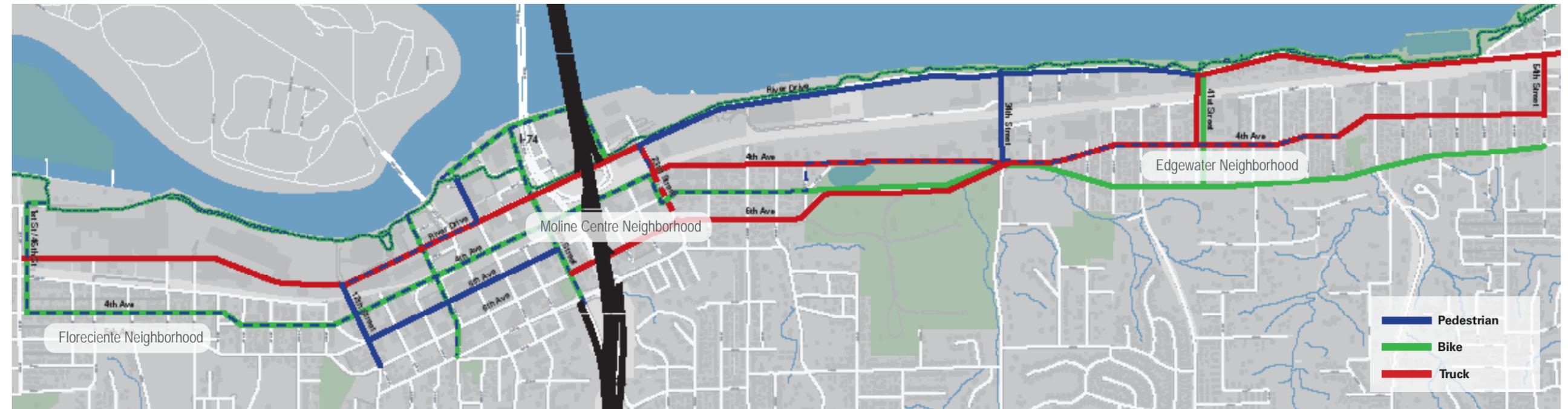
5th Avenue



New I-74 Bridge

The theme of “asset connectivity” is woven throughout this document and the many Plan recommendations. This page highlights some of the key assets. Moline’s Riverfront Communities can benefit greatly from greater connectivity between strategic and important assets

1.3 PLANNING FRAMEWORK



Proposed Street Hierarchy

Transportation Principles

Transportation affects all aspects of life in Moline that means a holistic approach to making improvements is required. The following are strategies for incrementally improving the transportation network in the Riverfront Neighborhoods:

- Establish a clear hierarchy of roadway functionality. Establish the key user (pedestrian, bicyclist, motorist, truck) for primary streets to help steer design decisions. Support multi-modal choices.
- Create a complete network for all modes of transportation. Design streets to ensure access between neighborhoods for all modes.
- Provide design concepts that support desired street use for short- and mid-term implementation. Identify pilot projects that can be implemented quickly and without significant cost.
- Support a “complete streets” approach by balancing the needs of trucks and automobiles with bicyclists and pedestrians. Make walking and biking a viable option for everyone in downtown.
- Recognize unique characteristics of River Drive, 4th Ave, 5th Ave and 6th Ave. Consider and support unique neighborhood context for each roadway.
- Promoted downtown growth and local retail centers by improving access and parking operations.
- Improve wayfinding throughout Moline Centre.
- Promote traffic calming on all streets with high pedestrian desirability, with retail and residential adjacencies.
- Support transit ridership through linking bus routes with community needs and enhancing bus shelters and pedestrian routes. Connect the future Q Multi-Modal Station to this network.

Recommendation

City of Moline should establish a new Street Hierarchy Map, reflecting the role of each street within the network for not only vehicles, but also for transit, and non-motorized transportation including pedestrians.

The map above outlines the hierarchy established as part of this study. It is used to guide the recommendations found throughout this document.

1.4 TRANSPORTATION APPROACH



New I-74 Bridge: Bettendorf Side Render

Key Transportation Components

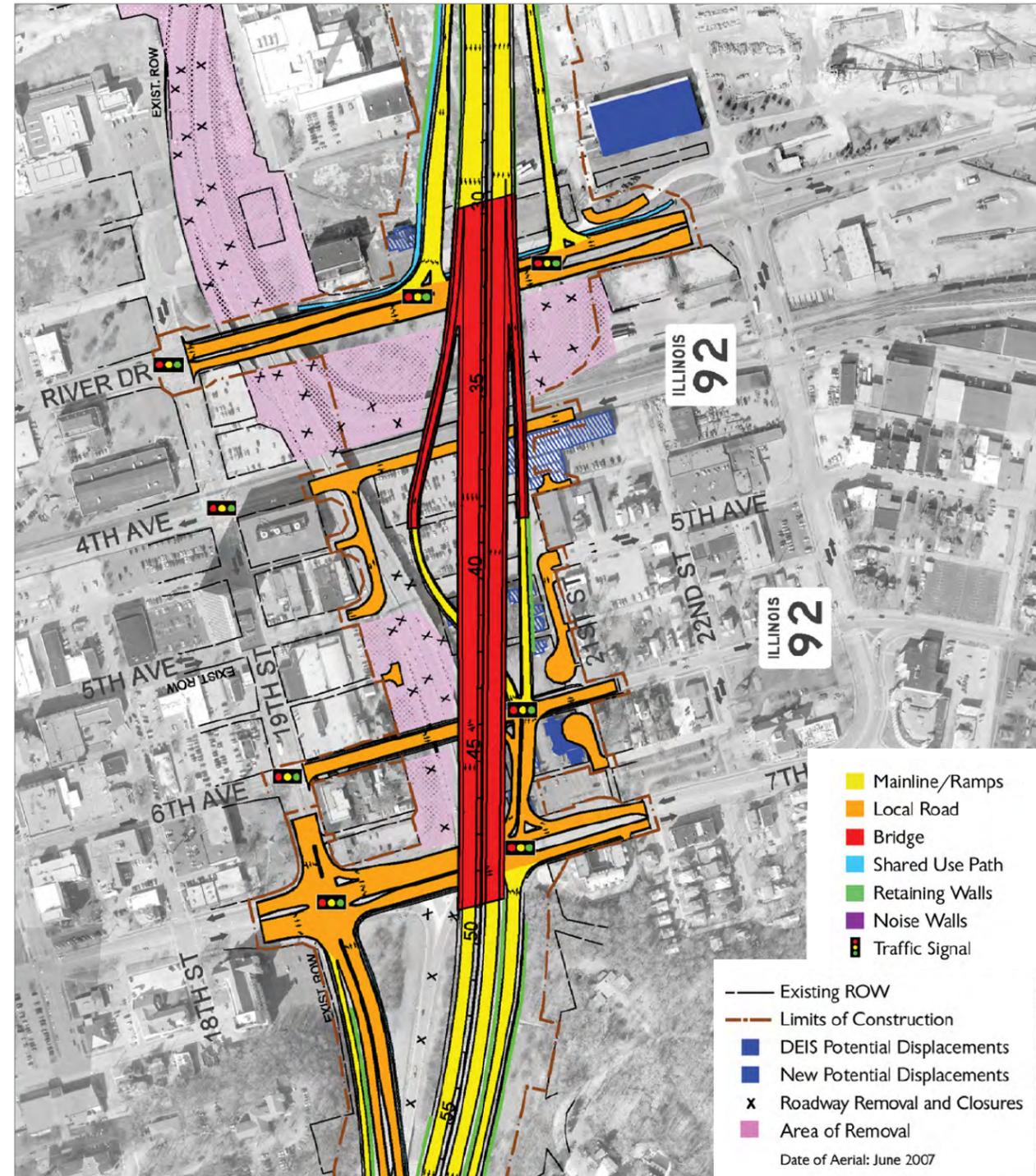
The City of Moline’s transportation network was analyzed to identify gaps, barriers or other issues related to the City’s pedestrian, bicycle, automobile, truck and transit environment. This planning effort relies upon previous plans and community engagement findings to prepare an updated transportation-focused plan for the neighborhoods of Moline Centre, Floreciente and Edgewater.

Information supporting the transportation roadway recommendations are based on local and sub-regional traffic circulation patterns, and impacts of the new I-74 bridge infrastructure on the western edge of Moline Centre and new multi-modal station on the eastern edge. Throughout the planning area, improving connections between destinations for

all modes of transportation is a priority. Existing pedestrian and bike routes are challenging. This Plan identifies ways to strengthen riverfront, downtown and neighborhood connections, with an emphasis on how to improve pedestrian and bike connections.

I-74 Bridge

The new bridge will transform transportation patterns, development sites and redefine the eastern gateway into Moline Centre. This Plan outlines ways the new bridge infrastructure can be integrated into the urban form, provide opportunities to increase connections between neighborhoods, preserve building character and establish cohesive areas for development with clear identity.



New I-74 Bridge: Realignment Plan

1.4 TRANSPORTATION APPROACH



Traffic and Parking Operations

Moline’s downtown is now, and will continue to be, a draw for locals and visitors from throughout the region who are attracted to the many local dining and activity destinations. It is vital for Moline to provide easy access to parking to support the needs of residents, visitors and vested businesses. Construction of a new I-74 Bridge, the Multi-Modal Station, and other developments in Moline Centre emphasize the need for the City of Moline to manage the demands on their current parking inventory and to better understand how to most effectively use their resources in the future.

To ensure increased desirability of Moline Centre for multiple purpose, having convenient, well-located, easily accessible and moderately priced parking is a major concern of property owners, local businesses, employers, visitors and residents. The Plan takes an in-depth look at Moline parking locations, options, operations and management for strategies to improve the parking experience and efficiency. Stakeholder interviews confirmed the need to address parking issues ranging from insufficient supply of parking in close proximity to destinations, to a lack of wayfinding and signage on existing facilities.

As development continues in Moline Centre, parking will become a more complex issue. As existing parking lots serve more residents and a growing business center, ensuring the availability of convenient spaces will be more challenging.

In an effort to better position Moline to manage its assets in the context of future development and growth in downtown, detailed parking recommendations including management and operations are outlined in a parking memorandum located in the appendix of this Plan.

Biking and Walking

Today the existing network of streets within Moline’s riverfront neighborhoods allows vehicles to move with relative ease and speed throughout downtown. As part of the roadway network, there exists a need for the expansion of safe, dedicated facilities for pedestrians and bicyclists. Growth of this multi-modal network must be carefully balanced to ensure the continued operation of long-standing riverfront industries and to support the economic growth of the city.

Growth in the local student population, a younger workforce moving into downtown, the development of The Q Multi-Modal Station with passenger rail, and the reconstruction of the I-74 Bridge will all contribute to a fundamental change in downtown circulation for all modes. To capitalize on this opportunity, the City must connect residents, visitors, and employees to area amenities via a safe and attractive network of walking and biking facilities. Inviting streets with neighborhoods serving retail that conveniently accommodate pedestrians and bicycles would help to promote the Riverfront communities as desirable place to live, work and visit.



Parking Garage at 6th Avenue between 16th & 17th Street



5th Avenue Downtown



Biker at the Riverfront Trail



13th Street & 5th Avenue Moline Centre



Downtown Bus 15th Street



4th Avenue Future Multi-Modal Station

1.5 URBAN DESIGN

Community Development and Design

Moline Centre and the adjacent neighborhoods of Floreciente and Edgewater were once urban walkable environments at a time when industry and manufacturers were central to their immediate communities.

Over time these neighborhoods' walkable urban character was eroded by the dominance of the automobile and industrial truck traffic. Today, the desire for walkable urban environments has once again risen to the forefront of conversation regarding livable communities. Quality of life choices are being made by a new generation who are seeking more urban lifestyles based on things such as proximity to work, shopping, entertainment and recreational amenities. As the City of Moline looks forward, there is an opportunity to re-think and re-position these three riverfront neighborhoods as a way to improve the quality of life for all residents and to grow in such a way as to attract more people and amenities to these neighborhoods.

Moline Centre

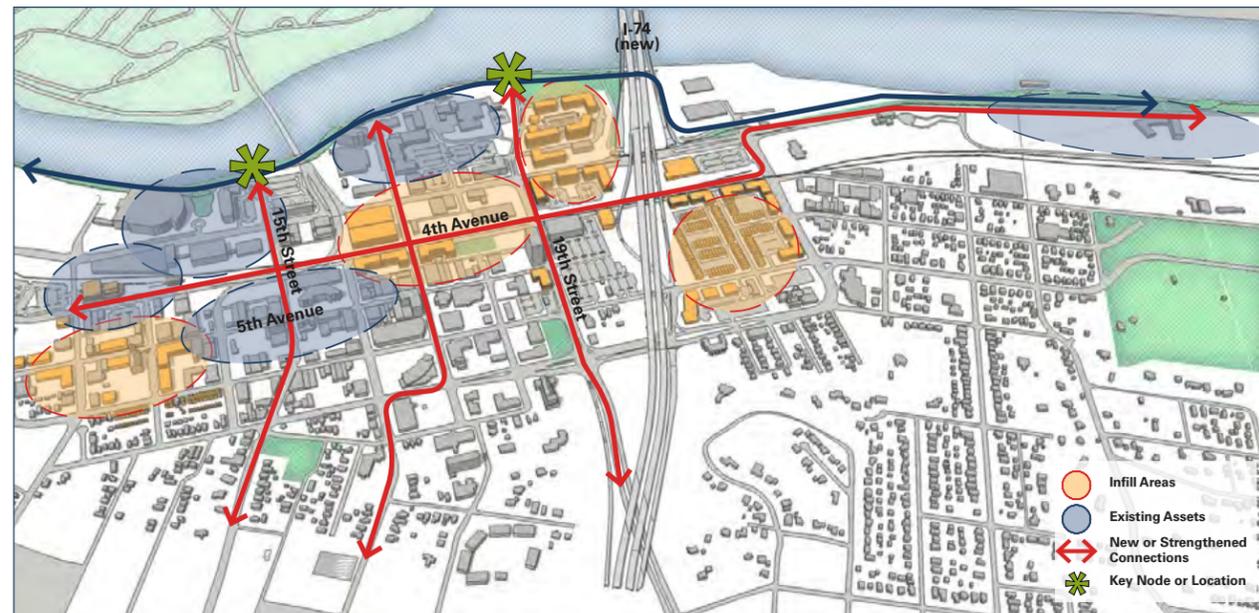
Through integration of transportation, land use planning, and urban design, Moline Centre will continue to grow as a vibrant neighborhood. Moline Centre can thrive as a mixed-use district that promotes a live-work-play approach, extending the life of the district to evenings and weekends. The heart of Moline is an accessible, desirable destination for residents and visitors alike. Strategic development,

infill and infrastructure improvements will allow the urban environment to become one cohesive, legible environment that is easy to navigate and a pleasure to experience.

Neighborhood Urban Design

Both the Floreciente and Edgewater neighborhoods are disconnected from the key assets of Moline Centre and the Mississippi Riverfront. This Plan investigates in detail many of the transportation objectives in these neighborhoods including traffic calming and how to re-define truck routes to increase pedestrian, bike and vehicular safety. The Plan also identifies opportunities to create a multi-use path to connect Edgewater and Moline Centre via a strategically located bike and pedestrian route. Sidewalks, crosswalks and intersection improvements were identified to help connect residents to neighborhood assets by providing better access bus transit, parks, bike trails and community amenities. Opportunities for future commercial and retail development exist at strategic locations and key intersections.

New loft conversion of historic buildings in Moline Centre and the new Western Illinois Quad-Cities Campus are significant factors in drawing new residents to these neighborhoods. Although the market demand is limited, well designed infill / mixed-use development will help to continue the downtown and riverfront neighborhood desirability. New development will also fuel renovation of existing building stock.



Opportunities for Moline Centre Neighborhood



Special Events Examples: Food Trucks



Special Events Examples: Farmer's Market



Existing Conditions along 4th Avenue in Edgewater



Visualization of an Enhanced Streetscape along 4th Avenue

1.5 URBAN DESIGN

Moline Centre Gateways

At the periphery of Moline Centre, at the eastern edge near the Edgewater Neighborhood and at the western edge near the Floreciente neighborhood, there is an opportunity to create distinct and significant gateway zones. With careful land and transportation planning these areas can be catalysts for development.

Eastern Gateway

This gateway zone encompasses the land where the old I-74 bridge will be demolished, as well as the eastern edge of Moline Centre. A portion of the land adjacent the River is designated as part of the Mississippi River Floodway. This Plan identifies opportunities for creating more open space within the floodway zone and the associated ecologic benefits. While rethinking the impacts of the new I-74 bridge, the land immediately adjacent to the new bridge, outside the floodway, can be repositioned into a new residential mixed-use neighborhood and commercial node.

Western Gateway

This gateway zone encompasses the land adjacent to The Q Multi-Modal Station, the Community Health Center and i-Wireless Center and has access from downtown to the tourist destination at Bass Landing. This gateway is an opportunity to transform a district from a series of individual properties into a interconnected and vibrant zone with a strong sense of place.

Active Programming

Many of the recommendations in this Plan outline ideas for active programming to increase visitor, resident, retail and business attraction.

Park Space

Parks are critical to creating livable communities in urban environments. This Plan outlines ways to increase neighborhood access to existing parks, increase park programming and offer ecological benefits to addressing flooding and storm water.

Streetscape Design

Attractive and well designed streets are significant neighborhood assets that can not only beautify a community but keep increase pedestrian and bike safety.

Wayfinding

Strategically located signage for vehicles, pedestrians and bikes can dramatically improve the visitor experience in and around Moline Centre. With the changes in roadway design due to the new I-74 bridge infrastructure, this Plan outlines ways vehicular, bike and pedestrian circulation can be improved to reduce and eliminate conflicts. Having well marked bike routes, directional signage to key downtown attractions and bridge ramp access will help both visitors and locals.



17th Street Streetscape



Corporate Sponsored Tricycle Race Example



5th Avenue Streetscape



McCandless Park in Moline



Family Fitness Event Example



Signage in Moline

1.6 MARKET ANALYSIS

A Market Perspective: Quad Cities Demographic Snapshot

Population Trends (2003-2013)

The 6-County region's population grew by 2.3% (10,723 people). Scott County's population grew by 7.8% (12,337 people) while Rock Island County's population, including Moline, declined slightly at less than 1%. These changes can primarily be attributed to natural change offset by in-migration. For example in 2013, 16% of Scott County (Iowa) gains offset Rock Island County (Illinois) losses.

Quad Cities Employment Trends (2003-2013)

Rock Island County has strong employers, including Deere & Company and the Rock Island Arsenal. The employment numbers for the 6-County region declined by 1.2% from 2003-2013. In the Quad Cities Bi-State area, Scott County (Iowa) gains offset Rock Island County (Illinois) losses. Many employees work in another county and state, and there is a unique shift of day and night time population. For example in 2013, 16% of Scott County residents work in Rock Island County.



Quad Cities Commuter Trends (2003-2013)

Market Assessment

An understanding the market potential is fundamental to creating successful infill development of strategic vacant sites and to re-position under-utilized properties in Moline. As part of this planning effort, Valerie S. Kretchmer Associates (VSKA), a real estate and planning consulting firm based in the Chicago area, prepared a market assessment of the key sectors including residential, office and retail for Moline's riverfront communities. To update to the 2014 market data, VSKA interviewed numerous local developers and brokers, and then analyzed demographic data in relationship with local real estate development and construction data, to better understand the market demand in Moline for key market sectors.

2014 Market Analysis

The City of Moline economic outlook and market assessment prepared for the 2014 Comprehensive Plan Update has not changed significantly in a year since completion of the Plan. The following is a brief synopsis the 2014 findings relevant to this Plan and in conjunction with 2015 market research prepared by Valerie S. Kretchmer Associates (VSKA).

Economic Conditions

The neighborhoods of Moline Centre, Floreciente, and Edgewater have very different economic issues and goals that require a variety of development strategies, tools, and incentives.



Local Retail and Dining: Riverhouse Bar and Grill at 1510 River Drive

1.6 MARKET ANALYSIS

Moline Centre’s local economy is supported by a diverse base of employers, including significant office anchors, service providers, restaurants and bars, and retailers. Local employers continue to invest in downtown, building the downtown workforce and demonstrating a long-term commitment to the City. Moline Centre is seeing new housing development, restaurants, and other investment. The clustering of dining and entertainment venues provides an anchor for downtown vitality. Diversifying the retail base will further establish downtown as a destination.

The neighborhoods to the east and west of Moline Centre are experiencing different economic and development environments. Edgewater’s future will be driven primarily by the expansion of Western Illinois University’-Quad City’s expansion, including residential student housing and planned retail amenities.

Florecente faces a complex set of challenges: while there are pockets of entrepreneurship, it is a neighborhood in need of economic development to combat decades of disinvestment. Current issues include aging and deteriorating building stock, the closing of a neighborhood school, and high vacancies in neighborhood retail spaces. Florecente, while stable in its population, has the low household incomes with limited community resources. However, recent projects such as the new Community Health Center, the new Heritage Church /Esperanza Center, and the rebuilding / renovation of homes by Habitat for Humanity are addressing some of Florecente’s community needs.

Edgewater’s development, on the other hand, is being driven by the ongoing growth of WIU-QC. Edgewater is the neighborhood with the highest incomes among the three neighborhoods yet it is still only 71 percent of the City’s median income. Household ages account for part of this difference: younger households typically earn less than older households. Edgewater’s population, at a median age of 38, is older than Florecente’s median age of 28 but is still younger than the City’s median of 40. Key economic issues include leveraging the impact of WIU-QC, re-thinking obsolete industrial land uses in strategic locations and attracting new neighborhood retail.

Demographics

The City of Moline’s population is stable and forecast to grow very modestly. In the three neighborhoods of the study area, the racial and ethnic mix is forecast to remain stable, but there are particular disparities in income and education when compared with the City overall. Educational attainment is low in all three neighborhoods, with only about one-third of the over-25 population having graduated from high school. In Florecente and Edgewater, and only 3 percent of the population has graduated from college. Some of these findings will influence housing development and retail; and will require additional social services and job training.



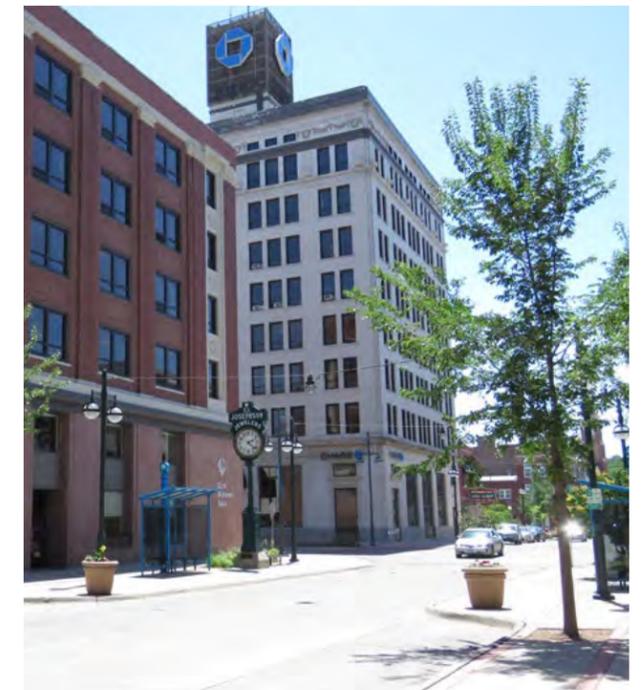
Phillips Lofts at 1320-1322 5th Avenue



LeClaire Apartments at 421 19th Street



Skinner Block Lofts at 1354 River Drive



Chase Bank Building at 501 15th Street



John Deere Pavilion at River Drive



KONE Centre & Stoney Creek Hotel



WIU-QC Campus at 3200 River Drive

Key Features:

The following are key economic generators for Moline Centre, Floreciente and Edgewater Neighborhoods.

Major Employers: Deere & Company, KONE, United Healthcare, Moline Forge, Evans Manufacturing, Parr Instruments, and WIU-QC.

- Multi-tenant office buildings: Heritage Center, Heart of America, Caxton Block.
- Rock Island Arsenal. The Arsenal is both a tourist facility and major employer in the Quad Cities. More than 6,000 civilians and military personnel work on the arsenal.
- Hotels: Radisson Hotel, and Stoney Creek Hotel and Conference Center together hold 302 rooms, meeting spaces, and restaurants. When the new hotel at The Q Multi-Modal Station is complete, 95 additional rooms will be introduced to the downtown.
- John Deere Pavilion: 250,000 visitors per year come to this event center. The Pavilion provides an exciting, interactive experience to see, touch and understand the history of Midwest agriculture and the development of state of the art farm, construction, forestry and consumer equipment. The Pavilion is a major tourist attraction with its awe-inspiring combines, tractors, bulldozers and other fascinating machines.
- WIU-QC: This campus has the fastest growing student population of all Illinois state universities. Enrollment in the fall of 2013 was 1,237 students.

Riverbend Commons is a new mixed-use residential development directly west of the campus.

- Restaurants: The Downtown Core features more than 20 restaurants offering a variety of cuisines. According to the survey, restaurants are the key draw to Downtown. Establishments range from upscale dining to the casual bar & grill and ethnic specialties. Weekends and post-event evenings are strong revenue-generators due to proximity to the iWireless Center, Riverfront events, and regional access provided by the nearby I-74 exits.
- Specialty Retail: Moline Centre includes specialty/ boutique retailers that offer unique goods, services, and food items often in restored historic storefront spaces. 5th Avenue from 14th to 18th Streets is the primary “main street” retail location, but shops are also found along River Drive near the Arsenal Bridge, on portions of 6th Avenue, and in the Bass Street Landing development at 18th Street and 2nd Avenue.

Retail

Moline Centre primarily serves as a retail and entertainment destination. Sales gap data for the study area shows modest needs in several categories, including groceries, drug store, and other convenience needs. With downtown serving as an attraction for regional customers (particularly for restaurants), there are opportunities for retail expansion and diversification despite neighborhood based sales gaps.

Demand for retail development will be driven primarily by housing and employment growth in and around Moline Centre. Currently the retail demand generated by new housing can support some additional small scale amenities, but cannot yet support a large format supermarket. As an interim measure to meet the local need, existing food stores should be encouraged to adjust their merchandise mix to address the changing demographics for new residents. The strong restaurant scene, along with the success of a few specialty stores, suggests that small-scale infill retail (particularly apparel and home furnishings) can be supported on or near blocks where restaurants are already driving traffic.

With continued growth in downtown office, residential, and lodging, and with the planned expansion at WIU-QC, the three downtown neighborhood districts can likely sustain additional retail in targeted categories.

The ground-floor business mix in downtown is spread over a relatively large area, disguising both its strengths and weaknesses. Among its strengths, retail and restaurant segments account for a majority of ground-floor businesses. Conversely, approximately one-quarter of ground floor commercial spaces in the retail core are vacant.

Typically, retail will follow rooftops and should be in close proximity to housing. Retail is an important amenity for downtown residents and workers. As more people live downtown, there will be a need for additional convenience retail. The Western Gateway

1.6 MARKET ANALYSIS

of Moline Centre is a prime opportunity location for complimentary retail development near The Q Multi-Modal Station, especially once the Starwood Element Hotel opens. Convenience retail will also compliment the new Community Health Center. In planning for new retail, it is important to analyze visibility, wayfinding, and safe walking and driving routes to ensure success. On the eastern gateway to Moline Centre, the frequency of freight trains affects retail opportunities especially near the WIU-QC campus where freight trains can restrict access.

Examples of Existing Downtown Retail include River Station at Bass Street Landing at 1601 River Drive and Brix at 425 15th Street.

Office

Quad Cities office market, including Moline Centre, is soft and won't be a major development driver in the near term. The Illinois Quad Cities, vacancy rate is 28% for Class A, 19% for Class B, and 35% for Class C office space. Companies reportedly favor Iowa's perceived lower taxes and government stability at the state level. Several vacant office spaces in Moline include some of the best quality buildings along River Drive. Rents are consistent with lower-priced new and Class A office space throughout the Quad Cities with the exception of the KONE Centre. The speculative office market is soft with a current vacancy of 19%-35%. Demand for new office space in Moline Centre could come from companies seeking new or more space, especially those already in Illinois.

Examples of high-quality offices in Downtown Moline include the Caxton Block at 1701 River Drive and the KONE Centre at 1 KONE Court

Residential

Downtown Moline is an attractive residential location due to the variety of amenities and urban activity with Quad Cities residents and workers feeling positive toward Downtown Moline as a dining and entertainment destination.

Approximately 440 housing units are located in Moline Centre (source mysidewalk.com 2010 census block data) with an additional units in design and under construction. The profile of the residents seeking downtown apartments and condominiums is typically young to mid-career, and are both singles and couples. Empty nesters are not exhibiting a strong interest in living downtown however that could change in the future as convenience shopping and amenities increase. Average residential sale prices in the Quad Cities have increased since 2013, driving new residential development in Downtown Moline, especially market-rate apartments.

Demand for downtown living can be attributed in part to major downtown employers such as John Deere and KONE, and employees who choose to live downtown for convenience to work and an urban lifestyle. A variety of factors drive residential growth and it is important to consider the competition amongst the Quad Cities. Davenport has many historic buildings that are and continue to be developed for residential.



River Station at 1601 River Drive



Caxton Block at 1701 River Drive



Brix at 425 15th Street



KONE Centre at 1 KONE Court

1.6 MARKET ANALYSIS



Moline offers an advantage by having a walkable cluster of restaurants and entertainment venues along 5th Avenue. The vitality associated with this restaurant and entertainment district can be a significant market factor in attracting new residential development to downtown Moline. However most historic buildings in Moline Centre of a viable scale have already been converted to housing.

Examples of existing rehabbed apartments in Downtown Moline include Phillips Lofts at 1320 5th Avenue and Skinner Block Lofts at 1354 River Drive.

Residential Recommendations

Opportunities for Apartments in Downtown Moline

- Developers plan to add more apartments downtown over the next 5 years.
- Rental rates may approach \$1.50 PSF.
- Plans include potential corporate housing.
- Occupancy at newly built, high quality, student housing near WIU-QC has improved greatly since opening last year.
- Plans are moving ahead for non-student, market rate, apartments.

It is anticipated that residential demand in the near term will come from renters and any new construction on vacant and under-utilized land will take place in the Downtown Core. The new bridge construction and demolition of the old structure will discourage residential on the east end of downtown for the short term.

Market Perspective: Residential

- Residential demand in the near term will come from renters and the for-sale housing will follow the market-rate renter base. The addition of residential units will need to drive development downtown. Market rate residential development is a crucial factor in the buildup of Downtown Moline.
- Market rate will require some incentives. Most of Moline’s rental properties to date, have used Low-Income Housing Tax Credits. Developing high quality market rate rehab rentals without the state historic preservation credit which are available in Iowa, is a challenge for Moline to be competitive.
- Apartments could be added in the form of smaller 12-unit rehabbed buildings along 6th Avenue.
- Townhomes for rent or sale may also be possible along the Mississippi River and east of I-74 once the new bridge is complete.
- New construction on vacant and under-utilized land in the Downtown Core will be more desirable in the near term and precede new construction in the bridge area.

Market Perspective: Retail

- Retail will follow rooftops and should be in close proximity to housing, which is especially true with neighborhood serving retail such as grocery, convenience, and drug stores. Increasing the local neighborhood population will be critical to bring in national brands.



Downtown Residential: Enterprise Live-Work Lofts



Heritage Place office building on 5th Avenue



The Mills at Riverbend Commons



5th Avenue Retail

1.6 MARKET ANALYSIS



- Restaurants are faring well in Downtown Moline and are attracting residents, workers, and visitors from the city and neighboring jurisdictions.
- More restaurants are in the works in the vicinity of John Deere Commons and the WIU-QC areas, with at least one under construction.
- Given the proximity of students and workers paired with the entertainment district environment of the Downtown Core, more restaurants should be supportable.
- Increasing the number of specialty retailers in Downtown Moline would create a more concentrated draw for shoppers to like to park once and walk or to stroll from nearby hotels. Rents can't be too high in order to facilitate this.
- Ruhl Commercial reports downtown net rental rates for retail space of \$6-\$12/SF across all the Quad Cities. These are reasonable, as suggested by the estimated vacancy of 5% – a low rate.
- Large format chain retailers would be difficult to attract downtown and would be less appropriate for the district. There is vacant land and available existing space to the south along John Deere Road, where large retailers will more likely locate.
- As more people live in Downtown Moline, there will be a need for additional convenience retail, “staple” shopping and personal services. Visibility, accessibility and reasonable pricing will be very important to support this.

- A 40,000 SF grocer is being considered at Phase 2 of the Riverbend Commons development
- The current prospect would stock a variety of standards, along with ethnic foods and a sizeable prepared food component. This would be appropriate for the lunchtime and student-oriented location.
- The extended stay hotel to be built at the multi-modal center is planned to include some retail, eating and entertainment open to the public. There will be some opportunity beyond this for spillover development, but it could be limited without the addition of more residences nearby.

Market Perspective: Office

- Demand for new office space could come from companies seeking new or more space, especially those already in Illinois.
- As a larger number of people are attracted to live in Downtown Moline and more amenities are added, companies already in the Quad Cities and the city will be more likely to fill existing office space and support a modest amount of additional small offices, including medical offices. Preferably, these can be located in adaptively reused older buildings to hold down rents.
- The office market is soft in Moline, including in the downtown as is the case elsewhere in the Quad Cities. However Iowa has more favorable corporate and property taxes making it more attractive to companies.

- Several vacant office spaces in Moline include some along River Drive in the City's best quality office buildings (KONE Centre, Riverstation, Caxton)
- Vacancy rates are much higher for Class A and Class C office space in Illinois than in Iowa, but they are similar for Class B offices.
- These existing spaces with 3,000-4,000 SF or less are attractive to companies already located in Moline and the broader Quad Cities, including service providers, small businesses and large firms seeking overflow space.

- There is weak demand for multi-tenant office development. Any significant office development will be driven by large single users.
- The Quad Cities have little in the way of large corporate offices with 10,000-30,000 SF on a single floor. While the new KONE tower has space available on this scale, it is reportedly priced well above what the market can currently support (\$25-\$30/SF).

Residential Moline Centre - Historic Renovation and New

Project Name	# Units	Unit Types
Skinner Block Lofts	12	1 to 3 BDRM
Chase Bank Building redevelopment	25	1 to 2 BDRM
River Bend Commons (new) (WIU) Student Housing	25	2 to 4 beds

Residential Downtown Davenport - Historic Renovations

Project Name
Market Lofts, Renwick, Hotel Blackhawk and Forest Block (all renovated in the past 5 years)

Most historic buildings in Iowa were developed using Iowa's state historic tax credit

Residential Planned Projects

Project Name	# Units
East Moline: Fountain Head	344
Bettendorf : Townhouses at 53rd street	42
Davenport: market rate residential (WIU) Student Housing	241

1.6 MARKET ANALYSIS



PROPOSED OR PLANNED DEVELOPMENTS 2015



The map outlines 2015 key proposed or planned redevelopment sites within the riverfront neighborhoods. The sites include a wide variety of conditions including of strategic infill locations, repositioning of existing property such as the former KONE site. The map also identifies catalytic clusters of development including the area around The Q Multi-Modal Station, the Western Illinois University–Quad Cities campus and the redevelopment of the land remaining after demolition of the old I-74 bridge

- 1** Parcels owned by John Deere(w) and Red Cross(e) - used as overflow parking by iWireless. No plans for redevelopment.
- 2** Parking lot owned by John Deere and used by iWireless. City working to acquire parcels for use as additional parking for Multi-modal Station (MMS).
- 3** City-owned block*. Future parking lot for MMS and Hotel. *SW corner not owned by city.
- 4** Prime development area. Owner of parcel is amenable to development around the Community Healthcare building. Potential consideration of some portion of parcel for 1-way-2-way tie in.
- 5** Ice rink/Sports arena described in Comp Plan on hold; Potential for grocery, drug store or open-air Mercado.
- 6** Owned by the Civic Center Authority, rented to Radisson.
- 7**
 - a. City-owned parking (very busy). No plans for development.
 - b. Parking for JD Commons - No immediate plans for redevelopment. Parking is over-dedicated in development agreements with JD Commons businesses.
 - c. City-owned parking. No immediate plans for development.
- 8** East side of parcel owned by John Deere - Used for JD IT Center employee parking (~500 employees).
- 9** Potential residential redevelopment opportunity
- 10** Site identified as site for future hotel* or other mixed use development (earliest 2017). *Included in the development agreement with MMS Hotel there has been a moratorium for hotel construction for 1 year after MMS opens.
- 11** Mixed use development (parking, retail, residential). Under negotiations to begin development 2016. Structured Parking to serve adjacent future hotel development
- 12** Development area. Interest expressed in Comp Plan to increase residential density to create a 'University Village'. No interested developers at present.
- 13** River Bend Commons expansion-office, commercial (restaurants/convenience retail), structured parking, and housing
- 14** Location for new water taxi stop.
- 15** New bridge to Sylvan Island - construction to begin early 2017.
- 16** Former Ericsson School Site - New Heritage Church-Community Center. Newly renovated building. MetroLINK to provide bus service for relocated students.
- 17** Kone facility. Potential for redevelopment.
- 18** New bike path as currently proposed by City (connection to west side of new bridge). Note: funding for the trail extension has been approved by our local MPO, but is currently subject to authorization/re-authorization of a transportation bill.
- 19**
 - a. Potential site Amtrak parking or MMS employee parking.
 - b. Potential RFP for re-development.
- 20** 12th street will need to be rebuilt to accommodate new train tracks that are 2 feet higher than current street. Possible complete streets construction.
- 21** New John Deere IT Center parking lot. There may be potential for development above at-grade parking on this site so that surface parking does not dominate the 19th Street corridor.



2.0 TRANSPORTATION

MOVING MOLINE FORWARD: URBAN DESIGN & TRANSPORTATION UPDATE TO THE
2014 COMPREHENSIVE RIVERFRONT PLAN

JUNE 20, 2016

2.0 INTRODUCTION



5th Avenue in Moline Center. A great example of a pedestrian oriented commercial street.

Transportation Overview

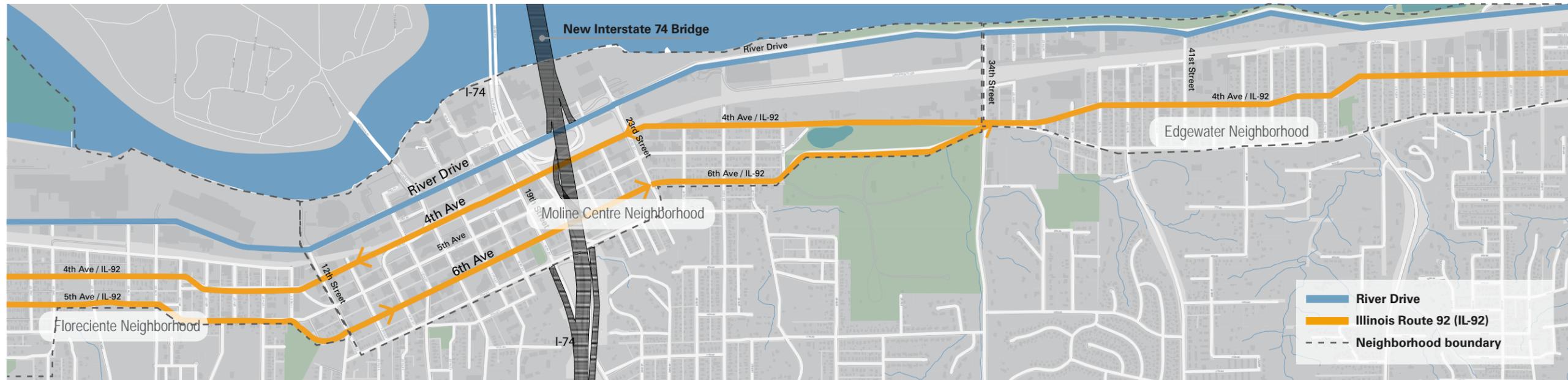
Over the past few years, Moline has completed a number of different planning projects in the riverfront neighborhoods of Moline Centre, Floreciente, and Edgewater. The purpose of this Chapter is to dive deeper into a few specific transportation issues, including alternative roadway improvements and initial implementation design concepts for consideration. Each recommended transportation design concept supports the City's goal to increase residential density and improve the livability and connectivity of the downtown neighborhoods for visitors and residents traveling by all modes of transportation.

The existing network of streets within Moline's riverfront neighborhoods allows vehicles to move with relative ease throughout downtown. Streets are built with wide lanes and large turning radii, posing few challenges to traffic flow of even the largest trucks in the area. As part of the roadway network, there exists a need for the expansion of safe, dedicated facilities for pedestrians and bicyclists, and improved connections to the areas transit assets. Growth of this multi-modal network must be carefully balanced to ensure the continued operation of historic riverfront industry, and to support the economic growth of the city.

The overall intent of the Plan recommendations is to provide guidance for the growth of high quality networks for all modes of transportation, within the riverfront neighborhoods. This Plan offers the City the "first step" towards implementation of catalytic projects by including concept drawings outlining potential alternatives for the project, and supporting the key recommendations. Concept drawings represent a starting point from which the City's planning and engineering staff can respond to new ideas, and begin developing project design alternatives.

It is important to note that after adoption of this Comprehensive Plan Update, the next step will be to begin engineering studies and advance implementation strategies.

2.1 EXISTING ROADWAYS



Existing Roadways

Existing Conditions and Key Issues

Moline’s existing transportation network consists of an urban street grid throughout the downtown with strong east/west roadway connections to adjacent neighborhoods, via Illinois Route 92 and River Drive. Throughout the comprehensive planning process community stakeholders voiced concerns that speeding occurs along segments of IL-92 and River Drive and traffic calming is needed.

Illinois Route 92

Illinois Route 92 (IL-92) is the primary east-west thoroughfare through Floreciente, Moline Centre and Edgewater. IL-92 operates as a couplet, or pair of one-way roads, on 4th Avenue and 5th/6th Avenue

between the City’s western municipal boundary and 34th Street. This one-way pair arrangement accommodates high volumes of fast moving traffic. As outlined in the 2004 Comprehensive Plan Update, IL-92 is designed to handle more traffic than it currently carries even during most peak times of the day.

River Drive

River Drive, which travels east-west through Moline, is located north of IL-92 along the Mississippi Riverfront. The industrial properties along River Drive generate a significant amount of truck traffic. Community stakeholders stated that the high volumes of trucks in combination with speeding vehicles are significant issues along River Drive east of Interstate 74, and requested traffic calming measures.

Interstate 74

Interstate 74 (I-74) is a north-south interstate highway that travels through the center of the Quad Cities, in Moline and Davenport. The bridge section of Interstate 74 that crosses the Mississippi River carries approximately 73,000 automobiles and 3,700 trucks daily according to 2014 traffic counts. In Moline the new I-74 bridge will be relocated two blocks east, as shown in the map above. The Iowa bound bridge will open by the end of 2019 with two lanes in each direction. The second span is expected to be completed and open by 2020.

At completion, the new bridge will have access ramps at River Drive, 6th Avenue, 7th Avenue and 19th Street. The new alignment involves bringing

5th Avenue to a dead end on both sides of the new bridge structure. Community stakeholders expressed concerns that the bridge is a barrier between Moline Centre and Edgewater and will impact connectivity for pedestrians, bicyclists and motorists traveling east and west. One of the goals of this study is to mitigate the negative impacts of the new I-74 bridge on all modes of transportation.

2.2 TRANSPORTATION APPROACH

Strategies for Roadway Network Enhancement

Transportation and mobility affect all aspects of life in Moline requiring a holistic approach to making improvements. The following are strategies for incrementally improving the transportation network in the Riverfront Neighborhoods:

- Support a “complete streets” approach by balancing the needs of trucks and automobiles with bicyclists and pedestrians.
- Establish a hierarchy of street use within downtown Moline that supports multi-modal choices.
- Focus on “Asset Connectivity” within the neighborhoods.
- Make walking and biking a viable option for everyone in downtown.
- Support downtown growth and local retail centers by improving access and parking operations.
- Improve wayfinding throughout Moline Centre.

Complete Streets

In keeping with the Quad City policy on complete streets developed by the Bi-State Regional Commission, this Plan supports designing the public right of ways to address the safety and accessibility of all users. This balanced approach gives consideration to the type of users of the transportation system including pedestrians, bicyclists, transit riders, motor vehicles, and freight interests in the design and operations of a street.

Adjacent land uses and connectivity of local assets are also a major considerations in the design of the public right of way, to ensure critical transportation needs are met. As a result, individual streets can not be designed in isolation and need to be thought of as one component of the larger transportation network.

Network Thinking

Network thinking acknowledges that within a street network, changes made in one area will have an impact on the function of a street in another area. It also requires that for each mode, the individual or vehicle must have the ability to move with relative ease in any direction from any place. In an ideal network, there exists an intuitive “path of least resistance,” which guides the individual to destinations. A complete network moves **people** safely and efficiently, regardless of their mode of travel.

Functional Street Design

In the riverfront neighborhoods, streets currently serve vehicles and trucks well. The qualities that make these streets comfortable for motorists often are the same qualities that make them more challenging for pedestrians and bicyclists. When defining networks for all modes, the key user of the street must first be determined to allow for context specific design decisions, which allows the street to serve its role within the network. These design decisions do not make streets impassible for other modes, but instead build a more intuitive network for each, to create a more fluid whole for all modes.

Design Considerations

Comparing the needs of a pedestrian and a truck illustrates the very real challenge in the study area today. For example, in an urban context, best practice typically suggests small curb radii to shorten crossing distances at intersections, however these conditions can make a turn difficult or impossible for turning trucks. Similar conflicts arise when considering aesthetic enhancements such as tree canopy. Tree canopy on a local street provide shade and support the neighborhood feel of the streetscape, but low-hanging branches close to the street edge can be a hazard to tall vehicles. Different modes have different needs and design requirements. In order to make these design decisions, it is necessary to determine a key user of each street. Some design elements are not required, but make the experience on the street safer or more comfortable for the key user.

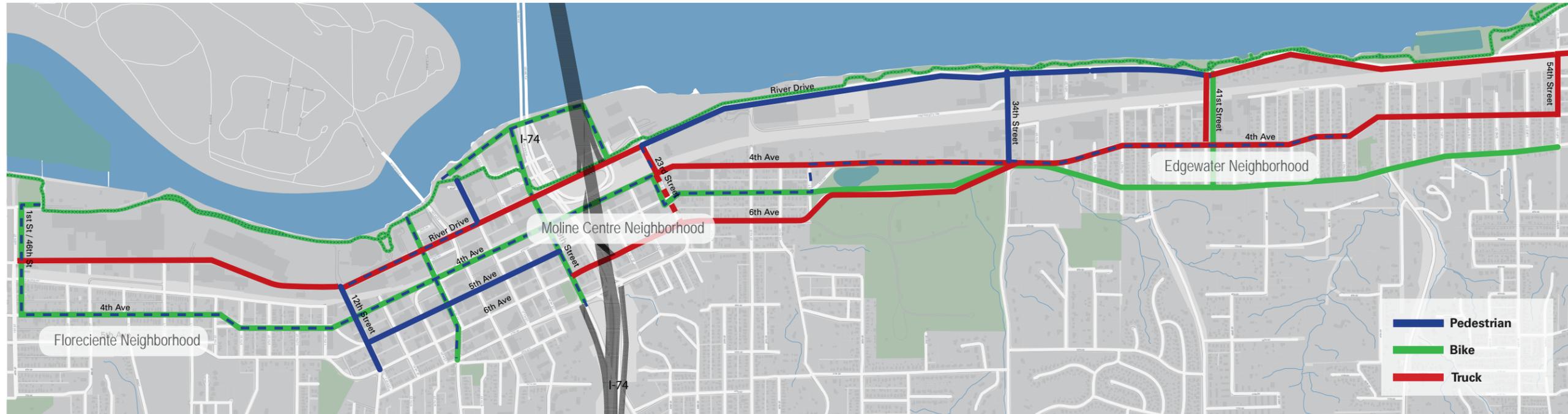


Great River Trail at 19th street adjacent to I-74 bridge



17th Street at 5th Avenue

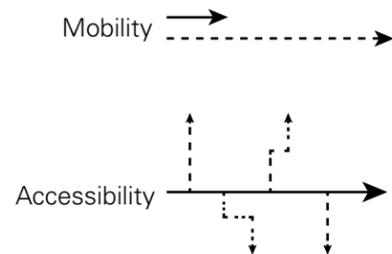
2.2 TRANSPORTATION APPROACH



Proposed Street Hierarchy

Functional Roadway Classifications

Streets serve two primary travel needs: mobility and accessibility. Mobility refers to the ability to travel longer distances with fewer delays; accessibility describes the ability to reach more destinations within the distance of travel with ease.



The ability for a street to function better for one purpose over another purpose has to do with its design

and function within the larger street network. For example, streets offering higher mobility to drivers may have a straighter path, with fewer intersections. Streets offering more accessibility might have more driveways or intersections, offering a driver more opportunities to reach a wider variety of destinations.

The Illinois Department of Transportation (IDOT) established a Roadway Functional Class System that uses the defined function of a street – considering its role to support driver access or mobility – within the larger network of streets. In addition to the Functional Class System, IDOT establishes a network of Local and State Truck Routes, which further regulate the design of such roads for larger vehicles.

A street’s Functional Class also determines some design standards for each street to ensure that the anticipated vehicles using the streets have safe passage. These Functional Class designations are important to consider in a place like Moline – where industries and major employers in the area have unique needs for larger vehicles and trucks on the City’s streets.

Creating a street hierarchy entails redefining the use of key streets through design improvements that support the priority user with descriptions of changes to the function of key streets within the network.

Recommendation

City of Moline should establish a new Street Hierarchy Map, reflecting the role of each street within the network for not only vehicles, but also for transit, and non-motorized transportation, including pedestrians.

The map above outlines the hierarchy established as part of this study. It is used to guide the recommendations found through out this document.

2.2 TRANSPORTATION APPROACH

This transportation approach is the foundation for a series of strategies and recommendations to incrementally establish transportation network enhancements and roadway modifications to address the changing land use dynamics in the three riverfront neighborhoods. The following are a summary of key transportation strategies that are described in greater detail further in this chapter.

1. Reconfigure Truck Circulation

The land uses along the Mississippi River are shifting from heavy industrial land uses to commercial, residential, tourist/entertainment and open space/recreational uses in Moline Centre; and to academic uses in Edgewater with the new Western Illinois University (WIU) Campus development; and park uses in Florencia. The influence of the parkland along the Mississippi River will continue to evolve with the popularity of the Great River Trail and Butterworth Parkway connecting all three communities and beyond. As a result, the truck circulation through the entire downtown, especially along River Drive, must be re-evaluated to best balance the competing transportation interests along these district roadways.

2. Promote Traffic Calming

In circumstances, such as along River Drive where adjacent land uses generate truck, automobile, pedestrian and bike traffic, consider traffic calming measures to create safe streets for all modes.

3. Potential Two-way Conversion of IL-92

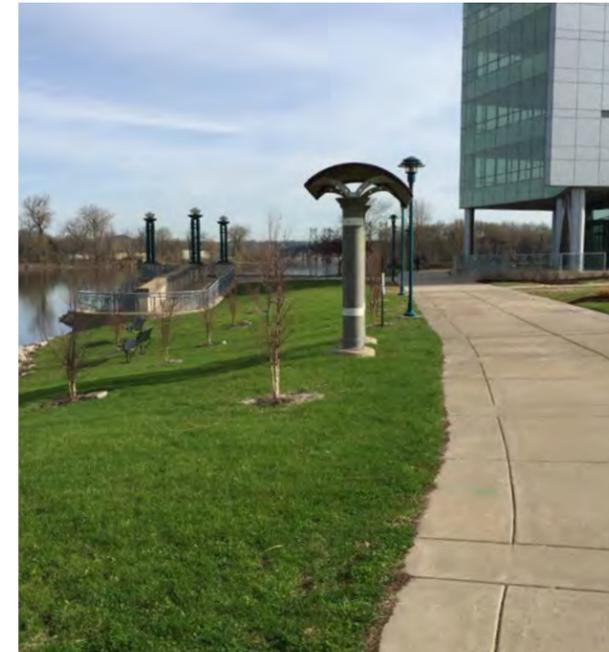
One-way couplets such as IL-92 were originally designed to move large volumes of traffic into and out-of the central business district. Today the traffic volumes are much less and the roadway design encourages speeding through residential neighborhoods. As the neighboring municipality of Rock Island considers converting IL-92 to a series of two way streets. The Florencia neighborhood is a logical location and next step for testing the two-way conversion alternative in Moline.

4. Link Downtown Moline Centre and Edgewater via a Multi-Use Path

With the introduction of the new WIU-QC Campus in Edgewater and emerging urban residential developments in Moline Centre, there is a student and new younger population seeking transportation alternatives. Many of the WIU-QC students seek housing in downtown, creating increasing demand for improved transportation linkages between the two areas. Creating a multi-use path connecting these communities will provide a significant amenity to support the emerging demographic trends and development, adding value to both WIU-QC and downtown.

5. Promote Asset Connectivity

Provide multi-modal alternatives to link local attractions and assets such as hotels, retail, restaurants, civic uses, the university, The Q Multi-modal Station and entertainment venues. Not only a destination, The Q plays a key role in connecting Moline's assets.



Great River Trail in Moline Centre with Channel Cat dock



Traffic Calming Example: Protected pedestrian crossing with median



5th Avenue and 15th Street



6th Avenue in Moline Center: Wide roads with fast moving traffic can negatively impact shops in downtown

2.2 TRANSPORTATION APPROACH



6. Expand the Pedestrian and Bike Network throughout Moline Centre and the Neighborhoods

Improve the pedestrian environment through enhanced streetscape, sidewalk maintenance, in-fill of sidewalk gaps, as well as signal and crosswalk improvements. Enhance the existing bike network to enable safer bike routes, and lessen barriers to bicyclists through the expansion of marked on-street bike paths.

7. Increase Riverfront Accessibility

The proximity of Moline Centre to the Mississippi River waterfront is a significant strategic advantage that Moline is not fully taking advantage of. There are opportunities to raise awareness of this community asset by strengthening the access to the riverfront through streetscape, enhanced development and wayfinding, especially along the north/south streets. Additional clusters of residential and commercial development along these strategic streets can increase synergies between the traditional Downtown Core of 5th Avenue and the riverfront developments such as Bass Landing, John Deere Pavilion and iWireless Center and Great River Trail, etc. This connecting of places will add to the vitality of the downtown and encourage increased pedestrian and bike activity.

8. Support Transit Ridership

Moline has an extensive bus transit network. The bus stops and shelters are visible in Moline Centre but less recognizable in the neighborhoods. Continue to

encourage ridership through linking bus routes with community needs and enhancing bus shelters and pedestrian routes.

9. Enhance Moline Centre’s Western Gateway at “The Q” Multi-modal Station

Enhance the development of a western gateway surrounding The Q multi-modal station. Focus on transit-oriented development (TOD), emphasizing highest density of development around this key transit hub.

10. Improve Freight Rail Crossings

Where possible improve the rail road crossings to increase safety for all modes. Continue to investigate quiet zone improvements for Moline Centre and potential funding options.

11. Improve Wayfinding and Legibility in Moline Centre

Supplement the existing Moline wayfinding system to provide clarity as to the location of downtown gateways and sub-districts with new information regarding access to activity zones such as the 5th Avenue commercial corridor, western gateway and multi-modal station area.

12. Support the growth of Moline Centre local retail centers by improving access to parking

Clearly mark all public parking alternatives with recognizable and consistent signage. Simplify parking regulations and hours of operations for ease of use.



Moline Centre Wayfinding Signage



Great River Trail near Sylvan Island Bridge



Bus Stop in Moline Centre



Biker at the Great River Trail



Railroad Crossing at 12th Street



Parking Signage in Moline Centre

2.3 TRUCK CIRCULATION

Industry plays a large role in Moline’s economy. Facilitating the mobility and accessibility of trucks is necessary to the success of these industrial businesses. River Drive currently plays an important role facilitating the movement of trucks in the larger street network. Truck trips originate from major industrial anchors in East Moline and the industrial hub west of 12th Street in Floreciente. Truck drivers prefer River Drive over other east-west streets in the area because it has wide lanes, few intersections, and provides a direct connection to I-74.

A Truck Circulation Strategy was developed to support prioritization of truck-focused enhancements necessary to support the City’s larger transportation and development goals including a suite of multi-modal improvements. This Plan balances improvements for pedestrians along River Drive in Moline Center to ensure its continued use to truck and vehicle circulation for the riverfront industry and iWireless Center with the increasing need for multi-modal connections.

Key Recommendations:

- Throughout all three neighborhoods, key intersections must be designed to clearly identify and separate pedestrian and truck traffic.
- In Moline Centre, consolidate truck traffic to River Drive, freeing up downtown streets for pedestrian oriented uses, and reducing congestion around the future Multi-Modal Station.
- In Floreciente, support pedestrian and bike activity in the residential neighborhoods along 4th and 5th

Avenues by promoting River Drive as a high-quality east-west alternative route for large vehicles.

- In Edgewater, encourage trucks to use IL-92 (4th Avenue) as an alternative to River Drive. Allow River Drive to become a neighborhood street that serves the adjacent growing residential, educational, and retail land uses.

In the context of the future street network in Moline Center and Floreciente, River Drive will play an increasing important role in the flow of trucks and larger vehicles through downtown. The Q Multi-Modal Station’s design includes mid-block crossings, on-street parking, and passenger loading and unloading making the passage of trucks more challenging along current routes. In addition, the potential conversion of IL-92 to two way traffic (see Section 2.4) in Floreciente will make River Drive a more attractive option for truck circulation.

Improvements to key streets and intersections may need to be made to ensure trucks can traverse the riverfront neighborhoods on the new, preferred network of streets for truck circulation. The Truck Circulation Strategy further describes the interplay between the prioritization of truck-focused enhancements and pedestrian improvements. Most of the roadway geometry improvements for trucks focus on Edgewater, as this is the area where the proposed preferred truck circulation differs most from the existing circulation patterns observed.

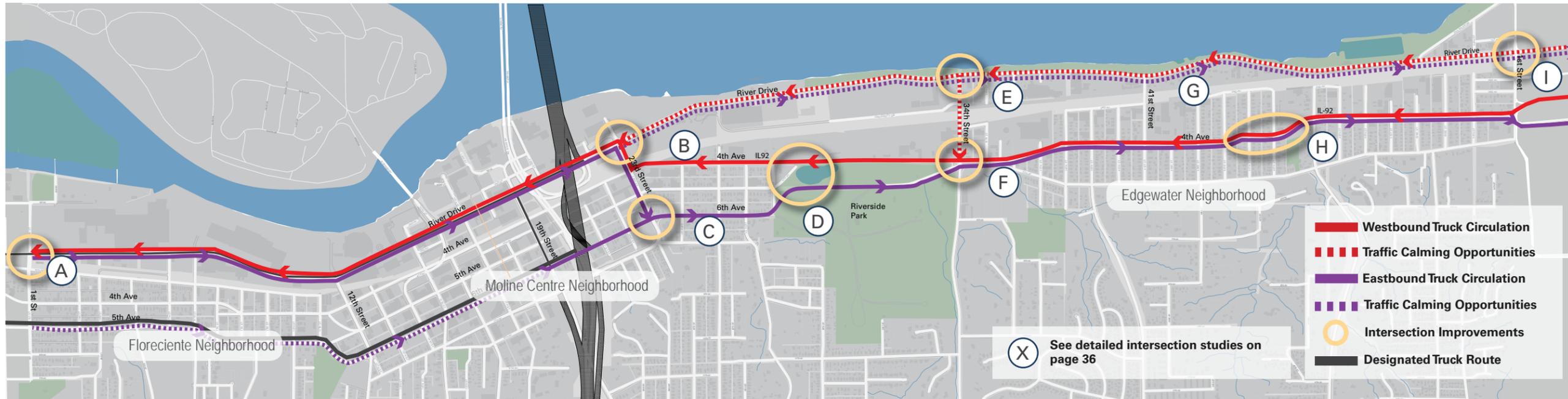


River Drive at WIU: Trucks are a constant present along River Drive



River Drive in Moline Centre: Trucks along River Drive pass through Moline Centre

2.3 TRUCK CIRCULATION



Truck Circulation Strategy Map with Key Intersections. See detail intersection studies on page 36.

River Drive is a logical street for trucks within the context of the existing network. It provides direct access to I-74, its width supports large trucks, and it provides direct connection to industrial land use. With the additional pedestrian, bicycle, and vehicular activity likely to be generated by the development of the Multi-Modal Station at 4th Avenue and 12th Street, 4th Avenue will naturally become a less desired route for trucks.

Along with designing River Drive to support truck circulation, pedestrian crossing improvements should be a priority between 12th Street and 19th Street.

Moline should consider improvements at the following intersections of River Drive:

- Key intersections: 12th Street, 15th Street, 17th Street, 19th Street, 23rd Street
- Leading Pedestrian Intervals should be installed at the intersections of River Drive and 12th Street, and 15th Street.
- Install signage alerting drivers to increased pedestrian activity approaching the Multi-Modal Station on 4th Avenue.
- Install additional lighting increase visibility of pedestrians at night at 15th Street and River Drive, and between 15th Street and 12th Street on 4th Avenue.
- Lane widths should be maintained on River Drive to support the ease of truck circulation through

the area, and to ensure a superior alternative to 4th Avenue in the context of the shifting modal hierarchy of streets downtown.

19th Street and River Drive will remain primary key connection routes between I-74 and Arsenal Island. In addition to River Drive, the design of 19th Street should be further studied to ensure adequate accommodations for trucks, cars, retailers, bikers and pedestrians alike. The character of 19th Street is further discussed in Chapter 3.0.

Currently the IL-92 one-way couplet begins 34th Street. As an alternative to the truck circulation diagram shown on this page, if 4th Avenue were converted to two-way traffic from 23rd Street to 34th



Arsenal Island to I-74 connections



Alternative truck circulation strategy through Riverside Park



5th Avenue through Riverside Park



Traffic on River Drive by the WIU-QC Campus

Street then both east-bound and west-bound truck traffic could be routed along this segment. This could have some advantages as it would move trucks out of Riverside Park and align the route with adjacent industrial land uses. This alternative is worthy of further engineering studies. At the time of this study, existing roadway configurations between 19th Street and 23rd Street are to remain as-is due to constraints imposed by the design and construction of the new I-74 bridge.

River Drive east of 23rd Street has evolved significantly over the last 5 to 10 years with the new WIU-QC campus and the growing residential mixed-use development at River Bend as well as additional residential development. These changing land uses

have brought an increased presence of pedestrians, bicyclists, and local vehicular traffic. Because of this important role River Drive plays with a variety of diverse stakeholder groups, it is important to reduce the impact truck traffic has on all user groups. Improvements to IL-92 to support truck circulation and calm traffic calming strategy implemented along the River Drive corridor need to be well coordinated. The approach outlined here furthers the 2009 River Drive Corridor Plan and the 2014 Comprehensive Plan Update and initiates a strategic plan for the IL-92 Corridor (see Section 2.4) in support of other planning initiatives in the area.

Moline should consider the following over arching ideas as they pertain to circulation improvement in along River Drive through Edgewater and the WIU-QC district:

- Maintain existing geometry of River Drive intersections where possible, keeping pedestrian safety as a priority while accommodating truck turns off of River Drive onto IL-92 in Edgewater.
- River Drive traffic calming strategies should encourage trucks to slow down considerably, without precluding their ability to use roadway.
- Extend River Drive traffic calming from 23rd Street to the eastern boarder of Moline, encouraging trucks to move to the preferred routes off of river Drive through out the entire Riverfront.

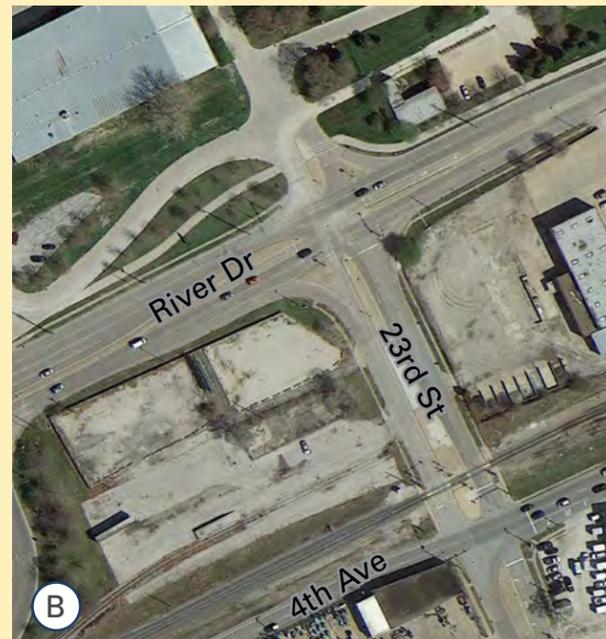
In order to ensure large trucks can circulate west-bound from River Drive it is recommended that Moline work with Rock Island to explore the redesign of intersection at 44th Street (in Rock Island) and 4th Avenue as well as the connections from River Drive to 5th Avenue or 6th Avenue at 44th Street. These intersections require geometric modifications with larger curb radii to ensure trucks can make safe turning movements. Additional pedestrian improvements are needed at 1st Street.

2.3 TRUCK CIRCULATION



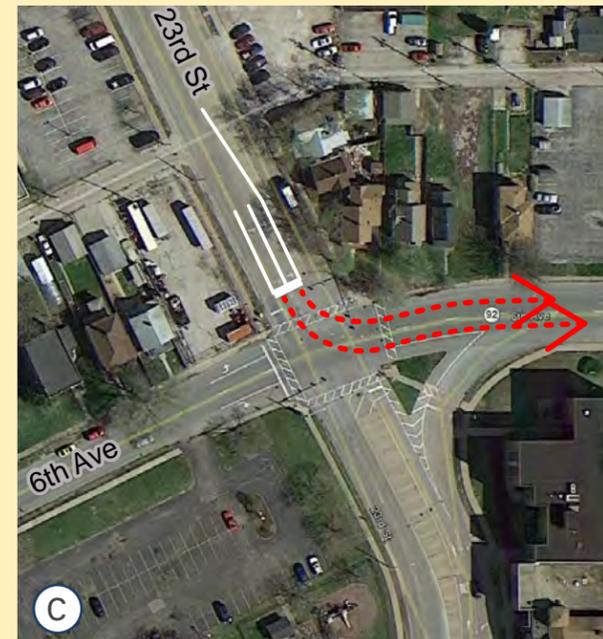
River Drive at 44th Street in Rock Island

- Consider reconstruction of the western end of River Drive and its connection to 6th Avenue in Floreciente. Reconstruction of the street segment and intersection at 6th Avenue may be required to ensure a safe connection to IL-92 for larger vehicles.



River Drive / 4th Avenue (IL-92) and 23rd Street

- 23rd Street provides an easy transition between 4th Avenue and 6th Avenue from River Drive in Moline Center.
- 23rd Street provides access from 4th Avenue and 6th Avenue in Edgewater, to north bound I-74.
- Current intersections are designed to accommodate large truck turns.
- Intersection improvements should coincide with the implementation of the 4th Avenue Multi-Use Path and River Drive Traffic Calming Pilot.
- Consider signage to encourage eastbound trucks to move off of River Drive onto 6th Avenue at this intersection.



6th Avenue (IL-92) and 23rd Street

- Southbound vehicles currently have two dedicated left-turn lanes.
- Further study should be undertaken to determine if both turn lanes are justified by turning vehicle volumes. If not, a single, wider left turn lane would allow larger vehicles an easier path to turn.



S-curve transition from 5th Avenue to 6th Avenue (IL-92) at 27th Street

- Existing conditions support large vehicle turning movements. Additional signage and speed enforcement may improve conditions for all vehicles making this turn. It is recommended that the City conduct speed tests for vehicles approaching this turn.
- The installation of a signal at 26th Street should be considered if results of speed test show significant speeding, particularly by trucks approaching this curve.



River Drive at 34th Street

- Provide a protected left turn signal for trucks turning from westbound River Drive to southbound 34th Street to continue westbound on 4th Avenue.
- Utilize signage to encourage trucks to continue westbound on 4th Avenue at this intersection.
- If determined necessary by the design of River Drive traffic calming measures, consider widening 34th Street to allow large truck turning movements from River Drive on 34th Street. Alternatively, encourage the largest trucks traveling westbound on River Drive to connect to 4th Avenue before this intersection.



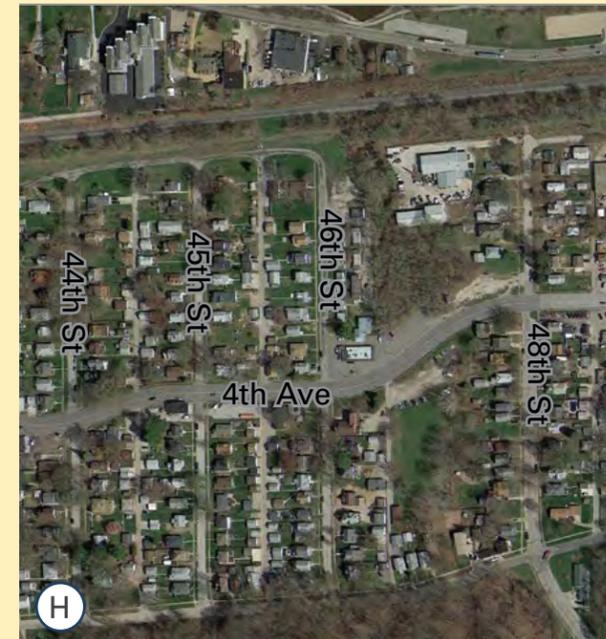
4th Avenue (IL-92) at 34th Street

- Enhance pedestrian connection in all directions.
- Study the need to increase radius of the northwest corner to improve conditions for turning trucks.
- Consider protected right-turn signal from southbound 34th Street to westbound 4th Avenue.



River Drive at 41st Street

- 41st Street intersection design, adjacent developments, and close proximity to rail grade crossing does not easily adapt itself to large truck turns or circulation. Trucks should be encouraged to turn onto 4th Avenue at 55th Street or 34th Street.



4th Avenue (IL-92) S-Curve between 44th Street and 48th Street

- The s-curve poses challenges to truck circulation. High-level review suggest three potential improvement alternatives:
 1. Widening and reconstruction of 4th Avenue to reduce the curve between 44th St. and 48th Street.
 2. Reduce 4th Avenue to three lanes approaching this curve. Two-lanes should favor the direction with more traffic.
 3. Reduce 4th Avenue to one lane in each direction with a striped buffer.
- Alternatives should be further studied to minimize the negative impact of physical changes on the level of service and adjacent land uses.



River Drive/12th Avenue (E. Moline) at 55th Street/1st Street (E. Moline)

- 55th Street/1st Street in East Moline provides easy connection for large vehicles moving westbound from River Drive to IL-92. The existing intersection geometry at 55th Street supports the turning movements of large vehicles, in all directions. Once IL-92 modifications are complete, signage should direct drivers to use IL-92 to access I-74.
- No major challenges for trucks turning from 55th Street to westbound 4th Avenue. Consider a protected right-turn signal with extended time for turning vehicles. Trucks traveling eastbound on 4th Avenue would need to continue on to 7th Street in East Moline to make a safe connection to 12th Ave / River Drive.

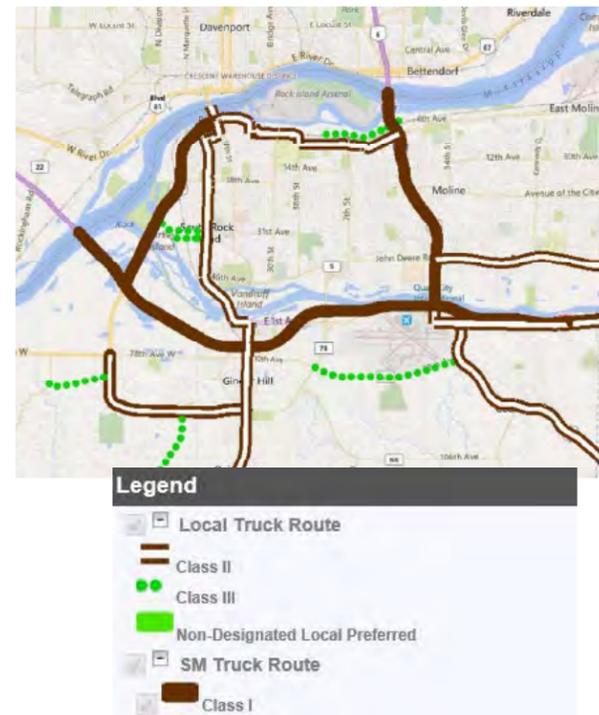


Industrial Land Use and Truck Access Points

Policy and Design Guidance

The following steps can be taken by Moline as a path towards implementation:

- Establish a Preferred Truck Circulation strategy connecting industry anchors to each other, and to larger regional connections.
- Refer to Truck Circulation Map in all streetscape decision-making.
- Ensure trucks have access to local industry via existing access points wherever possible.
- Utilize the largest expected vehicle in turning movement and lane design along the preferred truck circulation route.
- Provide protected left-turn signals at key intersections along preferred truck circulation.
- Engage industry stakeholders in development decisions as street uses shift to ensure daily operations continue without disruption.
- Engage industry stakeholders in the study of IL-92 and engineering of key intersections to promote preferred use.
- Distribute maps of the preferred truck route alternative to industry stakeholders in the area.
- Install signage directing truck drivers to the preferred route on IL-92 once upgrades are complete.
- Driver training and education should be conducted to ensure drivers are aware of the preferred use of IL-92 to River Drive through Edgewater once upgrades to street and key intersection geometries are complete.



IDOT Designated Truck Routes

Truck Route Designation

IDOT designates certain roadways as having sufficient size, geometry, weight-bearing capacity and context appropriateness for trucks to use when connecting between highways and interstates. Designations can be revisited or added under the purview of IDOT, but would require study to ensure that the roadway meets the physical specifications as required by the State. IL-92 east of I-74 is not currently a State Designated truck Route, but the City of Moline could initiate conversations with IDOT to understand if it may be a candidate in the future. If designated, this would support the use of IL-92, rather than River Drive, as the main truck connection through Edgewater and near the WIU-QC campus through policy. Counties and municipalities can also designate their own local access rules which govern which vehicles can use certain streets by size and weight limit. Both River Drive between 1st and I-74, and 19th Street between River Drive and 6th Ave are locally designated and maintained truck routes. As construction of I-74 is completed and in the context of other uses proposed for local streets – the City of Moline should consider limiting larger vehicle access to more context appropriate local streets in light of preferred development goals. Policy changes such as weight limit restrictions on local streets may help reinforce new uses as proposed in this Plan. Local industrial stakeholders and IDOT should be an integral part of this discussion.

2.4 RIVER DRIVE TRAFFIC CALMING

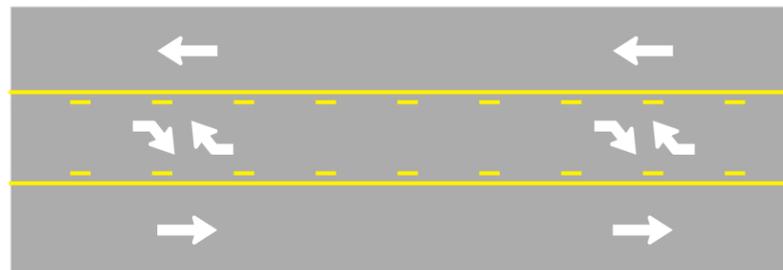


Traffic calming strategy East of 34th Street to be determined by pilot design and performance.

Two-Way Turn Lane

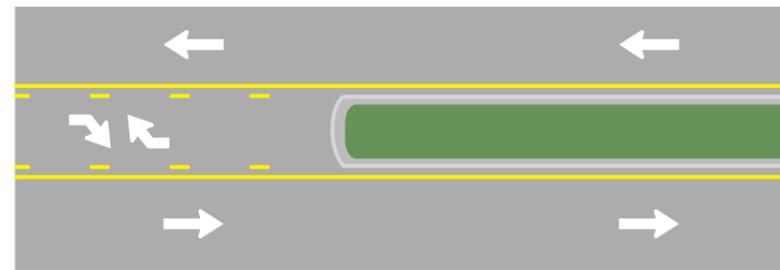
Protected Crossing / Extended Median

Phase I



Short Term: Two-Way Turn Lane

Phase II

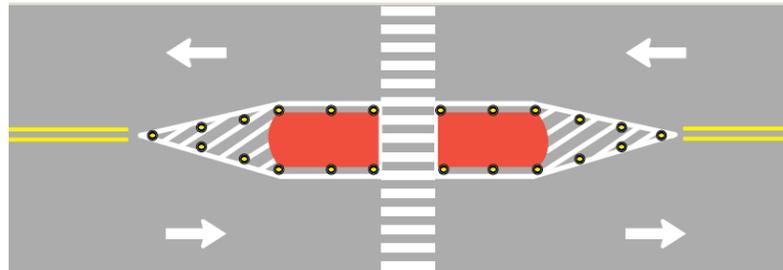


Long Term: Curbed/Planted Median



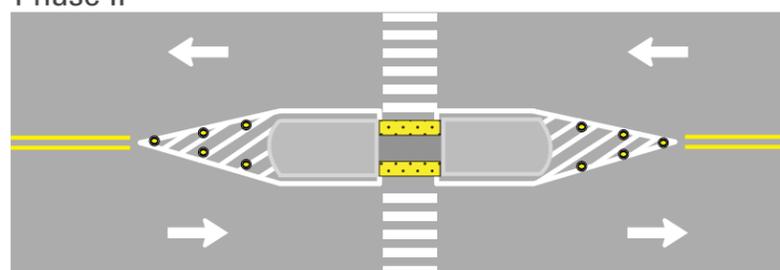
Two-Way Turn Lane Example

Phase I



Short Term: Paint and Bollard Protected Pedestrian Crossing

Phase II



Long Term: Protected Pedestrian Crossing



Protected Pedestrian Crossing Example

2.4 RIVER DRIVE TRAFFIC CALMING



River Drive Traffic Calming and Pedestrian Improvements

The influence of the changing land uses along River Drive through Edgewater requires that River Drive be able to accommodate vehicles of all types, local traffic, and pedestrians. As outlined in Section 2.3 there are alternative routes for trucks, allowing pedestrians and local traffic to take priority.

Phase 1: WIU-QC Campus Segment

This Comprehensive Plan Update is intended to reinforce previous planning efforts and furthers several specific goals and recommendations of The River Drive Corridor Plan (2009). By focusing on connectivity along the corridor, this design helps to position River Drive as a 'seam' rather than an 'edge' along the Campus Segment.

The first catalytic project is to pursue a low-speed, high amenity experience that extends and connects the WIU-QC campus to the Great River Trail and the Mississippi River. This concept shares similarities to the streetscape described in the 2009 Plan, but works within the existing right-of-way. Changes to this segment of River Drive must be part of a comprehensive approach to circulation in the area, and should be supported by physical improvements to IL-92 which ensure a high-quality path of travel for large trucks which currently rely on River Drive to access I-74 and riverfront industry.

Existing Connectivity

The WIU-QC campus currently has one mid block pedestrian crossing across River Drive with Rectangular Rapid Flash Beacons (RRFB) signs installed east of the 34th Street intersection. University Drive - a circulation road within the WIU-QC Campus - connects to River Drive from the south along the rear of the campus. MetroLINK's WIU-QC bus route, "The Connect," makes continuous trips between WIU-QC Campus at 34th Street and Centre Station.

The parking lots along the River provide parking for uses on both the north and south sides of River Drive. These parking lots have seven curb openings for vehicle entrances. Vehicles must turn into and out of the parking lots from a single through lane in each direction. Although turning vehicles may temporarily slow traffic, they also have the potential to increase the incidents of rear-end vehicle crashes.

Along the Campus Segment of River Drive, the Great River Trail runs parallel to the street, separated by a curb and grass buffer with no landscaping. The speed of vehicles along this segment and its use as a preferred truck route through the area negatively impact the environment around the trail. To build a more fluid connection between the WIU-QC Campus and the Mississippi River, traffic along the Campus Segment of River Drive must be slowed and the opportunities for safe crossings must be improved.

Concept and Description

The typical existing section of River Drive is 30 feet wide with one 15 feet vehicle travel lane in each direction. The proposed traffic calming concept narrows the vehicle through lanes to 10 feet each, opening up a central space to be used as a central median, two-way turn lane, and protected pedestrian refuge island interchangeable along the corridor. The concept design shows recommended locations for pedestrian crossings aligned with the existing WIU-QC sidewalk network to the south. The intent of the physical intervention on River Drive is to slow traffic. Narrowing travel lanes has been shown to be effective in slowing vehicles through an area.

A typical pedestrian refuge island must be a minimum of 6 feet width to meet typical design standards (FHWA, AASHTO, NACTO). Concept designs shown here extend the protected space to 8 feet, with an additional 1 foot buffer between the curb and outside line. This additional buffer maintains a continuous 10 feet vehicle travel lane along the length of the corridor. This makes the experience for the driver more predictable.

The two-way turn lane design offers protection for mid-block left turning vehicles and provides space for left turn lanes at intersections. Two-way turn medians are desirable for areas where mid-block turns are high. The 10 feet width two-way turn lane further encourages drivers to pull completely into the two-way

turn lane, discouraging drivers from placing their vehicles in an angular turning position. From wider two-way turn lanes, drivers tend to place their vehicles partially turned (not parallel to the pavement marking), which can cause the cars to encroach or spill into the through lane on the opposite side. Additional rationale for choosing a two-way turn lane design can be found in the Bureau of Design and Environmental Manual (BDE). General striping requirements for the concept shown here can be found in the Manual on Uniform Traffic Control Devices (MUTCD).

Signage for the reduced lane widths and traffic calming should begin far in advance of this segment in both directions to ensure trucks have time to turn off River Drive to use the preferred truck routes.

The Pilot Approach

It is recommended that the City take on a phased approach, or "Pilot Project" approach, when implementing this project. Pilot Projects are meant to test a new physical design over a set period of time to determine if a permanent installation of a new type of project will be a success. Pilot projects also initiate a period of adjustment for users while a larger permanent project is developed. The staged implementation approach also allows the City to test alternatives before extending traffic calming measures East of 34th Street on River Drive.

2.4 RIVER DRIVE TRAFFIC CALMING

Adjustment Period: Test narrowed lanes using traffic cones and signage for a period of 1-2 months. Observe the impacts vehicle speeds, damaged cones, changes to pedestrian crossing activity, etc. Establish a means for the public to submit feedback.

Pilot Phase: Implement the Paint and Bollard Strategy. Stripe central median or two-way turn lane space with pedestrian refuge islands at predetermined locations along the Campus Segment. Install temporary bollards protecting the pedestrian refuge islands to increase the visibility of the new design to drivers. Install all signs alerting vehicles to the changing roadway geometry and presence of pedestrian crossings. Maintain this Paint and Bollard phase for a predetermined period. Evaluate the success and make necessary design changes.

Final Phase: Install raised curb-protected pedestrian crossings.

Subsequent Phases: Extend through Edgewater east to city boundary with East Moline.

Operational Recommendations

- Establish a schedule of observations to document performance of the pilot design, or improvements for the final phase.
- To support truck traffic on IL-92, initiate engineering study of the s-curve at 46th Street, 23rd Street and 34th Street intersections to begin work toward modification to support permanent traffic calming design on River Drive.

Additional Recommendations

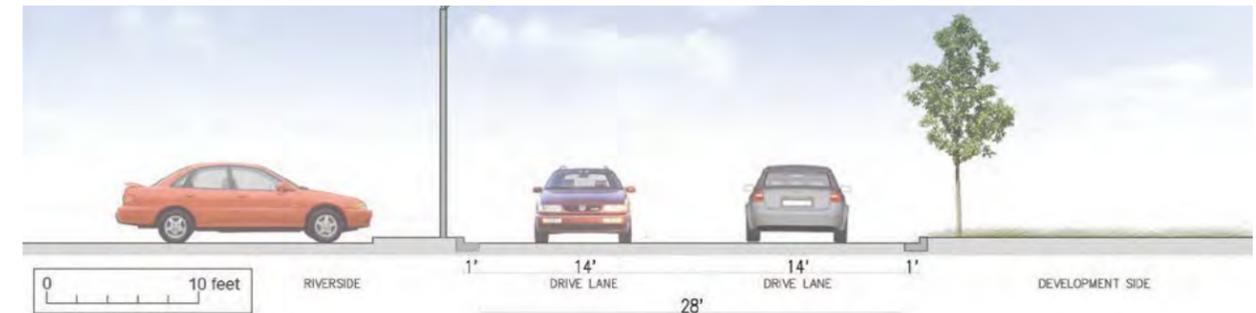
This intersection of River Drive and 34th Street serves as the gateway into the Campus Segment. Visual cues such as signage and landscaping should be used to alert drivers to the changing context including:

- High visibility crossings installed in all directions.
- Signs alerting drivers to pedestrian crosswalks, or install "Stop Here for Pedestrians" in advance of each protected crossing along the segment.
- Reduce lane width using a painted buffer on the north side of the westbound lane.

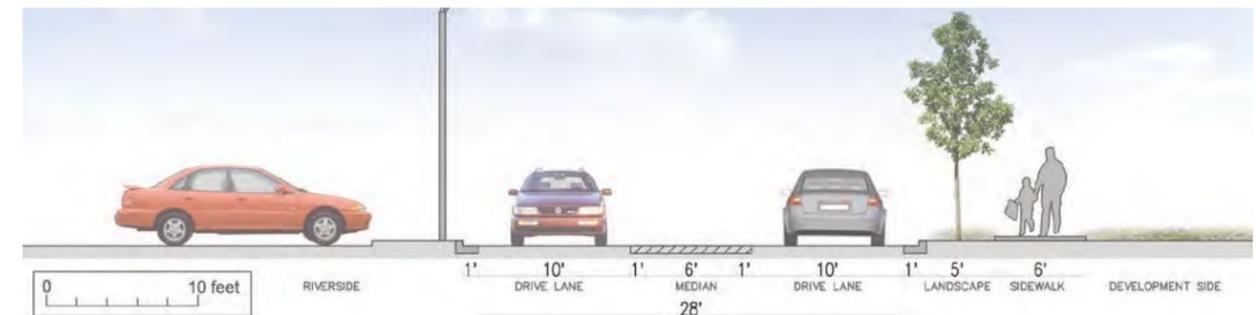
A design with a contiguous median allows for the same crossing design to be used at any point along the corridor. Modifications require trade-offs that lessen benefits for other users of this street. Modifications could include reducing the central median to 8 feet and reducing the protected pedestrian crossings to the minimum of 6 feet. At 8 feet, the central median space would not support a two-way turning lane; the central space would be hatched, visually narrowing the lanes with no vertical impediments to passing vehicles. This modification would allow for 11 feet driving lanes.

Policy and Design Guidance

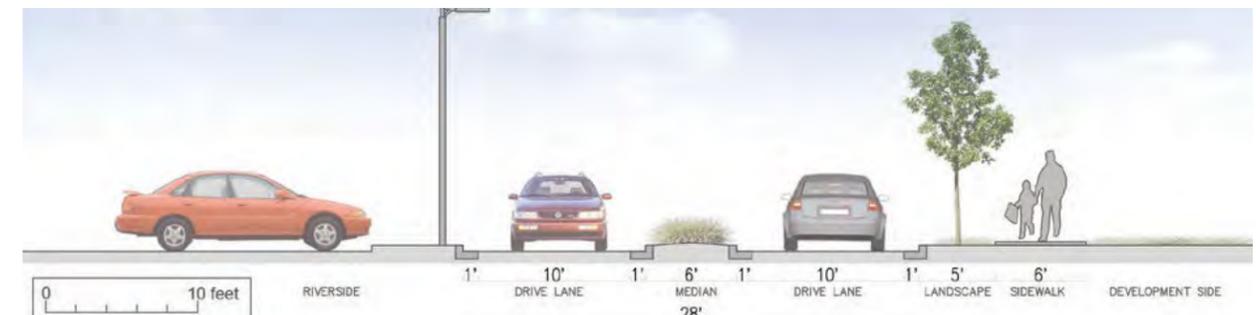
This segment of River Drive is neither a state-owned roadway, nor a IDOT designated truck route. This allows the City more flexibility in design decisions with fewer limits on elements such as lane widths. However, the physical changes in the street section must be balanced against the requirements of industry stakeholders.



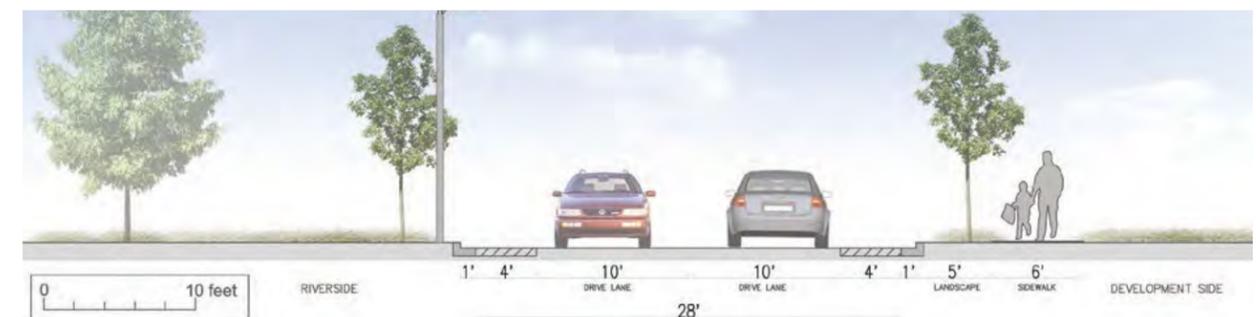
River Drive Existing Conditions. Typical dimensions, actual dimension may vary.



Pilot Phase, River Drive Proposed Section with Central Stripe Divider / Turn Lane. Dimensions are approximate.



Final Phase: Proposed Section Central Median. Dimensions are approximate.

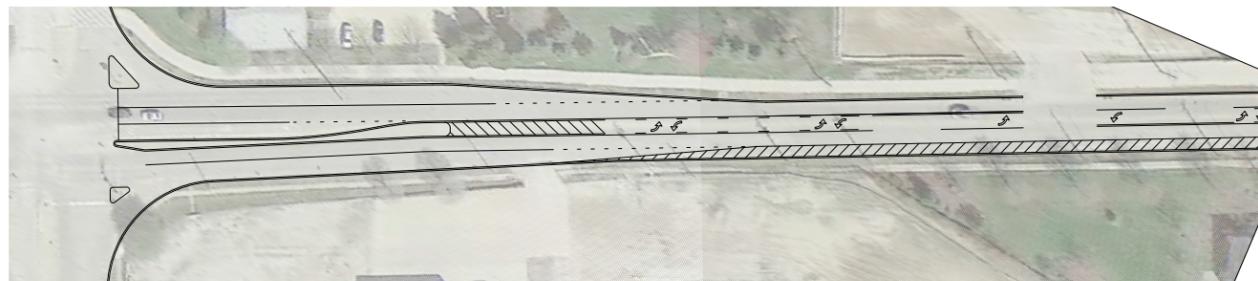


Proposed Section Narrowed Lanes

2.4 RIVER DRIVE TRAFFIC CALMING



River Drive Traffic Calming Key Plan



1. River Drive Traffic Calming



2. River Drive Traffic Calming



3. River Drive Traffic Calming



4. River Drive Traffic Calming

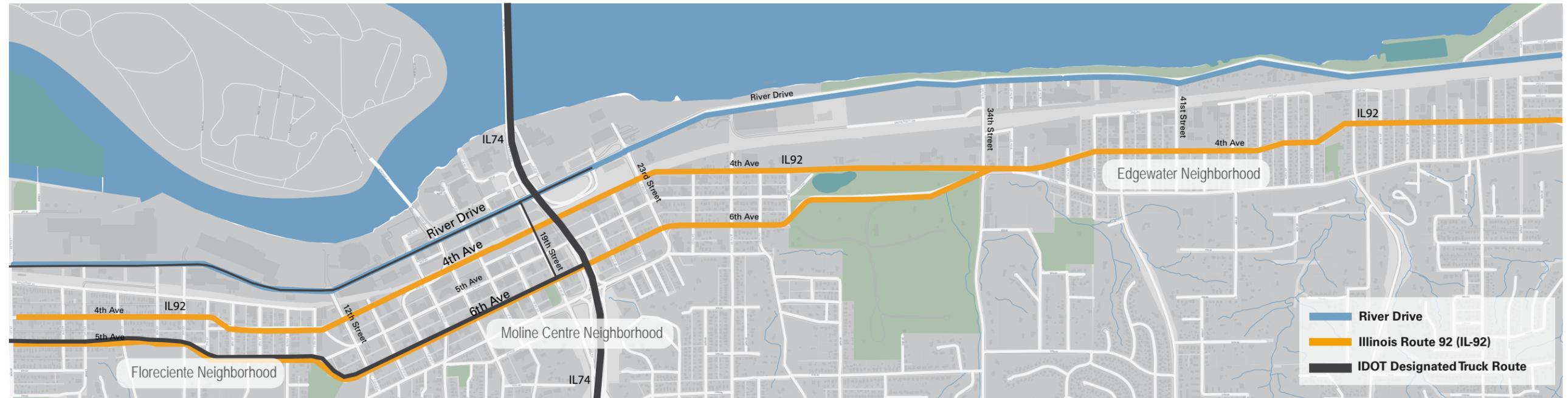


5. River Drive Traffic Calming



6. River Drive Traffic Calming

2.5 ILLINOIS ROUTE 92



Illinois 92, River Drive and IDOT Designated Truck Routes

Illinois Route 92

Illinois Route 92 (IL-92) is the primary east-west thoroughfare through Floreciente, Moline Centre and Edgewater with portions designated as a truck route under the jurisdiction of the Illinois Department of Transportation (IDOT). IL-92 changes in character and use as it transverses through the City. Each section has its unique needs but are presented here as a part of the wholistic network strategy.

IL-92 operates as a couplet, or pair of one-way roads, on 4th Avenue and 6th Avenue between the City's western municipal boundary and 34th Street. This one-way pair arrangement is designed to accommodate high volumes of traffic. According to

recent traffic volumes and analysis, IL-92 is designed to handle more traffic than it currently carries during most peak times of the day.

Key Recommendations

Throughout the planning process stakeholders voiced concerns that speeding occurs along segments of IL-92 and River Drive and traffic calming is needed. Especially along the IL-92 corridors there are many pedestrian zones. Traffic calming to lower speeds can be achieved by a variety of techniques including modifying signal timing, narrowing travel lanes through striping and curb extensions, as well as a one-way to two-way conversion of 4th and 6th Avenues.

- In Edgewater and the WIU-QC district, support truck traffic, improve pedestrian accommodations. Refer to Section 2.3 for how future truck circulation will influence IL-92 through Edgewater.
- In Moline Centre, recognize the residential and local retail adjacent land uses, strengthen pedestrian amenities by increasing sidewalks and narrowing roadway along 6th Ave. Encourage trucks to use River Drive. Adjust to changing uses based on design of new I-74 bridge. Introduce Multi-Use Path on 4th Ave.
- Support the growth of pedestrian and bike activity along 4th Avenue at The Q Multi-Modal Station.
- In Floreciente, recognize the residential nature of adjacent land uses. Slow down traffic, create pedestrian oriented streets, and consider returning 4th Avenue and 5th Avenue to two-way streets. Encourage trucks to use River Drive. Pedestrian needs along IL-92 should be at the forefront of all design strategies.
- Pedestrian and Bicycle considerations are addressed in Sections 2.8 and 2.9 and in the Urban Design Chapter 3.0. All modes of transportation should be coordinated with any roadway design modifications.

2.5 ILLINOIS ROUTE 92/ I-74 BRIDGE RECONSTRUCTION



The location of the new Interstate 74 ramps will have a major impact on the automobile and truck traffic traveling through Moline Centre, as well as limit pedestrian connections from one side of the bridge to the other. These impacts need to be considered in the future redevelopment of the bridge realignment zone and its connections with the urban street grid in Moline Centre and Edgewater. The new alignment of I-74 will further impact Moline by severing 5th Avenue.

The interconnection between I-74, IL-92 and River Drive has a strong influence on land use and Moline Centre development. With the recent residential development in Moline Centre and emergence of the WIU-QC campus, being able to walk or ride a bike under I-74 along a dedicated path preserves and enhances the overall connectivity of the transportation network in the context of major changes to vehicular traffic patterns.

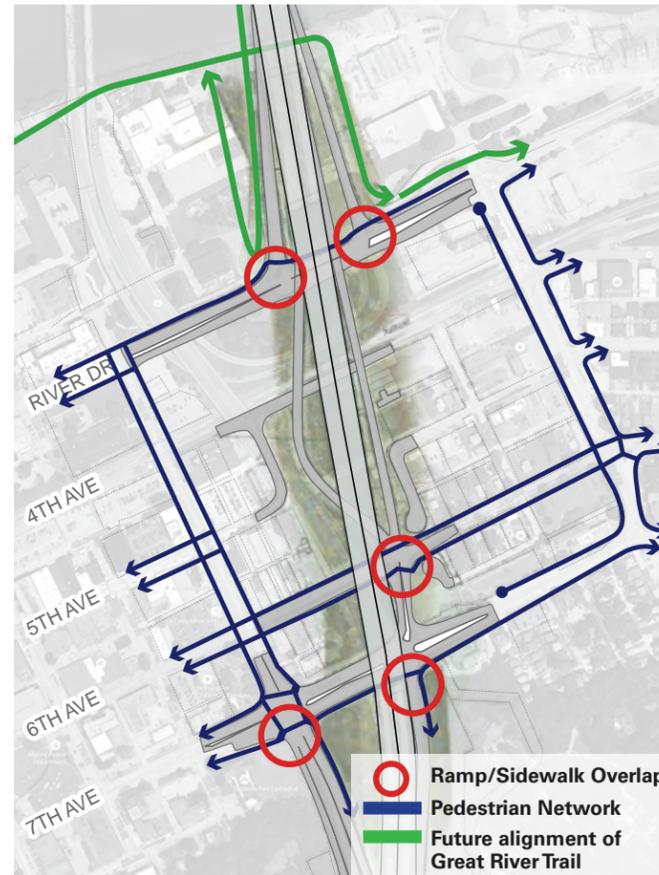
Pedestrian and bike connectivity is very limited between the Downtown Core and east of the new I-74 Bridge.

- Pedestrian circulation overlaps with on and off ramps for the I-74 Bridge. These intersections present inherent conflict between drivers and pedestrians and create “high stress” crossings.

- No sidewalks on the south side of River Drive under the I-74 Bridge.
- No sidewalks exist on 4th Avenue from 17 Street to 23rd Street.
- 5th Avenue is terminated at 19th Street.

The City of Moline can strengthen, reestablish, and create new pedestrian and bike connections for a more complete and efficient network.

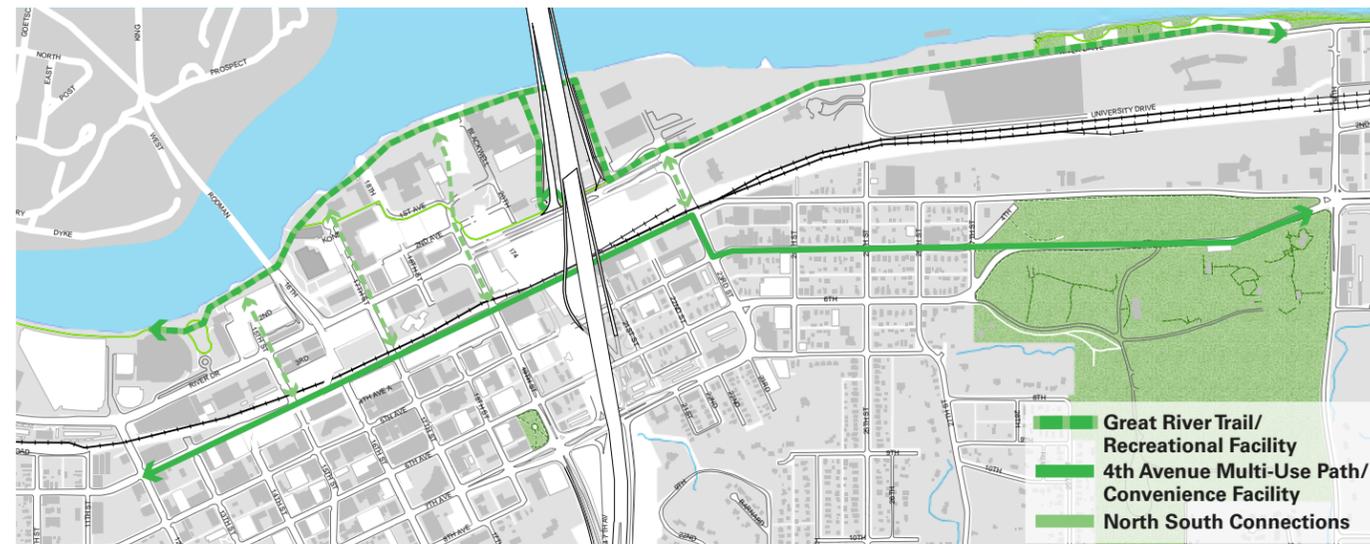
- Introduce a multi-use path as a protected, shared-use facility to increase pedestrian and bike connectivity.
- This multi-use path should be conceived as a “convenience” facility that strives to improve access and mobility throughout Moline Centre and into the neighborhoods.
- Locate path on 4th Avenue will provide direct connection under the new I-74 bridge, from 23rd Street to the Multi-Modal Station at 12th Street.
- 4th Avenue is the only opportunity to cross the new I-74 Bridge away from highway ramp intersections.
- Locating the multi-use path on 4th Avenue allows for an efficient, convenient connection throughout Moline Centre without turns or the need to cross the rail road tracks.
- 4th Avenue is an east-west connection within the heart of downtown providing direct to local destinations south of the rail line.



New I-74 Bridge: Future Bike/Pedestrian Circulation



New I-74 Bridge: Vehicle Circulation



Moline Centre Future Bike/Pedestrian Circulation

2.5 ILLINOIS ROUTE 92/ 4TH AVENUE MULTI-USE PATH

The construction of the new I-74 bridge will create challenges to the circulation of bicycles and pedestrians traveling east to west under the new bridge structure. 4th Avenue offers the only opportunity for a bike and pedestrian connection under the new I-74 Bridge, south of River Drive, that would not intersect with on- and off-ramps.

A multi-use path on 4th Avenue can create a vital neighborhood connection for pedestrians and bicyclists. This connection can improve the quality of the 4th Avenue streetscape for all modes, provide traffic calming as 4th Avenue approaches the new Multi-modal Station, and maintain a connection between 5th Avenue retail and the neighborhoods east of the bridge. This low-stress segment also provides connection between the WIU-QC and Edgewater residential areas, directly into the heart of downtown.

Input from stakeholders and local community members, as well as outreach from prior planning efforts, shows a demand for a more accessible downtown area for bikes. Growth in the bike mode share must be supported by a network of safe, convenient bike facilities, which connect people to the places they want to go.

There are two main types of bike facilities, recreational and convenience. These two types of facilities serve different users, and both types of facilities are necessary to create a great bike

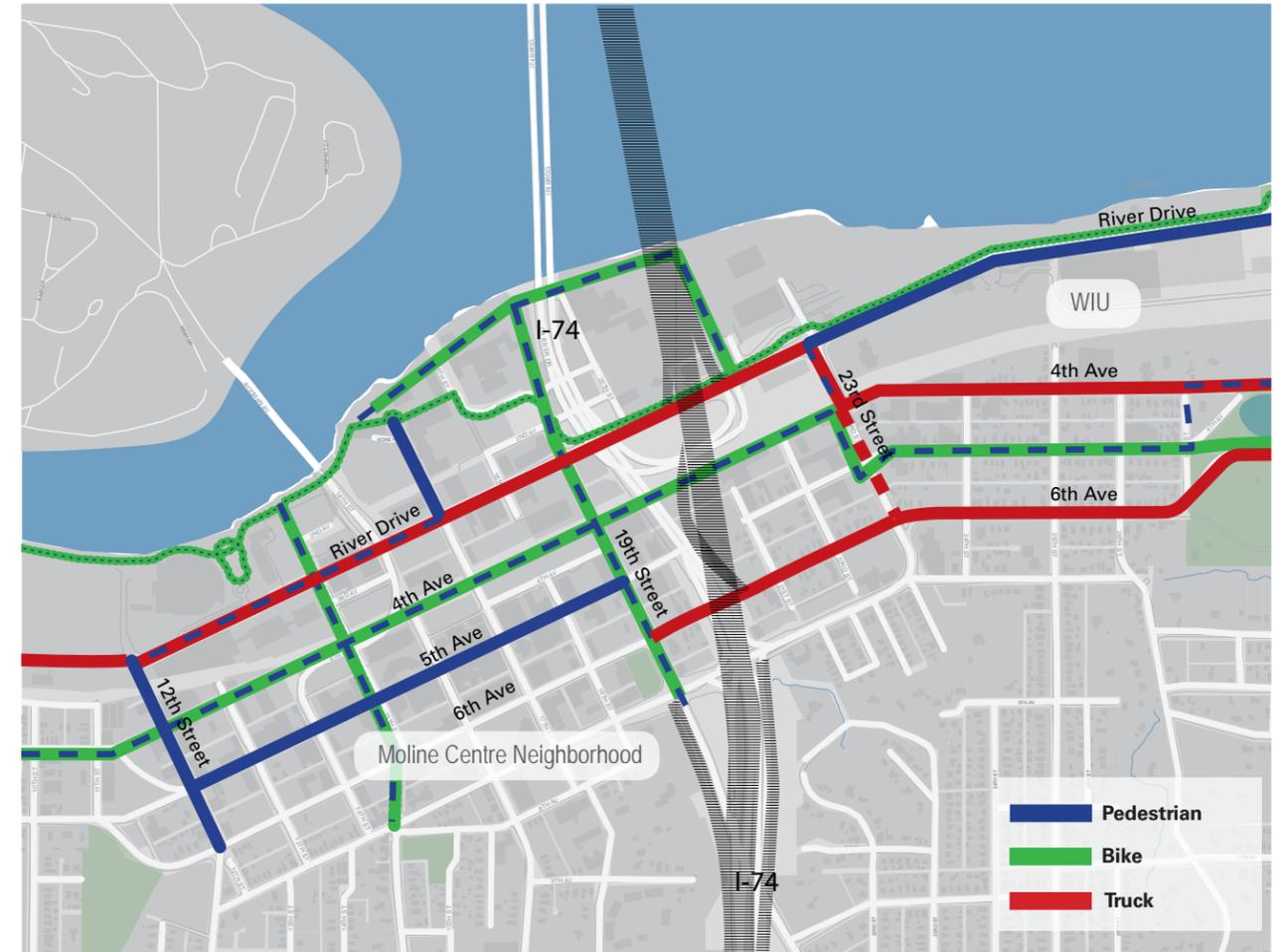
network. A multi-use path on 4th Avenue is the prime opportunity to establish a great convenience connection in downtown Moline.

- Intersection improvements should be designed to support pedestrian and bicycle safety and visibility wherever possible including high-visibility crosswalks, sidewalk bump-outs, and advanced stop bars. Pedestrian and bicycle facilities should be concentrated on the same side of the street, wherever possible, to minimize conflicts with turning vehicles.
- Pilot 4th Avenue Multi-Use Path Concept as part of an outdoor event staged on the parking lots fronting 4th Avenue.
- Initiate design of Great River Trail Connection along the River, under new I-74 Bridge.

4th Avenue Multi-Use Path Description

4th Ave, west of 23rd Street, is a one way street, with two westbound lanes and a dedicated left turn lane for the majority of the length.

Running along the north side of the roadway is the existing rail right of way, which includes an existing freight rail line and a future track for the High Speed Rail. Because of the uncertainty of the location of the future rail line, all strategies presented here do not encroach to the north of the existing shoulder into the rail line right of way.



I-74 Realignment Zone

4th Avenue Multi-Use Path

Proposed path provides as protected, shared-use facility for bicyclist and pedestrians along 4th Avenue connecting from 23rd Street, under the new I-74 bridge, to the Multi-Modal Station at 12th Street.

- 4th Avenue is the only opportunity to cross the new I-74 Bridge away from highway ramps.
- Provides an east-west connection within the heart of downtown providing a finer grain network connecting directly to local destinations.
- Efficient alignment limits the number of intersections and turns within the system.
- Also avoids the need to cross the rail road tracks

2.5 ILLINOIS ROUTE 92/ 4TH AVENUE MULTI-USE PATH



Two basic concept alternatives were developed for a 4th Avenue Multi-Use Path design: on-street and off-street. The on-street option shows a multi-use path completely within the existing street with exception of the 19th-23rd Street segment due to site constraints. The off-street option considers the opportunity to expand to the south to preserve the existing roadway configuration and / or to increase the horizontal separation between the path and the vehicles.

Unique elements of each of these concepts are described block-by-block along with the concept drawings. To be considered a standard multi-use path, the path must maintain a minimum of 10 feet width. Vertical signs and pavement markings are recommended to communicate the shared nature of the space and increase visibility of the project. (Per AASHTO's Guide for the Development of Bicycle Facilities and Bureau of Design and Environment Manual (BDE) Chapter 17.

The concepts for the 4th Avenue Multi-Use Path shown here represent two options in a much larger range of design alternatives that should be considered as the City moves forward with recommendations included in this Plan.

Typical On-Street Concept

The on-street concept shows a 10 foot wide multi-use path which predominantly replaces the existing dedicated left turn lane on 4th Avenue. The design

concepts show a "Jersey Barrier" type vertical separation from motorized traffic in the adjacent lanes. The path should continue through each intersection by striping a high-visibility crosswalk. Historic vehicle counts on streets crossing 4th Avenue north-south, including 19th Street, show a decrease in vehicles since 2001 counts conducted by IDOT. The impact of the loss of one a vehicle lane is not expected to significantly impact traffic operations and should be evaluated as the design concept is developed.

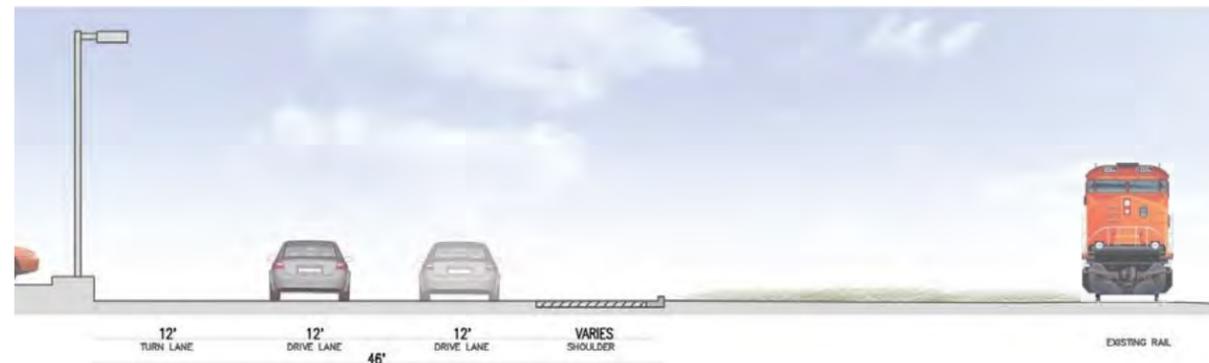
At 16th Street, it is recommended that the on-street concept transition off-street following the path of the existing sidewalk, as described in the off-street concept. The existing sidewalks should be widened to 10 feet to accommodate mixed pedestrians and bicycle activity along the existing path. Modified Crosswalks should be devised to facilitate smooth transition to sidewalk connection at 16th Street.

To increase visibility of a new on-street facility to drivers, consider the following additional improvements:

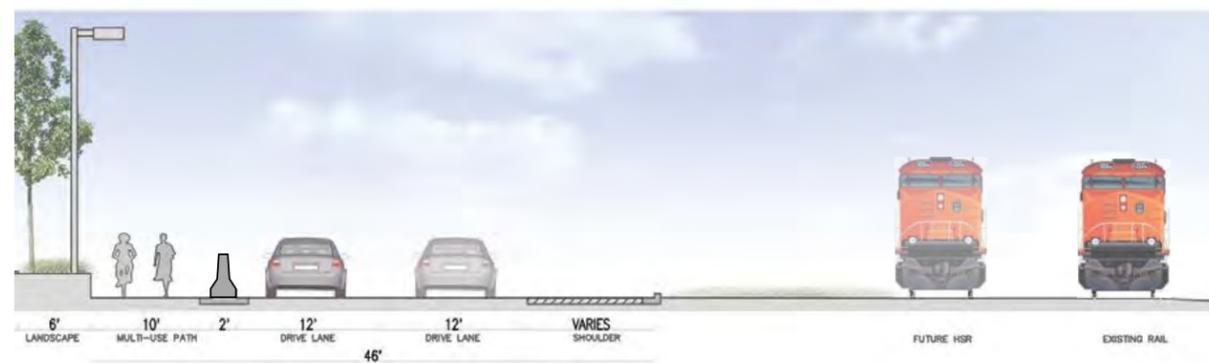
- At each intersection install a bollard at the center of the multi-use path blocking vehicle entrance.
- Make the path more visible using unique material, ground cover, or paint color, particularly near intersections.
- Use consistent vertical signage along the entire facility; landscape and install pedestrian scale lighting to highlight the new amenity.



4th Avenue Existing Conditions



4th Avenue: Existing Section



4th Avenue: Proposed - Multi Use Path

2.5 ILLINOIS ROUTE 92/ 4TH AVENUE MULTI-USE PATH

Off-Street Concept

The off-street multi-use path concept would follow the existing sidewalk between 15th and 17th Streets, expanding it to 10 feet to accommodate mixed modes. Along the north edge of the parking lots spanning between 17th Street and 19th Street, a new 10 foot path would be installed, and should feature a landscaped buffer to widest possible extent, allowing for full function of parking lots and ensuring parcels are viable depth (120 feet) for future development.

19th Street Constrained Edge

The southern curb of 4th Avenue, between 19th Street and 23rd Street, poses a challenge to the multi-use path design. The John Deere IT Center and permanent secondary structures, as well as a business at the intersection of 23rd Street, constrain the south edge of 4th Avenue along this block. Resolution of the multi-use path concept at this location requires further study.

Opportunities may exist as the design and construction of the I-74 Bridge and surrounding area are finalized. Integration of an off-street path connection should be explored as part of the new bridge's landscape strategy. Based on engineering and landscape concept drawings provided by IDOT, the setback of the pier structure of the new I-74 Bridge would accommodate an off-street path between the piers and existing street edge. Regardless of the physical loca-

tion, building redundancy in the bicycle and pedestrian networks is key to improving the transportation networks in Moline.

At the intersection of 4th Avenue and 19th Street, a left-turn only lane exists at the eastern approach to the intersection. Design alternatives to consider:

- Reduce the number of vehicle through lanes to allow for a wider protected path on-street.
- Reduce the on-street facility to separated bike lane (FHWA SBL Design Guidance); provide alternative sidewalk connection around rear of John Deere IT Center
- Further reduce the lane width to accommodate a wider on-street path

Protect two-way bikeways located less than 5 feet from the traveled way (generally, the face of the curb) with a 3.5 feet high barrier. Such barriers serve both to prevent bicyclists from making undesirable movements between the path and the highway shoulder and to reinforce the concept that the bicycle path is an independent facility.

Concept Flexibility

These concepts could alternate block-by-block, or elements of these designs could be taken in part and combined with other strategies to achieve a similar goal of connecting bicycle and pedestrian facilities along 4th Avenue.



Bike Lane On-Street with Concrete Barrier (Seattle, WA)



Cultural Trail (Indianapolis, IN)



Skunk Creek Path (Greenville, NC)

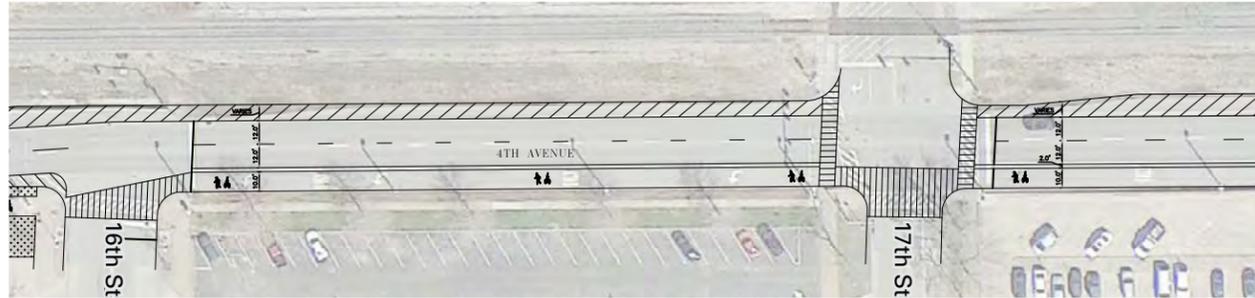


Duluth Lakewalk (St. Louis, MN)

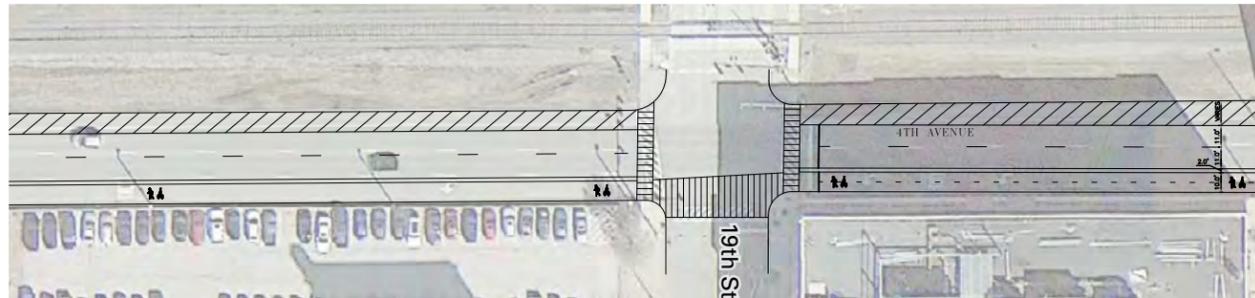
What is a "Multi-Use Path?"

A Multi-Use Path, similar to a Bicycle Path, Shared-Use Trail, and Side Path, is a facility physically separated from the roadway and intended for bicycle or other non-motorized transportation (e.g., pedestrians, disabled persons in wheelchairs, in-line skaters). The terms path and trail generally are describing the same type of bike facility.

On-Street Concept



Proposed Multi-Use Path On Street 4th Avenue between 16th and 17th Street



Proposed Multi-Use Path On Street at 19th Street: Transitions to occupying existing left turn lane

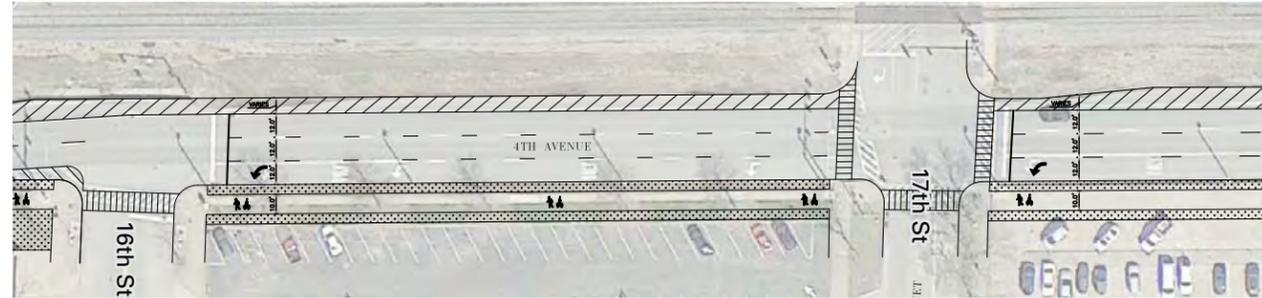


Proposed Multi-Use Path On Street between 19th and 23rd Streets: Shifts roadway to shoulder on north side

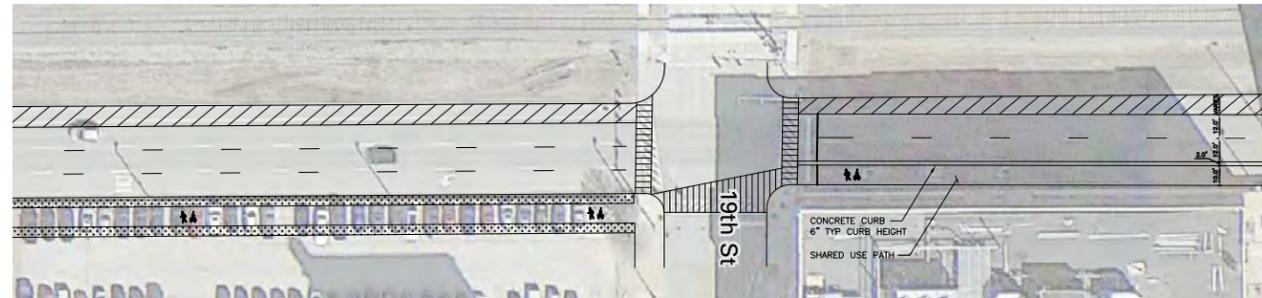


Proposed Multi-Use Path On Street at 23rd Street: Shifts roadway to shoulder on north side

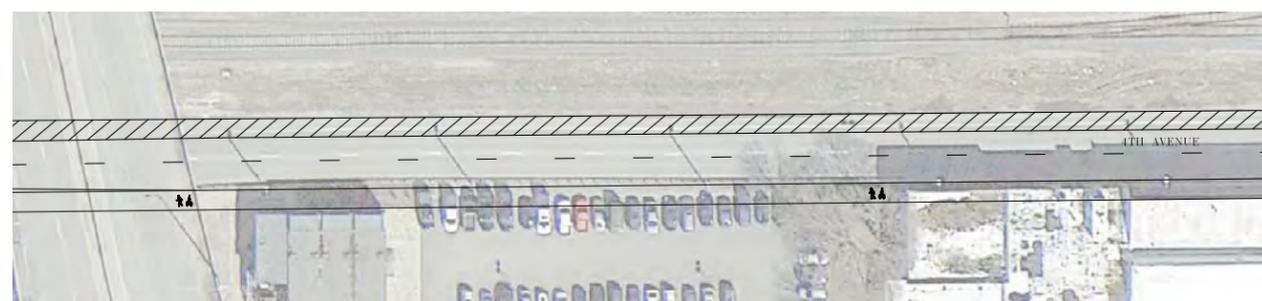
Off-Street Concept



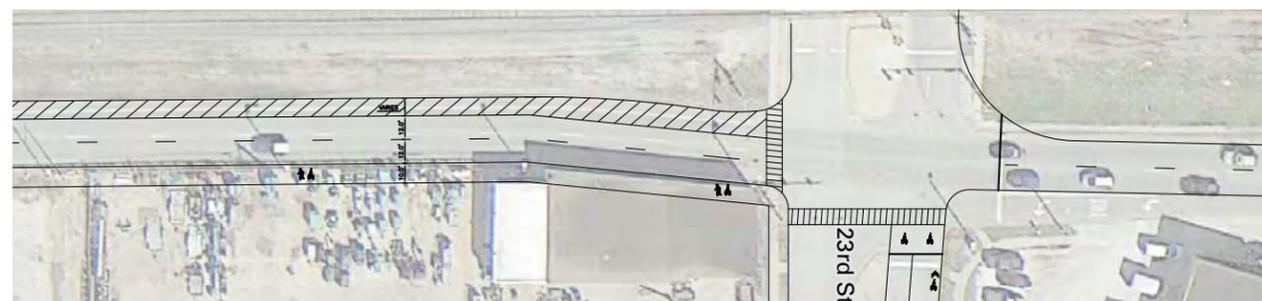
Proposed Multi-Use Path Off Street at 16th-17th Street: Expands into existing landscape buffer and parking lots



Proposed Multi-Use Path Off Street at 19th Street: Existing parking lot to be reconfigured to accommodate path. Note pinch point at existing Deere IT facility. Explore possibility of expanding northern edge of roadway.



Proposed Multi-Use Path Off Street between 19th and 23rd Streets: Expands into adjacent parcels on south side - note majority of land on this block is owned by City of Moline or IDOT and is currently impacted by I-74 Bridge construction



Proposed Multi-Use Path Off Street at 23rd Streets: Expands into adjacent parcels on south side or expands or could expand north pending further exploration into rail road right away.

Additional Challenges and Considerations

Both multi-use path configurations shown here continue east of the I-74 Bridge along south side of 4th Avenue to the 23rd Street intersection.

A design concept for the 23rd Street connection for bikes and pedestrians is shown in the sketch on this page. This intersection concept shows geometric improvements, which would make the connection from 4th Avenue to River Drive an easier transition for larger vehicles and trucks, while minimizing potential conflicts with other modes.

- At 23rd Street, pedestrians can continue in all directions via sidewalk connections. A possible off-street multi-use path connection connecting 4th Avenue to both the Great River Trail and 5th Avenue via 23rd Street should be explored.
- Bicycle “sharrow” symbols designate the path for bicyclists connecting south along 23rd Street to 5th Avenue.

The Plan recommends 4th Avenue as the preferred alignment for the multi-use path. However if engineering studies, site constraints, timing, or other obstacles are such that a multi-use path would be precluded on 4th Avenue, other alignments should be studied, including but not limited to River Drive for the section under the I-74 Bridge.

Connecting to the Surrounding Context

Because of its connection to downtown, it is imperative to provide high-visibility pedestrian crossings in all directions at the 15th Street intersection. The viability of this intersection improvement is contingent on the final configuration of the second railway tracks to be constructed. As the final site plan develops for The Q Multi-Modal Station, the City should pursue opportunities to continue bike connections both off-street and on.

By installing a hub of bike racks, fix-it stand, or other bike amenities close to the intersection of 15th Street and 4th Avenue, at the eastern edge of the Multi-Modal Station plaza, this intersection will become a node of bicycle activity. These amenities will also encourage bicyclists to get off their bikes before entering the shared plaza space.

It is recommended that the bike facility continue into the Floreciente neighborhood on 4th Avenue, and into Edgewater via 5th Avenue. See Section 2.10 for further discussion on the overall bike network.

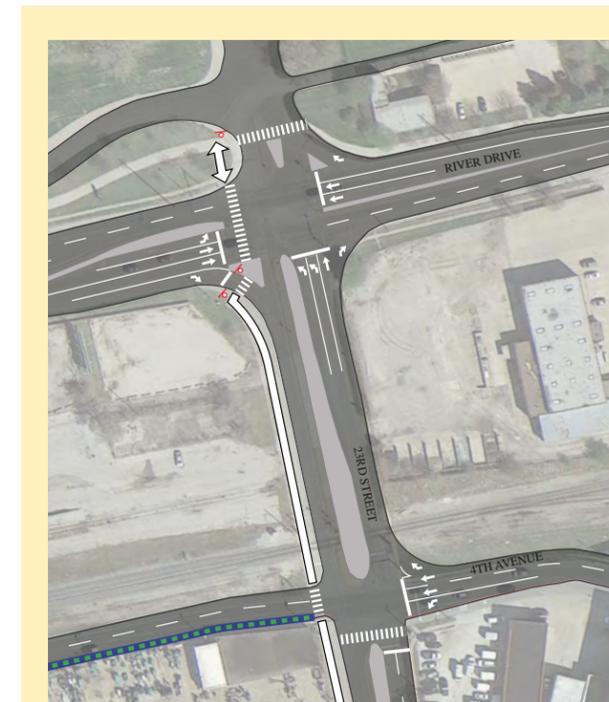
Time line

In the context of new construction at both ends of this segment, The Q and the New I-74 Bridge, the City should initiate a study of preferred design alternatives for this segment immediately. Opportunities may exist to begin design of the multi-use path as

part of roadway reconstruction scheduled for both of these projects.

Concept Implementation Strategy

It is recommended that the City pilot a temporary multi-use path along this segment as part of a larger street festival or outdoor event located on an adjacent property between 16th Street and 19th Street. This will give the public an opportunity to experience the project, and gain support for this new facility type in downtown. The final construction of bike/ped connection should coincide with the opening of the New I-74 Bridge and The Q, to fully integrate the project in the midst of changing traffic patterns in the area.



4th Ave: Multi-Use Path Design Guidelines

Bureau of Design and Environment Manual (BDE) guidelines suggest a 5 foot width horizontal separation from motorized traffic, or a 3.5 foot vertical barrier separation if the 2-way bicycle path, or shared use path, is located less than 5 feet from motorized traffic.

BDE Chapter 17: Where a two-way bike path is physically located within the street right-of-way, it shall be separated horizontally from motorized traffic so as not to interfere with the operational aspects of the roadway. This separation should be as wide as practical, but not less than 5 feet (1.5 m), and still allow the bicyclist to be visible by the motorist. For example, in an urban section, a two-way bike path would be placed much like a typical sidewalk, provided the edge of the path is more than 5 feet from the curb face, see Figure 17-2.Za of BDE.

Transition to Great River Trail at 23rd Street

- Transition 4th Avenue multi-use path on west side of 23rd Street with 10 foot multi-use off street path
- Use sidewalk on northwest corner of River Drive and 23rd Street to connect to trail
- Restrict pedestrian and bike access on east side
- Consider double left turn from 23rd to River Drive to promote truck flow
- Increase radius for right hand turn from 4th Avenue to 23rd Street



Possibility for Conversion from One-Way Street to Two-Way Streets in Rock Island and Moline (Floreciente)

Reconfigure the IL-92 couplet to better support the Floreciente neighborhood

One-way streets were historically constructed as an attempt to move vehicles quickly through a downtown area. Efficient movement of vehicles often comes with high vehicle speeds, which can have a negative impact on the pedestrian environment. On one-way streets, vehicles tend to speed up because the cars around them are flowing in the same direction.

4th and 5th Avenues are one-way streets running through the Floreciente neighborhood, with curb to curb widths ranging from 35-45 feet. The current de-

sign of these streets, which encourage high speeds and are difficult to cross, do not fit the residential character of the neighborhood. This type of street design is more appropriate in a commercial setting with limited pedestrian activity, not corridors that consist primarily of single-family homes. Re-designing these streets to better match the residential character and the existing traffic volumes will help improve the safety and livability in the Floreciente neighborhood.

The overall transportation goal for Floreciente is to slow traffic down to make the neighborhoods safer for pedestrians, provide better access to stores, and improve neighborhood feel.

Consider reconfiguring existing streetscape to calm traffic and provide an on-street bike connection

Strategies to calm traffic include:

- Reducing the number of vehicle lanes – either by reducing the street to a single through lane and increasing space for sidewalks and landscape; or removing on-street parking on one side of the street to allow for a larger or protected bike facility (further study required).
- Reduce the width of the parking lane on one side of the street and install angled parking on the other side of the street.
- Add additional mid-block crossings

- Provide horizontal traffic calming, such as neck-downs or chicanes, to slow traffic down

These recommendations would require re-striping of the existing roadway. Although the addition of a bicycle facility would slow traffic, and better support bicyclists in the neighborhoods, these concepts do not address the aesthetic and functional conflicts between the residential context and a one-way arterial couplet street configuration.

Pursue conversion of the IL-92 couplet from one-way to two-way traffic in Flore-ciente; Partner with Rock Island to move forward with design development

The conversion of the IL-92 couplet to two-way traffic in Flore-ciente would better support the residential character of the neighborhood. This concept was the subject of the IL-92 Corridor Study (2003), which put forward a concept of two-way streets from Rock Island to 34th Street. The conversion of the IL-92 couplet in Moline Center is not currently feasible due to the reconstruction of the I-74 Bridge. However, the recommendations for converting the couplet to two-way streets west of 12th Street are still feasible.

Rock Island is in the process of developing a similar conversion of the IL-92 couplet, ending at the Moline boarder. The concept shown here proposes a tie-in to the Rock Island, continuing the conversion to 12th Street. This design will provide a smooth, logical transition from Moline Centre to neighboring Rock Island.

Plan for Future Life of the Street

As the design for 12th Street and 4th Avenue related to The Q Multi-Modal Station are finalized, the City should look for opportunities to ensure design of the intersection would not prohibit one-way to two-way conversion at 12th Street. This is one of many opportunities for Moline to lay a foundation to meet future goals through the design details of existing projects.

Intersection Tie-Ins at 12th Street

Intersections where the transition to two-way traffic occurs must be extremely well marked and designed. Permanent physical barriers, and the geometry of the intersection should reinforce the transition to two-way traffic. The transition space shown in the concept drawings could also serve as a “bump-out” for pedestrians crossing 4th Avenue at the Multi-Modal Station.

Concept Approach and Time line

The City should meet with representatives developing the Rock Island IL-92 conversion project to align project goals and design details immediately. This project also relies on coordination of the intersection detail development at 12th Street near the Multi-Modal Station, and should be introduced into the conversation as details are finalized for circulation surrounding that site.

To further this idea, it is recommended that the City of Moline:

- Engage the residents of the Flore-ciente neighborhood in meaningful outreach so that they may provide insight into the design solutions that work best for their community.
- Partner with Rock Island to pursue funding for joint project.
- Utilize conclusions from the IL-92 Corridor Study (2003) and included case studies.

Louisville Case Study

Converting one-way streets to two-way streets can be an affordable strategy to renew downtown neighborhoods. In Louisville, KY, in the downtown neighborhood of Old Louisville, Brook Street and 1st Street were converted back to two way streets, becoming better and safer neighborhood streets.

A study by California Polytechnic State University and the University of Louisville reported that compared with nearby, parallel streets that remained one-way (2nd and 3rd), Brook and 1st experienced fewer collisions, less crime, and higher property valuations.



1st St One-Way at 1211 S (2007)



Brook Street One-Way at 1325 S (2007)



1st St Two-Way at 1211 S (2011)



Brook St. Two-Way at 1325 S (2011)



1st St. Two-Way with Bike Lane at 1211 S 1st St (2014)

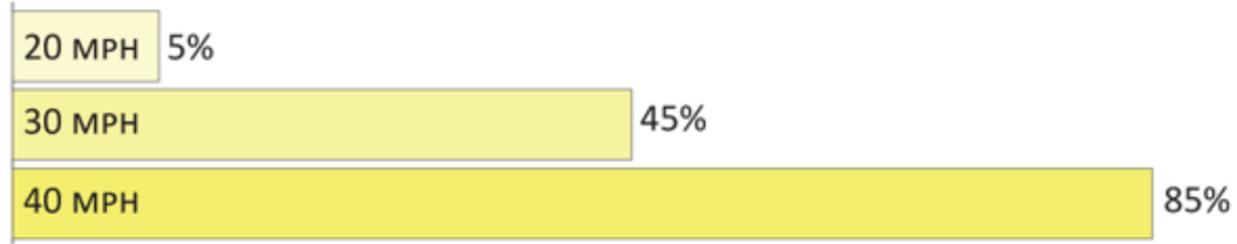


Brook St. Two-Way with Bike Lane at 1325 S (2014)

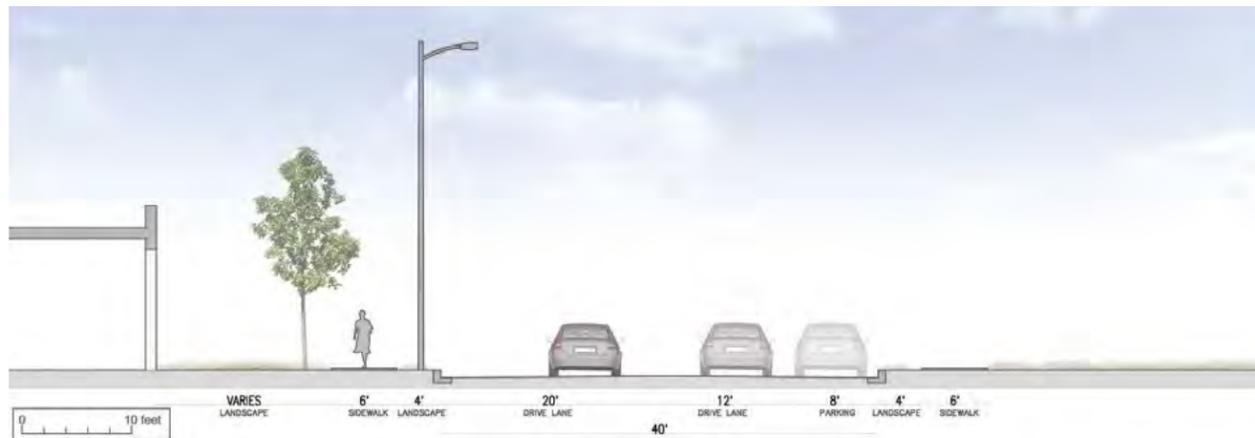
2.5 ILLINOIS ROUTE 92/ FLORECIENTE



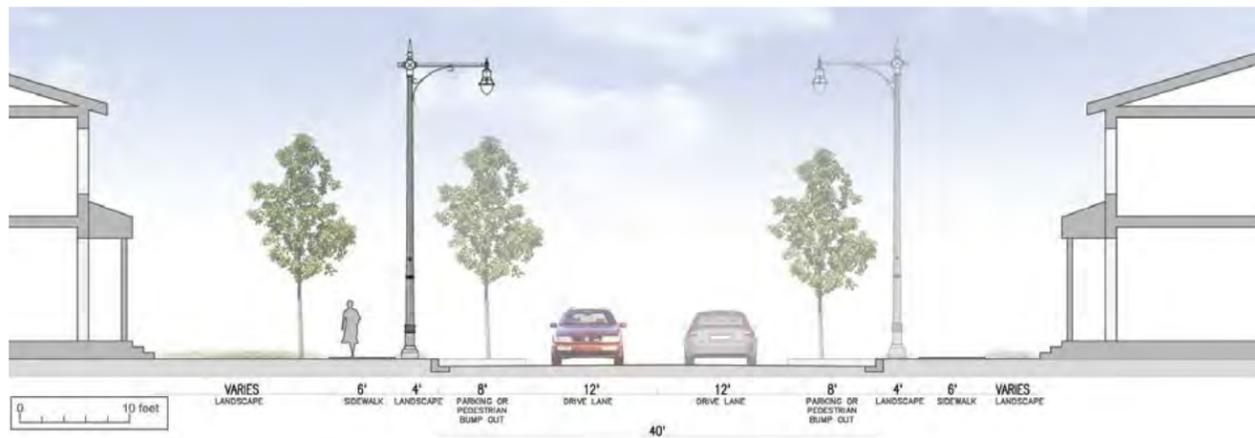
% Chance of Pedestrian Fatality at a Given Motor Vehicle Speed



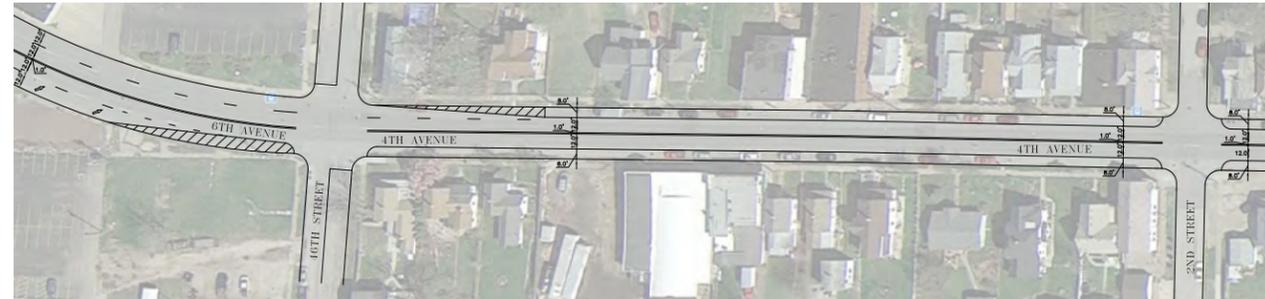
Chance of Pedestrian Fatality based on Vehicle Speed (Federal Highway Administration) The primary goal in Floreciente is to slow down traffic and promote pedestrian safety.



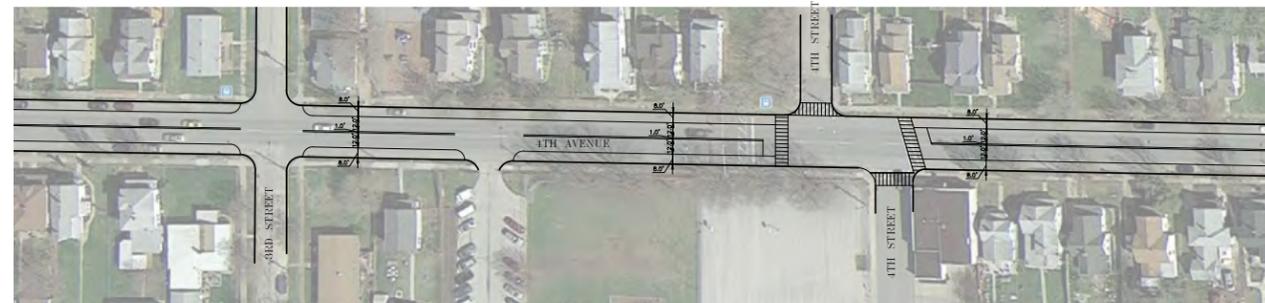
4th & 5th Existing Street Section



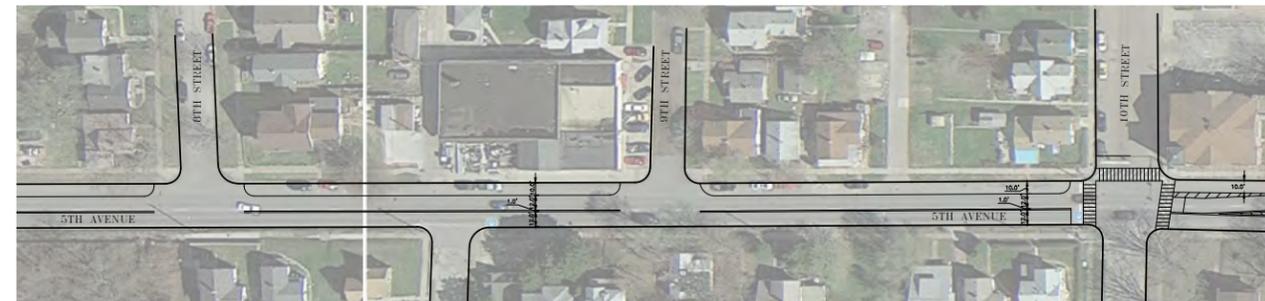
4th & 5th Avenue Proposed Street Section



Potential Two Way Configuration: 6th Avenue at 46th Street (Rock Island) and 2nd Street



Potential Two Way Configuration: 4th Avenue from 3rd Street to 4th Street



Potential Two Way Configuration: 5th Avenue from 8th Street to 10th Street



Potential Two Way Configuration: 4th Avenue at 12th Street (tie in to The Q Multi-Modal Station)

2.0 Transportation

2.6 FREIGHT RAIL



Railroad Crossings

Crossing Railroad Track Alignments

The Iowa Interstate Railroad (IAIS) and the Burlington Northern Santa Fe Railroad (BNSF) run through Moline, south of River Drive. In Moline Centre, the railroad tracks run parallel to 4th Avenue, bisecting the downtown retail district.

Concerns raised by stakeholders regarding railroad crossings fell into two main categories, the volume of train traffic through the area and subsequent impact of traffic, and the condition of railroad crossings. Additional concerns come from the noise created by trains blowing their whistles as they approach grade crossings.

There are 12 at-grade crossings (intersections where vehicles and pedestrians cross train tracks at the same elevation as the train) within the riverfront neighborhoods. At-grade railroad crossing safety is a major concern in urbanized areas.

The poor condition of at-grade crossings is visible at all crossing locations. Inconsistent pavement levels negatively impact passing cars and trucks, and pose significant challenges to bicyclists navigating the uneven pavement. At many of the grade crossings, sidewalk conditions deteriorate or disappear as they approach to the track. The fragmented sidewalks are unsafe conditions for pedestrians, and do not meet minimum design standards. The sidewalks do not adequately delineate areas where a pedestrian can

safely wait for a train to pass, or where a pedestrian has passed into the area of potential conflict.

Railroad crossing recommendations include:

- At 4th Avenue intersections in Moline Centre, upgrade the circuitry of at-grade crossings where warning signals are connected to the adjacent traffic signals so that the two systems operate in a synchronized manner stopping both cars and pedestrians from crossing the north leg of the intersection.
- Submit an application to the Crossing Safety Improvement Program overseen by Illinois Commerce Commission (ICC) for Grade Crossing Protection Funds for railroad crossing improvements in Moline Center where there are a high number

of pedestrians such as 15th, 17th, and 19th Street crossing. Utilize public feedback and Comprehensive Plan Update effort to support application.

- Complete sidewalk network connections leading up to grade crossings. Install signage indicating to pedestrians and bicyclists to use caution when crossing the tracks.
- Pursue establishing a Quiet Zone as a larger intersection upgrade strategy for the Riverfront neighborhoods.
- Refer to Sioux City successful incorporation of a downtown quiet zone along their rail alignment, as a precedent for the City of Moline.
- Estimate cost to build a pedestrian / bicycle overpass at 15th Street as a future amenity extension to the Multi-Modal Station.



The Q Multi-Modal Train Station rendering. Credit Legat Architects

The Q Multi-Modal Station

New Passenger Rail Service is planned between Chicago and the Quad Cities as part of the High Speed Rail Initiative. The Q Multi-Modal Station will work in partnership with the existing Centre Station and will be the key component of the transit-oriented development (TOD) plan for creating a Western Gateway into Moline Centre. The Q will tie the new passenger service to the existing metropolitan bus service, pedestrian, bicycle, personal vehicles, taxis, water taxis, and shuttles to the nearby Quad Cities Airport.

Transit Oriented Development (TOD) is comprised of a mix of land uses that add to district vitality near

transit. TOD's favor a variety of travel mode with an emphasis on pedestrian and bicycles activity over reliance on automobile access. A TOD must function efficiently and needs to address vehicular circulation and integrate a district parking strategy. The rendering and plan below illustrate how the station will create a high quality public place through the streetscape design along 4th Avenue and plaza designed on land adjacent to the tracks. The former O'Rourke building will be renovated into a boutique hotel adding more life and people 24/7 into the district.

The following page outlines a series of transportation recommendations to successfully integrate The Q Multi-Modal Station into the district.



The Q Multi-Modal Train Station Site Plan Credit: Legat Architects and Upland Design

Connect The Q Multi-Modal Station to the surrounding context

As the designs for the Multi-Modal Station site and surrounding street connections are finalized, it is important that the City maximize on the opportunities presented by this investment.

Although the streetscape design surrounding The Q Multi-Modal Station is not yet finalized, preliminary drawings show on-street parking on the north and south of 4th Avenue, as well on 12th Street south of 4th Avenue. The additional vehicular activity generated by the kiss-n-ride parking bay, passenger loading and unloading, hotel drop-off, and projected increase of vehicles circulating on side-streets through the area all will lead to a much more active and unpredictable driving environment on 4th Avenue between 12th and 15th Street. Pedestrian volumes are also projected to increase around the station and hotel. In the midst of increase vehicle circulation, it is vital that pedestrian amenities increase the visibility of pedestrians amongst other activity, particularly at intersections and crossings where there is a higher potential for conflict.

Moline Centre Strategies

It is recommended the City explore pedestrian enhancements, such as wider pedestrian crossings, advance stop bars at intersections, leading pedestrian intervals in signal timing, and pedestrian countdown

timers, to support the growth of pedestrian activity around the new station. The following recommendations address enhancements to improve pedestrian safety and create a welcoming atmosphere for people walking around the area.

- Improve sidewalk connections from the Multi-Modal Station in all directions.
- Use aesthetic elements similar to 5th Avenue to visually unify the streetscape into downtown.
- Design for maximum pedestrian visibility and safety at all intersections connecting directly to the Multi-Modal Station and Centre Station. Install Leading Pedestrian Intervals (LPIs) and high-visibility crosswalks at the intersections of 12th Street /River Drive, and 12th Street/4th Avenue.
- Install pedestrian crossing markings across all curb-cuts (private drives, garage access, parking lot access, alleys, etc) on all streets adjacent to the Multi-Modal Station and plazas.
- Install a bicycle hub including bike racks, fix-it stand, or other bike amenities, near the intersection of 15th Street and 4th Avenue, at the eastern edge of the Multi-Modal Station plaza. By concentrating these amenities, the 15th Street end of the plaza will become a node of bicycle activity. These amenities will also encourage bicyclists to get off their bikes before entering the shared plaza space.
- Create a new pedestrian and bike connections to the Great River Trail and Riverwalk at 15th Street.



Future home of The Q Multi-Modal Station

- Install an on-street bike facility connecting from The Multi-Modal Station to 5th Avenue on 13th Street.
- Install a speed table on 13th Street approaching the station to slow vehicles as they approach the high-pedestrian area.
- Consider the potential conversion of IL-92 in the Floreciente neighborhood to two-way traffic while designing the 12th Street intersections at 4th and 6th Avenues.
- Consider inclusion of indoor or covered bike parking at the entrance to the Multi-Modal Station.
- Promote passenger's ability to travel with bicycles on future rail trips from The Q. Should Bike Share be implemented in Moline, ensure that a Bike Share Station be located at the station.

Provide a high-quality east-west alternative route for large vehicles away from the Multi-Modal Station to allow for the growth of pedestrian and bike activity along 4th Avenue

With the additional pedestrian, bicycle, and vehicular activity likely to be generated by the development of the Multi-Modal Station, the City should identify opportunities to provide a smooth east-west passage for trucks traveling through Moline Center and Floreciente. The most logical street within the context of the existing network is River Drive, with direct access to the I-74 Highway. See section 2.3 and 2.5 for more information on truck circulation in the area.



MetroLINK Transit Service Map for the Riverfront Communities

Bus Service

Through MetroLINK, Moline has an extensive bus transit network that connects the neighborhoods of Floreciente, Moline Centre and Edgewater and to other locations in Moline and the Quad Cities. The primary bus routes travel along the east/west roadways of 4th, 5th and 6th Avenues and River Drive. The City's bus stops and shelters are visible in Moline Centre and provide a sheltered seating area and transit information. However the bus amenities in the neighborhoods are less prominent and typically are marked by a bus stop sign. Many stakeholders in the Floreciente neighborhood requested greater bus service frequency and additional routes to regional shopping areas and sport venues for teenagers. Greater ridership in the lower income neighborhoods could be

achieved through linking bus routes with community needs and by enhancing bus shelters and pedestrian routes to bus stops.

Ferry Service

MetroLINK also provides Channel Cat Water Taxi. The Channel Cat operates seven (7) days per week from Memorial Day through Labor Day, as well as weekends in September and October (weather permitting).

Bus Rapid Transit (BRT)

The Bi-State Regional Commission identified an east-west bus transit service corridor in the region's Transit Development Plan. This transit service corridor is not fully analyzed and could entail Bus Rapid Transit

(BRT), which is a transit system with specific design standards, service requirements and infrastructure to improve system quality and remove the typical causes of delay. Significant east-west roadway connections along IL-92 or River Drive could be considered for segments of the BRT alignment options, in the future.



Centre Station



Existing bus shelter

2.0 Transportation

2.8 BIKE NETWORK

Make biking more viable options for everyone in downtown.

Growth in the local student population and a younger workforce moving into downtown, the development of The Q Multi-Modal Station with passenger rail, and the reconstruction of the I-74 Bridge will all contribute to a fundamental change in downtown circulation for all modes. To capitalize on this opportunity, the City must connect residents, visitors, and employees to area amenities via a safe and attractive network of walking and biking facilities.

The focus is two-fold: thoughtful growth at the network-level to ensure the system is low-stress and intuitive, and strategic implementation of catalytic projects to gain momentum and garner enthusiasm for new amenities in the riverfront neighborhoods. Implementation of strategic, highly visible projects has the potential to jump-start biking in Moline as a means of transportation, not just recreation.

The current bikeways map shows a single existing bike facility in the riverfront neighborhoods, the Great River Trail. In order to grow a more comprehensive bike network, on-street bike facility expansion must build upon the City's existing off-street trails and parkways. The Moline Bikeways Plan 2011 proposed bike lanes along IL-92 through the Riverfront neighborhoods. Paired with modification of street designs outlined in the Street Use Hierarchy, the IL-92 on-street bike connections can be realized in downtown.

A bike community cannot build from bike lanes alone. As the City improves the network of bike facilities, amenities and City programs that support bicycling must grow as well.

Encourage on-street bike facilities in the riverfront neighborhoods.

The Great River Trail is currently Moline's only bike facility connecting the riverfront neighborhoods. This off-street multi-use path follows the southern border of the Mississippi River, but has no dedicated bike connections into the residential areas in the riverfront neighborhoods. The next phase of growth of the network requires expansion of low-stress on-street bike facilities connecting local neighborhoods to downtown destinations and the River.

On-street bike facilities will be a new infrastructure solution in the riverfront neighborhoods. With the introduction of any new street design, a period of adjustment is anticipated, along with the need for public outreach and education.

The City should renew and update the Moline Bikeways Plan with a fine-grain map of proposed bike routes, categorized by facility type, for the riverfront neighborhoods. The Bike Map should support the Street Use Hierarchy Map by establishing priority bike and pedestrian friendly streets.

The following recommendations will assist in addressing the bike network improvements and help to guide roadway design decision-making.

- Establish an annual goal of a specific number of bike facility miles to be constructed for each of the next 10 years.
- Review schedule of street resurfacing and reconstruction projects for opportunities to integrate bike facilities or wide shoulders in design.
- Implement catalyst projects such as the 4th avenue Multi-Use Path and pair with public outdoors events and publicize widely.
- Uphold the Quad City Area Complete Streets Policy as an approach to all streetscape projects in downtown.
- Review the Dan-O Watkins Memorial Bike Route for possible connections to planned or proposed trails in adjacent communities. Evaluate potential of using the historic trail as a framework for bike facility expansion in the riverfront neighborhoods and a means to acquire additional funding for improvements.
- Update signage and establish a new connections under 1-74 Bridge via 4th Avenue, as an alternative to 5th Avenue.

Further discussion of the Multi-Use Path on 4th Avenue in Moline Centre can be found in Section 2.5



Indianapolis Cultural Trail



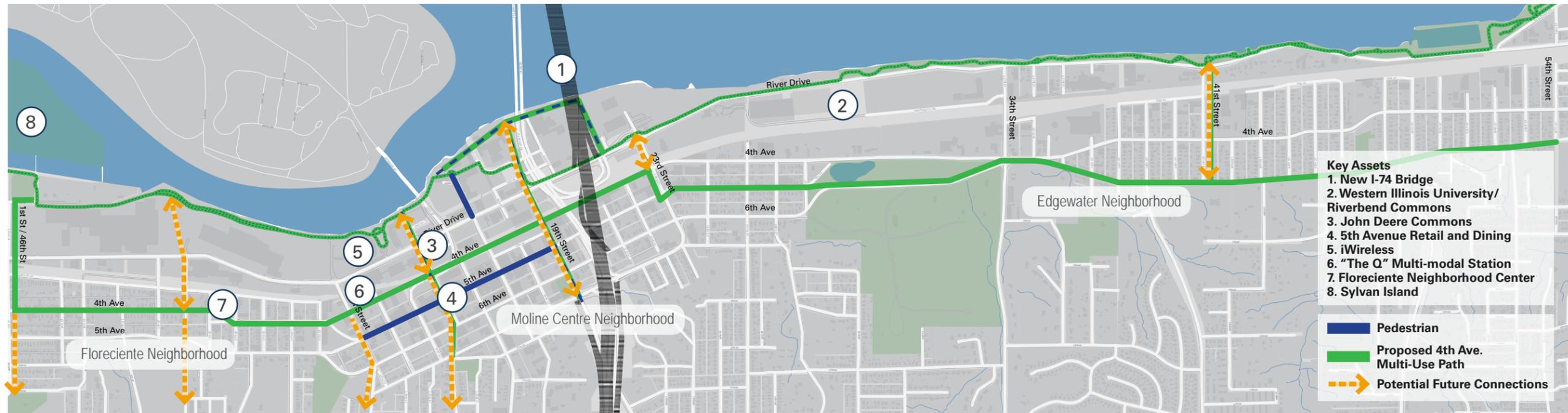
Bike Share example



Indianapolis Cultural Trail

2.0 Transportation

2.8 BIKE NETWORK



Existing and Proposed Trail Connections

Provide bike and opens space connections where people live, work, and play.

Build and sign Great River Trail connections to nearby neighborhood residential areas and adjacent destinations. Use signs or pavement paint to highlight preferred bike paths and destinations.

- Moline Centre: 15th Street Sharrow lane marking connecting 5th Avenue to the Great River Trail. Sharrows, or Shared Lane Markings are road markings used to indicate a shared lane environment for bicycles and automobiles. The MUTCD outlines guidance for shared lane markings in section 9C.07.

- Floreciente: Bike lane connection at 1st Street/ 46th Street connecting from 5th Avenue to the Sylvan Island Bridge Park.
- Edgewater: Sharrow facility and bike boxes on 34th Street connecting north and south to 5th Avenue, Riverside and Peterson Park.
- Utilize a web-based crowd source mapping tool to collect public input for the locations of potential inter-neighborhood connections.
- Build sidewalk or multi-use path along 5th Avenue between 27th Street and 34th Street in Riverside Park.
- Install bike parking within 100 ft of the front entrance to building with 25 employees or more.

Focus amenities for bicyclists around the Trail-to-Transit Connections in the area.

- The City of Moline should capitalize on the multi-modal growth downtown by highlighting Trail-to-Transit Connections.
- Centre Station, The Q Multi-Modal Station, and the new Channel-Cat Water Taxi Station at WIU-QC/ 34th Street are examples of potential locations to highlight Trail-to-Transit Connections. This concept was introduced in the Moline Bikeways Plan 2011. Beyond highlighting locations to build network connections, these Trail-to-Transit connections should be celebrated with signage and a concentration of bike- and transit-supportive amenities. This type of program could be

championed by Special Service Areas.

- Provide covered bike parking and transit shelters at Trail-to-Transit Connections.
- Continue effort to re-open Sylvan Island Bridge to Public

Continue and expand existing programs and policies to increase visibility of cycling and safe cycling in Moline

- Continue to support and expand the Safe Routes to School Program in Moline
- Support growth of the Moline Police Department Bicycle Patrol. Schedule regular patrol routes through the heart of the Downtown Neighborhoods via bike and pedestrian priority streets.

2.8 BIKE NETWORK

- Engage the Moline Police Department Bicycle Control and Parks Departments to participate in outdoor events and activities.
- Continue to work towards full implementation of the 2007 River-to-River Corridor Plan.
- Build from neighboring bike network expansion including the bike path on the new I-74 bridge and links to Bettendorf, IA.
- Partner with adjacent cities to create on-street connections that extend outside of the municipal boundary.
- Encourage multi-unit residential and commercial buildings to integrate indoor or covered bike parking facilities.

Better support the user choice to take alternative modes by providing support amenities.

- Increase the visibility of bike facilities by implementing consistent “Bike Route” Signage, and maintaining street striping on streets with bike facilities.
- Engage public to determine locations for bike parking. Establish a dedicated e-mail address to receive bike parking requests from area businesses.
- Expand bike rental options along the Great River Trail. Partner with Riverfront Hotels to build a network of bike rentals.

- Incentivize outdoor activity organizations to relocate along the riverfront or in Moline Centre.
- Update and distribute Bikeway Network maps to local businesses and institutions, annually.

Support user choice to take alternative modes by fostering the bike community

- Improve public access to information about bike facilities and programs on the City’s website. Create a high-resolution, printable Regional and Local Bikeways map of Moline.
- Feature local and Quad Cities bike community activities and programs on the city’s website.

Establish Bike Supportive Policies for the Riverfront neighborhoods

- Establish minimum bike parking incentives for new construction or major renovation of multi-story residential and commercial buildings.
- Establish a “Take-the-Lane” Policy for The City of Moline. This type of policy gives bicyclists use of the full lane along streets that are not wide enough to safely support a bike and vehicle within the same lane. This type of facility better supports on-street biking in an area without a contiguous striped network of bike facilities.



Great River Trail in Edgewater



Birks Recreational Trail



Great River Trail along 19th Street north of River Drive

2.9 PEDESTRIAN NETWORK

Pedestrians are a very significant component of any urban transportation network. Every neighborhood strives towards achieving a safe and pleasant walking experience for their community. This requires a focus on the pedestrian first at roadway intersections to ensure visibility, continuity and good condition of the walking path for safe passage. Most community assets generate pedestrian activity and it is important to connect people to the places they want to go. The following are key goals for improving the sidewalk network in the three riverfront neighborhoods.

- Articulate an area-wide approach and improvement strategy for sidewalks in the riverfront neighborhoods.
- Build legible linear pedestrian connections between neighborhood assets.
- Fill in the gaps to complete the network of sidewalks connecting key features in neighborhoods.
- Meet minimum ADA and safety standards.
- Conduct an area-wide audit of sidewalk and intersection in the riverfront neighborhoods.
- Build on the City's existing record-keeping of intersection condition and upgrades in Moline Centre. The City should establish a baseline data set of sidewalk conditions by surveying all streets. Opportunities may exist to partner with local schools or community organizations to conduct surveys within each neighborhood.
- Address sidewalk network issues within each neighborhood as part of a larger strategy to build neighborhood character.

As part of the analysis of downtown neighborhoods, the consultant team conducted a high-level sidewalk survey along key streets in Edgewater and Flore-ciente. Streets were chosen based on their role in the proposed street hierarchy and streets that serve as important bike or pedestrian connections were a priority. The survey broke down the observed condition of sidewalks along key roads into four main categories: Good, Fair, Poor, and Missing. The survey of sidewalks showed that the needs and challenges in the area vary depending on the neighborhood and adjacent land uses. This survey is located in the document appendix.

Build and fill in missing connections

There is a lack of sidewalk connectivity in the Edgewater neighborhood especially along 4th Avenue, and few connections from the existing sidewalk segments and adjacent businesses. The auto-centric nature of the corridor requires investment in sidewalk facilities to ensure pedestrians are comfortable and visible in the context of a large roadway with many driveways and intersections. Based on high-level sidewalk condition review, sidewalks connections in the Edgewater residential areas are in better condition overall than those in the Flore-ciente Neighborhood.

- Review Edgewater neighborhood for continuity and condition of sidewalks along residential streets. Infill and improve where necessary.



4th Avenue in Flore-ciente



Sidewalk underneath 16th Street Ramp

2.9 PEDESTRIAN NETWORK

- Focus reconstruction and improvement around transit stops where needed. Ensure transit stops are connected to residential streets via a contiguous network of sidewalks.
- Review sidewalk continuity and crossings between 5th Avenue and River Drive via 34th Street, 41st Street, and 1st Street. In fill sidewalks and improve where necessary.
- Review sidewalk continuity from residential streets to local area parks.
- Consider proposed pedestrian upgrades at 23rd and 34th Street intersections.

Further discussion on the impact of truck circulation and River Drive traffic calming on pedestrians in Edgewater can be found in Sections 2.3 and 2.4.

Improvements around The Q Multi-Modal Station

The City should explore pedestrian enhancements, including wider pedestrian crossings, advance stop bars at intersections, leading pedestrian intervals in signal timing, and pedestrian countdown timers, to support the growth of pedestrian activity around the new station. These enhancements will improve pedestrian safety and create a welcoming atmosphere for pedestrians.

Strategies to improve the area around The Q Multi-Modal Station include:

- Improve sidewalk connections from the Multi-Modal Station in all directions, use aesthetic elements similar to 5th Avenue to visually unify the streetscape into downtown.
- Design for maximum pedestrian visibility and safety at all intersections connecting directly to the Multi-Modal Station and Centre Station.
- Install pedestrian crossing markings across all curb-cuts (private drives, garage access, parking lot access, alleys, etc) on all streets adjacent to the Multi-Modal Station and plazas.
- Create a plaza with enhanced streetscape near 15th Street and 4th Avenue, at the eastern edge of the Multi-Modal Station plaza.
- Create a new pedestrian connection to the Great River Trail and riverwalk at 15th Street.
- Install a speed table on 13th Street approaching the Multi-Modal Station to slow vehicles as the approach the high-pedestrian area.
- Consider the potential for future tie-in to two-way traffic in Florencia in the design of the 12th Avenue intersections at 4th and 6th Avenues.



4th Avenue in Edgewater



5th Avenue at Riverside Park

2.9 PEDESTRIAN NETWORK



Improvements in Moline Centre

As the historic center of Moline’s downtown, Moline Center has the most contiguous network of sidewalks. Issues identified in Moline Centre focus on intersections, rather than contiguity of the network.

- The City of Moline should establish a policy of installing high-visibility crosswalks at all intersections within the area bound by the River, 12th Street, 7th Avenue and 23rd Street. This will serve to reinforce the urban center of downtown, and increase the visibility of pedestrians in the areas of highest pedestrian traffic.
- Establish a maximum daily vehicle count and speed for streets allowing the typical two-bar border crosswalks. All streets above this minimum standard should be priority for intersection improvements.
- Install pedestrian countdown timers at all pedestrian crossings at River Drive and 4th Avenue.
- Consider installing Leading Pedestrian Intervals at all crossings adjacent or connecting to the iWireless Stadium, Centre Station, the Multi-Modal Station, John Deere Commons, and remove, combine, or minimize under-utilized curb cuts or access points to parcels in downtown.

Formalize a signalized mid-block crossing connecting to iWireless Entrance.

The city of Moline should investigate the possibility to introduce a curbed boulevard with a center median to provide mid-block pedestrian refuge islands.

- Designs should ensure the median island does not conflict with truck turning movements.
- A large center median allows for some flexibility in design explorations.
- Expand sidewalks or utilize central median space to allow for pedestrians gather to make fewer crossings in larger groups as part of a larger strategy to mitigate the impacts of major events on traffic flow in downtown.

See Urban Design Chapter 3.0 for further discussion about streetscape.



5th Avenue in Moline Centre



16th Street at Moline Centre

2.10 MOLINE CENTRE / PARKING



Support the growth of downtown and local retail centers by improving access.

Moline’s downtown is a local and regional draw because of the number and variety of destinations located within its borders. It is vital for Moline to provide easy access to available parking to support the needs of residents, visitors, and vested businesses. Construction of the new I-74 Bridge, The Q Multi-Modal Station, and other developments in Moline Centre highlight the need for the City of Moline to manage the demands on their current parking inventory and to better understand how to most effectively use their resources in the future. As existing parking lots serve more residents and a growing business center, ensuring the availability of convenient spaces will be more and more difficult.

Parking in downtown is discussed in the Moline Centre Master Plan Update, Illinois Highway 92 Corridor Study, and the 2014 Moline Comprehensive Plan Updates. Each plan identified similar needs and challenges, but contain little consensus on recommended improvements. Stakeholder interviews confirm the issues, which range from insufficient supply in close proximity to destinations, to a lack of wayfinding and signage on existing facilities.

Parking Strategies

Based on the evaluation of the existing parking conditions and stakeholder interviews, the project

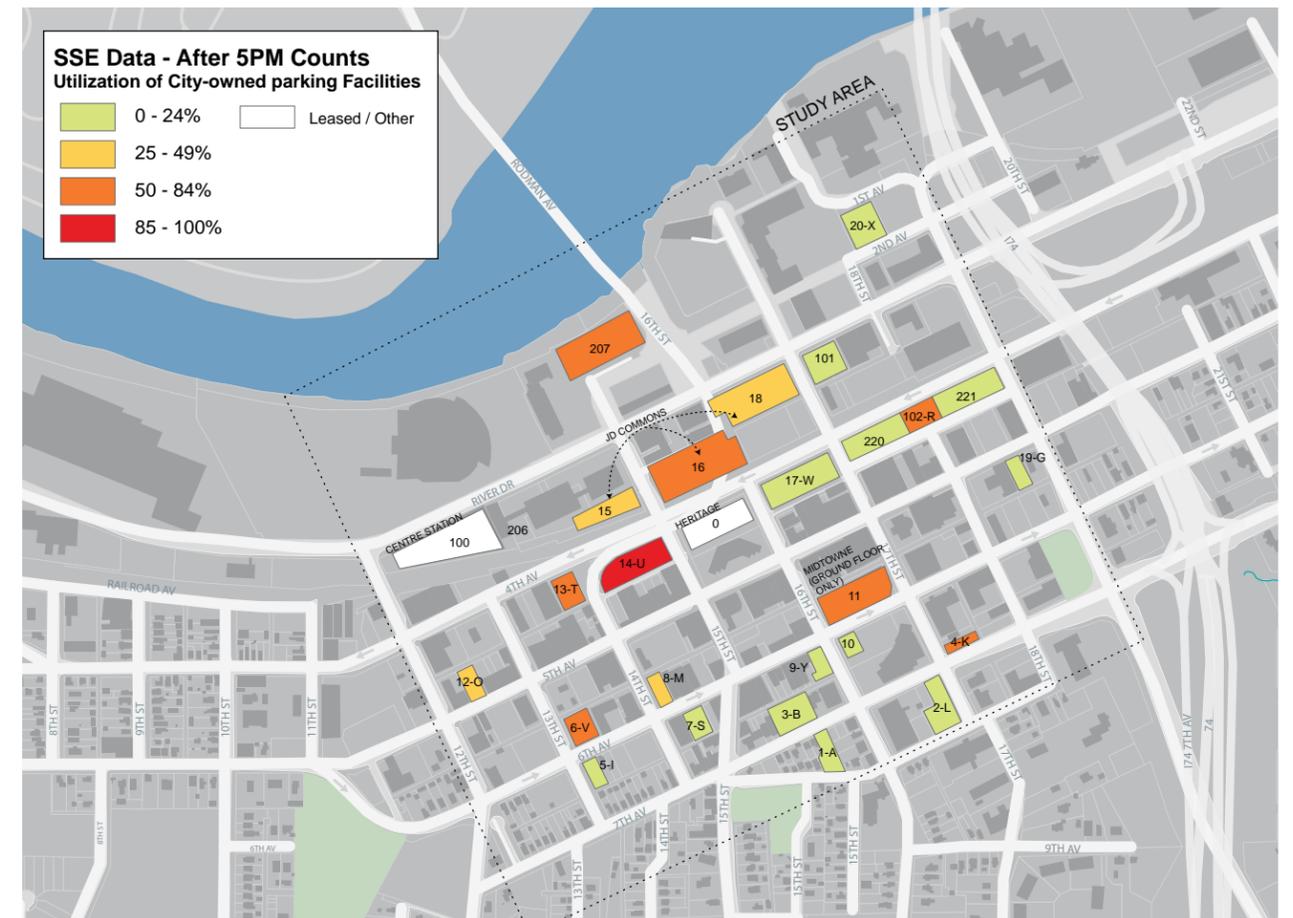
team identified the following primary issues, and developed recommendations related to parking in Moline Centre.

Simplify the System

In Moline Centre, parking is difficult to identify and regulations are confusing for users. As a small urban area, drivers will make the assumption that they will be able to find parking close to their destination. Drivers circulating in downtown looking for parking need to be able to quickly and easily identify an available lot and how to access it from their vehicle.

Upon arriving downtown, drivers looking for parking in Moline are confronted with inconsistent signage, and unclear regulations. Some facilities are signed facing main roads, some at the lot entrances, and some within the lots themselves. Different colors and sign types are used to describe a single parking lot use. In many lots there are mixes of leased spaces, signed as both rows and individual stalls, and public parking spaces. Some signs are posted with three or four subsections of information referring to restrictions on different times of day, days of the week, enforcement types, and lot uses. On most signs, the messages are displayed in a text size that would be challenging to read from a passing vehicle.

Signage for a lot doesn’t serve drivers if they are unable to safely stop and turn into the lot once they identify the entrance.



Parking in Moline Centre



On Street Parking in Moline Centre

2.0 Transportation

2.10 MOLINE CENTRE / PARKING

The primary concern varied from each stakeholder perspective on this issue. General concerns from the City's perspective surround the impact of large events on the circulation of traffic and availability of parking for local retail patrons during large events. The perspective from the iWireless representatives centered on the lack of available parking for event attendees within a reasonable walk to the arena.

At present the arena and the City are sending conflicting information regarding arena access and parking to event attendees. In addition, there appears to be no coordinated traffic management strategy. New development coming to the immediate area will exacerbate the existing issues. Congestion approaching the intersection at 12th Street and River Drive will likely be compounded by the construction and subsequent traffic generated by the future Multi-Modal Center and Hotel at 12th Street and 4th Avenue. In light of development plans for the area adjacent to the iWireless center, the time is right for the City and iWireless to address traffic management and parking in the area and devise a collaborative strategy for better management of traffic circulation and parking in the area.

- Establish a parking and traffic management strategy for large events.
- Establish consistent communication between all stakeholders.
- Seek out all additional resources available on an as-needed basis.

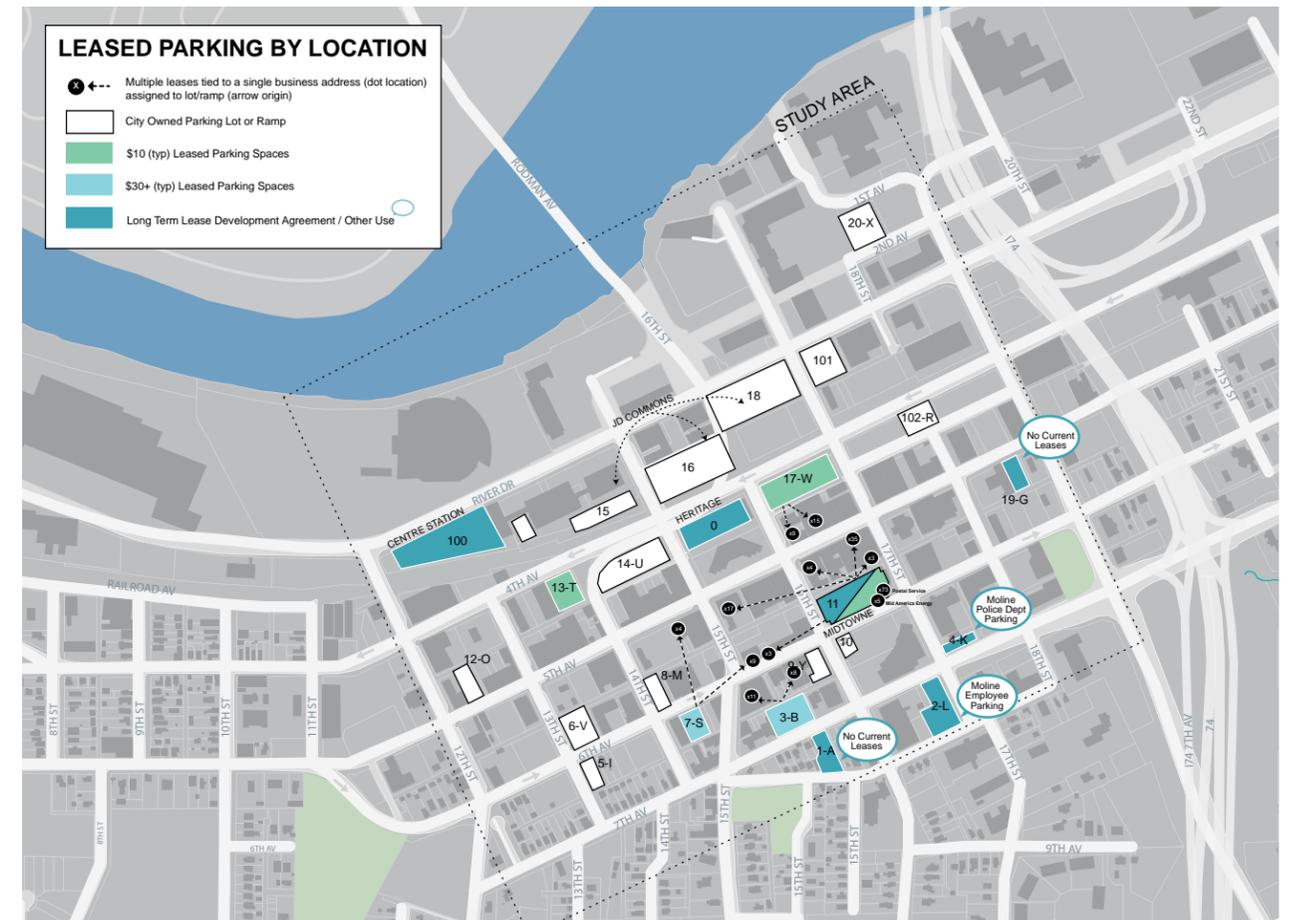
User-restricted parking, such as employee parking or customer-only parking makes it difficult for motorists to park and walk to multiple destinations. It also limits supply of available parking during certain time periods. Public shared use parking can provide a better alternative to private parking that is only for the users of the principal building on the property.

Surface parking can distract from the quality of the urban environment and discourage walking. Wayfinding signage needs to be improved to public parking and improve the connections between public parking facilities and commercial land use and public establishments. The need for pedestrian improvements also was discussed with connectivity barriers and traffic calming.

The supply of parking needs to be studied in greater detail to identify areas of surplus and deficit, and to determine overall occupancy levels at various time periods throughout the day.

Improve Wayfinding and Legibility in Downtown

Community stakeholders also stated that directional wayfinding signage would be helpful to access the public parking facilities. It also was suggested that the walking environment between public parking facilities and public attractions needed to be improved. See Section for 2.11 for more information on wayfinding and signage.



Leased Parking in Moline Centre



Existing Parking Signage can be confusing

2.11 MOLINE CENTRE WAYFINDING



Introduction to Wayfinding

Within an urban environment a person unfamiliar with a particular location should be able to navigate using traditional visual cues such as landmarks and a general understanding of how a city, town or village is traditionally organized. When clues are missing or the network breaks down, signage is often used to provide missing information and help guide a person to their destination.

To be successful, a wayfinding signage system provides a comprehensive, consistent, easy to read, and easy to understand network of signage.

Moline

Downtown Moline is a popular destination for visitors. There are several elements within Moline that could support intuitive wayfinding, such as the River, the I-74 bridge, and the bluff. Unfortunately these major landmarks, especially the River, are not easily visible. Navigation around Downtown Moline is further complicated by a network of one way streets.

Over time, Moline has implemented a series of wayfinding signage systems. The current system has challenges. It is inconsistent, in that it is comprised of several generations of designs. The text heavy, inconsistent signs are difficult to read and digest from a moving automobile.

Moline suffers in that one of its key assets, the River, is difficult to find even from other key assets such as the John Deere Pavilion and major hotels, even though they are sometimes only a block away. New or updated signage will be required since visitors will soon be arriving from two new entrance points including the new I-74 bridge and the Q Multi-Modal Station making this the ideal time to revisit the current signage system.

Signage related to parking is addressed in Section 2.10 and should be folded into one comprehensive system.

Previous Recommendations

The 2001 Moline Centre Master Plan Update included recommendations for a signage and wayfinding system the addressed legibility of the system. The recommend signs were comprised of a uniquely colored simple icon for each destination that is centered on the sign with a directional arrow above it and its name below. These signs could be placed at critical locations along the designated routes on existing light posts and directional signals. Though some of the design may be dated, these recommendations are still relevant.

The 2009 River Drive Corridor Concept Plan introduced community gateways elements to reinforce the character of Moline’s neighborhoods.



Existing wayfinding signage is difficult to read from a car

Over arching Recommendations

It is recommended that Moline engage with a wayfinding consultant to audit the existing system and provide recommendations for either updates to the existing system, or to develop a new, coherent system in its entirety. Moline would benefit most from developing a new comprehensive wayfinding system with replacement of all legacy systems.

Directional Signage

The wayfinding directional system that the City of Moline has implemented works well from a pedestrian point of view, but due to the small text size it is not legible from an automobile point of view.



A variety of sign systems and designs exist within Moline

Three similar, but not consistent, systems work to direct people to destinations. This inconsistency in the way signage is presented makes following the system from starting point to destination difficult.

It is recommended that Moline:

- Updated or replace signs to include a system that is legible from a moving automobile.
- Establish consistency within the graphic style.
- Revisit the signage system developed 2001 Moline Centre Master Plan Update for relevant recommendations.
- Improve directional signage to key destinations such as the Moline Riverfront at 15th and 19th St, WIU-QC and the future Q Station.

2.11 MOLINE CENTRE WAYFINDING



Existing gateway signage at I-74 exit ramp



Existing neighborhood gateway signage



Existing neighborhood identifiers

In addition to directional signage there are a wide variety of signage types to be considered within a comprehensive wayfinding system

Gateway Signage

Existing gateways are subtle and difficult to read from a moving car. Gateways also do not reflect unique nature of each community.

It is recommended that Moline:

- Relocate gateways to coordinate with new exit ramps locations for the new I-74 bridge
- Strengthen visual impact of gateways
- Signage should be legible from car
- Neighborhood gateways should be reflective of neighborhood identity and community image

Neighborhood Identifiers

Some districts have developed their identifiers that creating a sense of place while other neighborhoods are lacking in clearly defined boundaries or identifiers.

It is recommended that Moline:

- Promote the use of neighborhoods identifiers to enhance a sense of community
- Establish guidelines for implementation that allow for unique expression while ensuring quality of design and construction
- Engage the community in design process



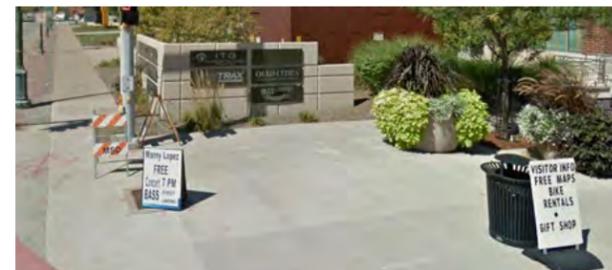
Gateway signage to Sylvan Island



Ben Butterworth Parkway signage



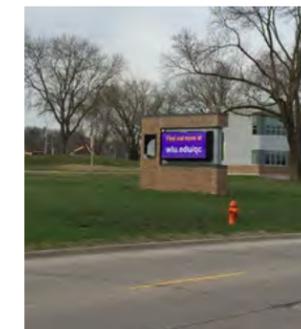
Birks Recreational Trail signage



Temporary signs promoting events at Bass Street Landing



Existing informational kiosk



Electronic sign at WIU

Trail Signage

The interconnected system of riverfront trails is one of Moline's great assets. Signage is present along the trail, however it is inconsistent. Connections between the neighborhood and trails the neighborhoods are hard find. Opportunities to access downtown assets from the trail are often times not clearly identified.

It is recommended that Moline:

- Improve the consistency and continuity of the wayfinding system between the City, the Great River Trail, the Ben Butterworth Memorial Parkway, and the Birks Recreational Trail
- Improve wayfinding access points to and from the Great River Trail

Informational Kiosks

Moline has a thriving active downtown with many popular special events, but currently has no consistent way to promote these events.

It is recommended that Moline:

- The design and implementation of informational kiosks should be included in future signage systems
- Consider promotion of large and small events in the design of the system and the ability to update signs quickly and remotely.
- Consider an electronic message system to provide information and updates on parking, directions, events, etc.



The background image shows a street scene in Moline, Illinois. On the right, a brick building features a prominent bay window on the second floor. Below the bay window, there is a sign for '415' and a circular logo for 'Bar & Grille' depicting a cartoon character. To the left, a street sign on a pole indicates 'MOLINE CENTRE'. The entire image is overlaid with a semi-transparent orange filter.

3.0 URBAN DESIGN & LAND PLANNING

MOVING MOLINE FORWARD: URBAN DESIGN & TRANSPORTATION UPDATE TO
THE 2014 COMPREHENSIVE RIVERFRONT PLAN

JUNE 20, 2016

3.0 Urban Design & Land Planning

3.0 INTRODUCTION



The Moline riverfront neighborhoods are part of the City's industrial legacy and are pivotal to re-defining and leading future growth and development. Ideally located adjacent to the phenomenal Mississippi River and existing transportation infrastructure, Moline Centre and the adjacent neighborhoods of Flore-ciente and Edgewater were once walkable urban environments built at a time when industry and manufacturers were integrated into their immediate communities.

Over time these neighborhoods' walkable urban character was eroded by the dominance of the automobile and industrial truck traffic. Today the desire for walkability has once again risen to the forefront of conversation. Quality of life choices are being made by a new generation who are seeking more urban lifestyles based on things such as proximity to work, shopping, entertainment and recreational amenities. As the City of Moline looks forward, there is an opportunity to re-think and re-position these three riverfront neighborhoods as a way to improve the quality of life for all residents and to draw more people and amenities to the community. This dynamic helps attract new talent, creates an environment where new businesses want to locate, and brings in culture and institutions that energize the community.



Moline Riverfront: Great River Trail at 15th Street

3.0 Urban Design & Land Planning

3.0 INTRODUCTION



Moline Riverfront Neighborhoods: Floreciente, Moline Centre, Edgewater

A traditional downtown is a unique resource that is being rediscovered through the renaissance of post-industrial cities throughout the US. The notion that a suburban lifestyle is the only residential choice is being challenged by more diverse urban housing choices including renovations of buildings into residential lofts and new housing of all types. Urban lifestyles are being sought in cities both large and small across the US, and Moline is no exception to this trend. A younger generation is showing a preference for living in more compact urban developments in close proximity to resources and amenities. This type of environment supports the growing number of people who are looking for ways to minimize dependency on the automobile. For example, based on the survey conducted for

this Plan, 9% of the respondents do not use a car for their primary mode of transportation. Employers and residential developers are also looking to co-locate in urban neighborhoods creating a mixed-use environment that then encourages more retail businesses, restaurants, and other community amenities.

Creating a vibrant downtown takes decades of deliberate economic development, urban design and strategic transportation decisions. While this plan focuses on two key components of urban revitalization – transportation and urban design – the social and economic issues are equally, if not more important to harnessing success. This Plan emphasizes bringing new residential (loft conversion

and infill) to Moline Centre and attracting assets that enrich the existing residential communities of Edgewater and Floreciente. Establishing safeguards for maintaining affordability is a key component to preserving cultural diversity as the downtown transitions with new development. This Plan defines a framework for infilling vacant and underutilized parcels with development that can be catalysts for further expansion in the future. This strategic infill strategy is needed to create an urban environment that feels like a cohesive place where people are encouraged to walk freely through their neighborhood undeterred by disconnected sidewalks, vacant lots or empty buildings. This Plan proposes an incremental approach to infill development, realizing that the market demand can only absorb a certain amount

of new development at any one time. Therefore development clusters are being proposed to support and strengthen the impact of catalytic projects such as a Western Gateway surrounding The Q Multi-Modal Transportation Center and an Eastern Gateway to downtown surrounding the new I-74 bridge.

The transportation goals in this Plan are based on the premise that changes in mobility are critical to signaling that Downtown Moline is accessible to motorized vehicles but is also moving towards a mixed-use, 24 hour, urban, walkable, transit oriented and bike friendly environment. The Plan also shows that Moline is invested in the success of the adjacent neighborhoods by connecting them directly to downtown, the River, jobs and amenities.

3.0 Urban Design & Land Planning

3.0 INTRODUCTION



Creating safe, multi-purpose, inviting streets are key indicators of those fundamental changes. Roadways have many users and are not just a means for conveyance of automobiles, trucks and goods. Streets are representative of the community front doors and signals of the overall community's economic and cultural vitality.

As the industrial waterfront changes to incorporate community assets such as the Great River Trail for recreation and biking, Bass Street Landing for tourism, iWireless Center for entertainment, The Q Multi-Modal Transportation Center, and Western Illinois University-Quad Cities (WIU-QC) there can be inherent conflicts between the various land uses, property owners and community needs. Mobility needs of industrial users are different from the needs of residents, retailers, employees, students and tourists seeking to access businesses, retail, hotels, new urban amenities and cultural institutions. This Plan lays out a series of incremental interventions that allows multiple users to share roadway infrastructure in a safe, functional and aesthetically appealing way. Streets play an important part in setting the stage for development so the design of the pedestrian experience must be inviting. The strategies in this Plan address streetscape design including creating a well-connected sidewalk network and calming traffic. It provides guidelines for roadway configurations, landscape, street trees, parking lots, signage and wayfinding design.

Planning Principles

Moline's Riverfront Neighborhoods can continue to grow, and thrive through the series of positive actions and urban interventions outlined in this chapter. Strategies presented here support the following principles:

- Set a realistic vision that is implementable within a 10 year time frame.
- Identify focus areas for first phase development and catalyst projects to spur future development.
- Respond appropriately to market trends and conditions.
- Connect the riverfront neighborhoods within themselves, to each other, and to the Riverfront.
- Connect visitors and residents to key destinations.
- Emphasize proximity to the River.
- Reinforce a walkable urban fabric.
- Integrate transportation recommendations.
- Preserve existing building stock; and promote adaptive reuse.
- Establish strategic infill and redevelopment strategy.



Bass Street Landing: Mixed-use district with offices, hotel, restaurants, a public plaza and connections to the Great River Trail.



Sylvan Island Gateway Park: Open Space at the entrance to Sylvan Island adjacent to the Slough and the Great River Trail in Florencia.



WIU-QC Riverfront Campus: Growing campus bringing people and amenities to Moline.

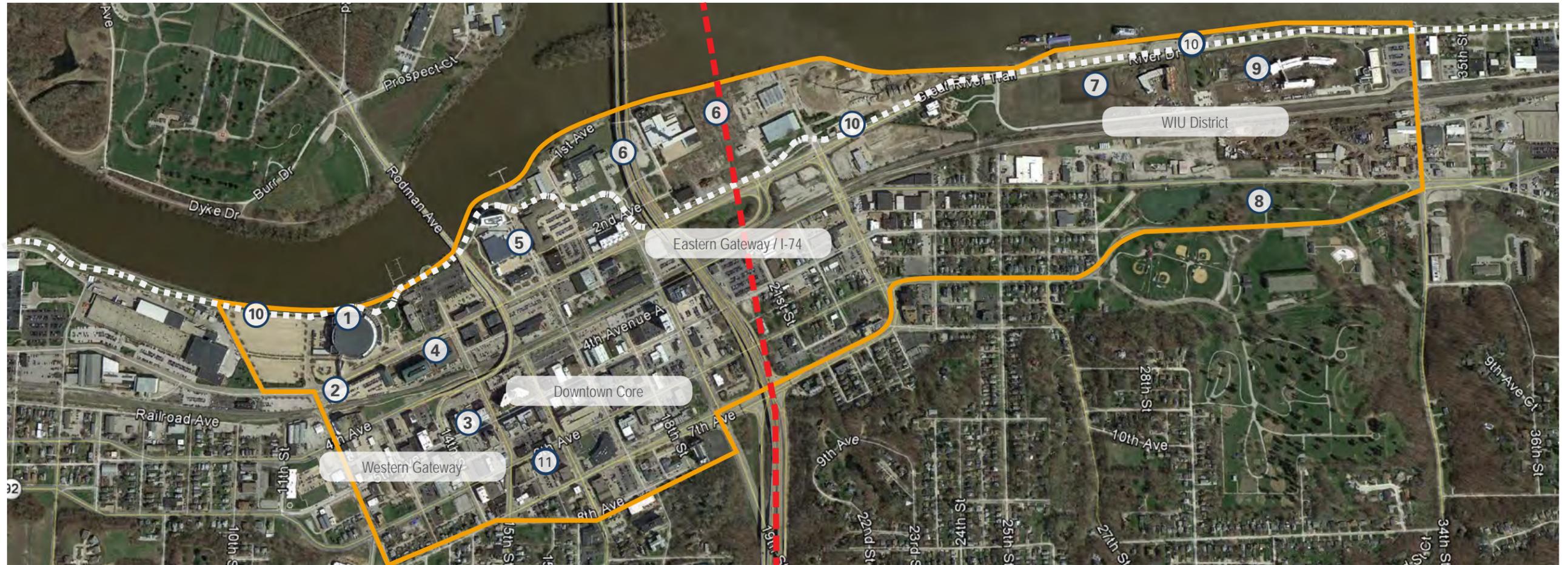


5th Avenue in Moline Centre: Pedestrian friendly downtown mixed-use district.



The Mississippi River and the Great River Trail: Some of Moline's great assets.

3.1 MOLINE CENTRE NEIGHBORHOOD



Moline Centre: Comprised of four subdistricts - The Western Gateway, the Downtown Core, the Eastern Gateway/I-74 Area, the WIU-QC District

- Key Features**
- | | |
|-----------------------------------|---|
| 1. iWireless Center | 8. Riverside Park |
| 2. The Q Multi-Modal Station | 9. Western Illinois University - Quad Cities (WIU-QC) Riverfront Campus |
| 3. 5th Avenue Mixed-Use Corridor | 10. Great River Trail |
| 4. John Deere Pavilion | 11. City Hall |
| 5. Bass Street Landing | |
| 6. I-74 Bridge / Realignment Zone | |
| 7. Riverbend Commons | |

Moline Centre Overview

Moline Centre plays an important role within the community and is home to the central business district and the civic core. It provides a wide variety of amenities to the community including neighborhood retail, regional entertainment, jobs from light industrial and corporate offices, and is home to major institutions including Western Illinois University - Quad Cities (WIU-QC).

Moline Centre is experiencing a growing residential population, is increasingly becoming a destination for dining and entertainment, and continues to be an important job center with strong commitment from local employers.

Moline Centre Subdistricts

Moline Centre is comprised of four subdistricts including: the Downtown Core; the Western Illinois University - Quad Cities District; the Eastern Gateway

around the I-74 Bridge; and the Western Gateway around the future Q Multi-Modal Station. These four subdistricts will be studied in more detail on the following pages.

Current projects/development sites

There are several projects influencing the development of Moline Centre today. I-74 Bridge Re-Construction will change the circulation patterns downtown and open up valuable riverfront land for

3.1 MOLINE CENTRE NEIGHBORHOOD

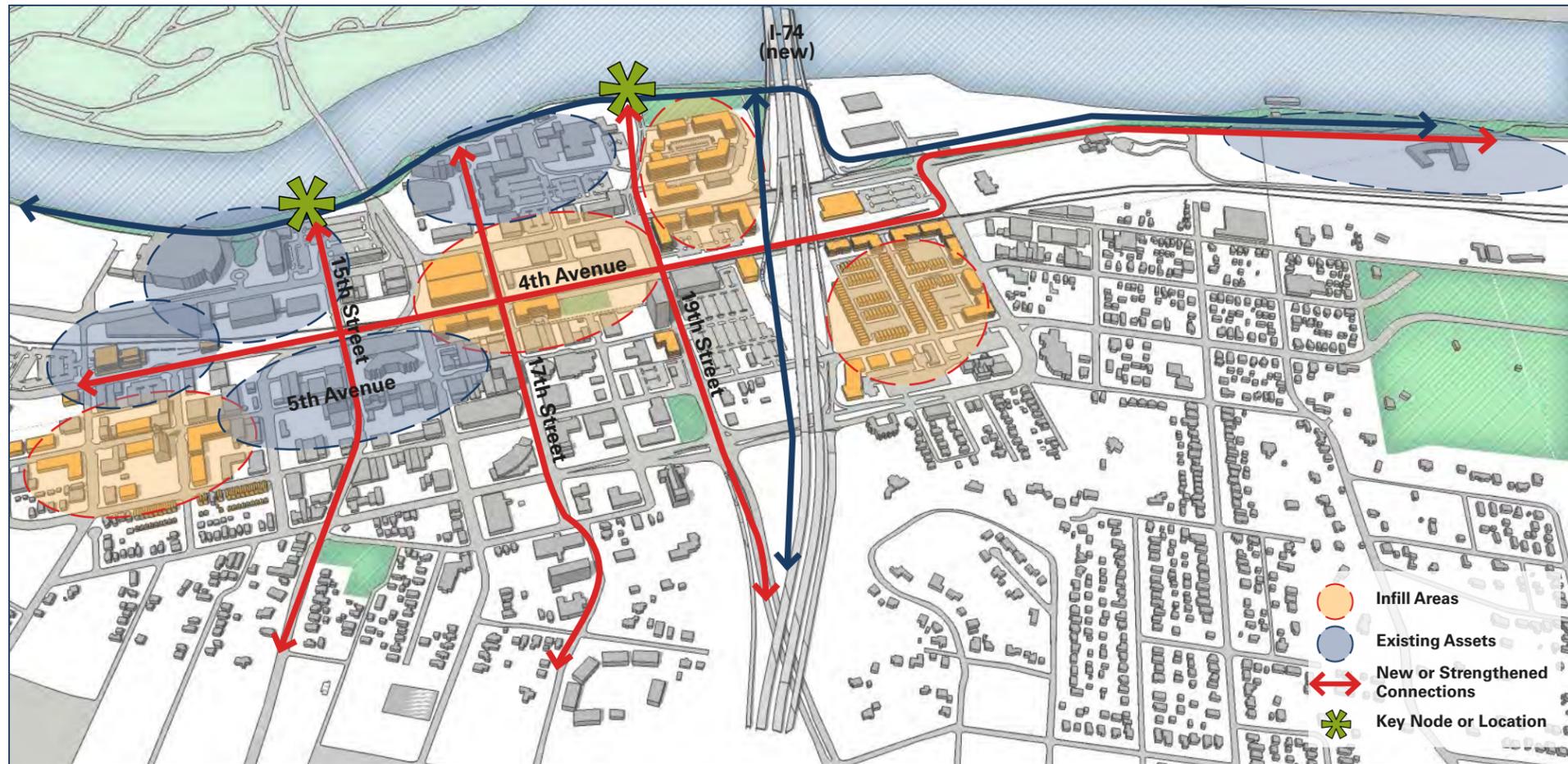


development. Changes include relocation of entrance and exit ramps and termination of 5th Avenue. The impact of these changes are discussed throughout the document.

The Q Multi-Modal Station will introduce passenger rail service to The Quad Cities and create a new entry point into the community. It will increase activity in, and prominence of, the area surrounding the station.

Western Illinois University - Quad Cities Riverfront Campus growth and expansion, including the Riverbend Commons phased development will continue to bring more people and amenities to the area east of I-74.

Residential loft conversion, renovation of storefronts, and growing commercial investment in the Downtown Core is expected to continue, especially adjacent to the recently completed 5th Avenue streetscape.



Opportunities for Moline Centre Neighborhood

Moline Centre Goals

Through integration of transportation, land use planning, and urban design, Moline Centre will continue to grow as a vibrant neighborhood. Moline Centre can thrive as a mixed-use district that promotes a live-work-play approach, extending the activity of the district to evenings and weekends. The heart of Moline is an accessible, desirable destination for residents and visitors alike.

Strategic development, infill and infrastructure improvements will allow the urban environment to become one cohesive, legible environment that is easy to navigate and a pleasure to experience.

Goals for Moline Centre include:

- Build upon existing assets.
- Connect assets through infrastructure enhancements, urban infill and redevelopment.
- Connect to neighborhoods on the bluff.
- Increase awareness of, and enhance connections to, the Mississippi River and the Great River Trail
- Reinforce the Western Gateway, consider the forthcoming Q Multi-Modal Station as the central element to the development.
- Prioritize improvements along north-south connections throughout the neighborhood, linking residential development, the River, downtown businesses, and commercial districts.
- Complete urban fabric through adaptive reuse and infill.
- Activate vacant lots and under utilized parking lots through temporary programming and special events.
- Build a complete pedestrian and bicycle network, and support a multi-modal environment. (See also Chapter 2.0)
- Concentrate development in areas that can act as catalysts for future development.
- Align land uses with vehicle circulation patterns.

3.1 MOLINE CENTRE NEIGHBORHOOD/ DOWNTOWN CORE



Downtown Core Existing Conditions

The Downtown Core spans roughly from 7th Avenue to the south, the River to the north, 15th Street to the west and 19th Street to the east. It is defined by a traditional urban character, walkable blocks, historic commercial buildings and a mix of compatible uses including office, commercial, retail, residential and entertainment. It is comprised of a traditional “Main Street” district along 5th Avenue and 6th Avenue that is characterized by two to four story historic masonry buildings with ground floor retail and office or residential above. Intermingled within the Core are larger buildings such as the Chase Bank building, Heritage Place, and the KONE Center.

Many blocks downtown remain intact and have experienced recent investment including new streetscape enhancements, residential loft conversions and new storefronts. Conversely, other blocks have become dominated by large parking lots and wide streets with fast moving traffic.

The Downtown Core is home to many high-quality older buildings, several of them exist within the boundary of the Moline Historic District. These buildings provide Moline with a wealth of opportunities for adaptive reuse including loft apartments, offices, and retail and community resources. A philosophy of adaptive reuse first can help preserve the scale and character of the existing downtown and promote a type and scale of growth

that is aligned with market demand. In addition to the success of 5th Avenue, the “Historic Block” at 15th Street and River Drive continue to see investment including new residential units, new restaurants, a brew house, coffee shops, office and retail.

Key Assets

This Plan recommends building upon and connecting the key assets and concentrating development around areas that already bring people and energy to the area. Within the Downtown Core activity generators and key assets include two hotels, major employment centers, numerous civic facilities, restaurants, entertainment destinations, and a growing residential community.

Hotels: Existing hotels include the Radisson Hotel and the Stoney Creek Inn. A new boutique hotel is planned as part of The Q Multi-Modal Station.

Employment Center: An estimated 3,000 people work downtown. Major employers include KONE, John Deere and United Healthcare.

Civic Facilities: City Hall, City Annex, Emergency Services Building, County Court, and Post Office.

Shopping and Dining: 5th Avenue is the center of the business district with growth happening throughout the Downtown Core and historic storefronts including the “Historic Block” and Bass Street Landing.



Existing Land Use / Moline Centre Downtown Core (City of Moline GIS 2010)



5th Avenue Commercial Corridor: Image of this traditional Main Street environment.



Carnegie Library: Prime example of Moline’s high quality stock of historic buildings appropriate for adaptive reuse.

3.0 Urban Design & Land Planning

3.1 MOLINE CENTRE NEIGHBORHOOD/ DOWNTOWN CORE



Entertainment: Free summer concerts run on Thursday nights at the Plaza at Bass Street Landing.

John Deere Pavilion and Commons: John Deere Pavilion features an agricultural museum that displays farm implements as well as presentations on the history of farming and the John Deere & Company. The Commons hosts festivals that run throughout the season. There are an estimated 250,000 patrons to the Pavilion and Commons annually.

Housing: Downtown housing development continues to grow, including the Skinner Loft Apartments, the Enterprise Lofts, and the Berglund Flats and Phillips Lofts.

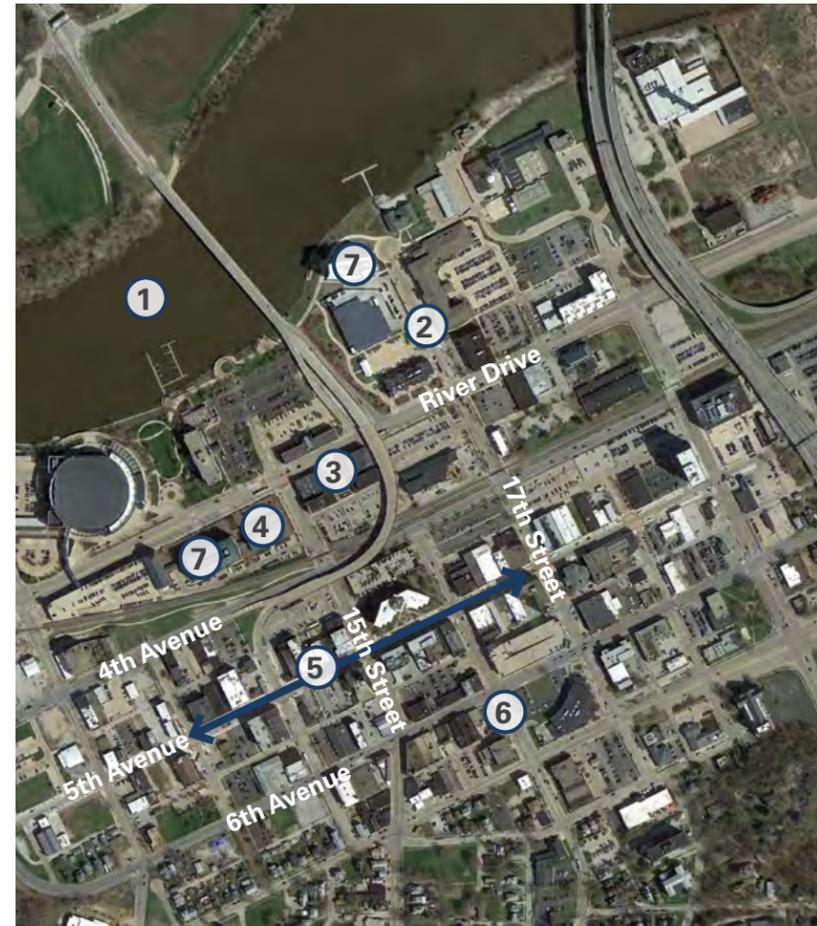
Prior Plans

The 2014 Comprehensive Plan Update identified several key strategies that are still relevant including: adaptive reuse of quality buildings, infill of vacant parcels, gateway enhancements, streetscape improvements, facade improvement, and potential parking lot consolidation and redevelopment. This 2016 Plan builds upon prior streetscape concepts, broadens the vision for key parcels, expands the dialogue for land use, and emphasizes traffic calming. Priority is given to improvements that celebrate Moline's assets by strengthening visibility, connectivity and accessibility by all modes of transportation. It refines the preferred bike network and expands the role of 4th Avenue.

Downtown Core Strategies

The Downtown Core is growing into a thriving district. It can continue on this trajectory by further investing in adaptive reuse of its historic resources, creating more opportunities for live-work environments, celebrating the River, improving pedestrian mobility, and by pursuing events and programs that bring people downtown and create excitement about the district. The Downtown Core is increasingly becoming the place to be. It is a key destination for work and entertainment, and is a desirable place to live. This residential growth, as evident in the ongoing loft development, brings people downtown, and creates energy that becomes the catalyst for future retail development.

Presenting a welcoming and compelling environment for the visitor is key to creating sustained interest in Moline Centre. When a person comes downtown for a specific activity or event and are encouraged to stay longer, retailers and restaurants benefit. Simply stated, people attract people. The more people that are visible and active downtown, the more people want to be downtown. Special events and programming are a great catalyst. According to a recent survey conducted in conjunction with this study, people come to the Downtown Core for a wide variety of reasons but encouraging them to "stay and linger" today can be a challenge due to disconnections in the physical environment and event programming.



Downtown Core Overview

Key Features

1. Mississippi River
2. Bass Street Landing (Stoney Creek Inn, Kone Center, Plaza)
3. Historic Block
4. John Deere Pavilion
5. 5th Avenue Mixed-use Commercial Corridor; Residential Loft Conversions
6. Civic Core (City Hall, Post Office, Police)
7. Employment Centers (United Health Care, KONE, Deere)



Skinner Block: Includes offices, retail, and recent residential loft conversions.



Bass Street Landing: Mixed-use district with offices, hotel, restaurants, a public plaza and connections to the Great River Trail.

3.0 Urban Design & Land Planning

3.1 MOLINE CENTRE NEIGHBORHOOD/ DOWNTOWN CORE



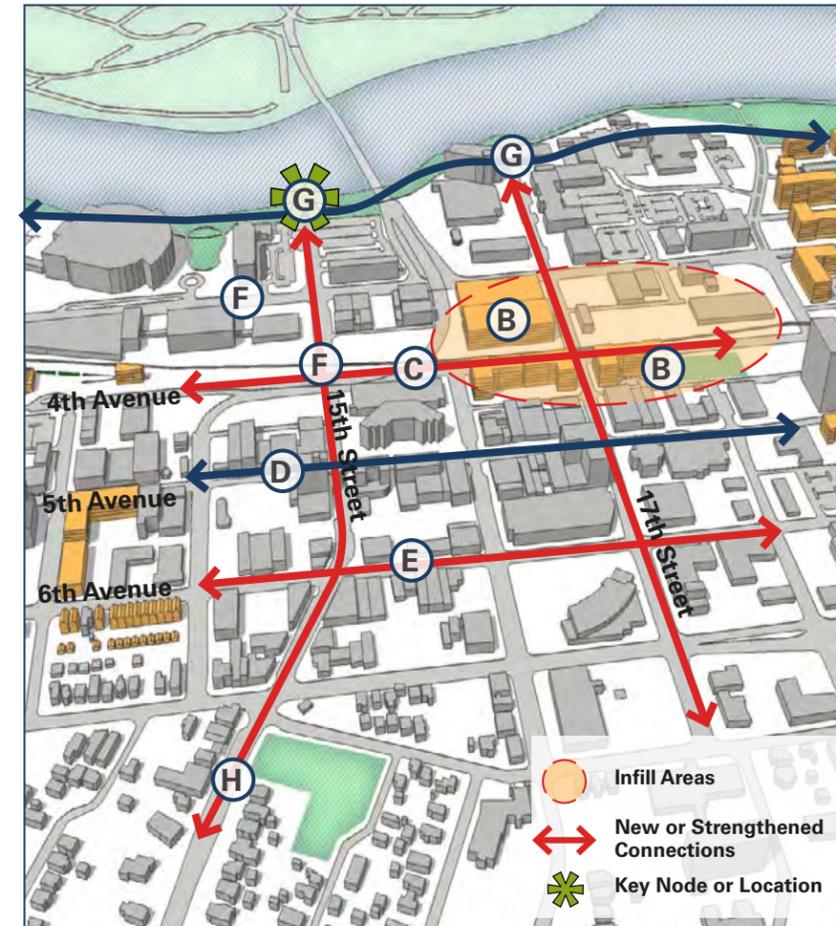
4th Avenue, the rail road tracks, vacant land and large parking lots all create perceived distances that make walking from one activity to another activity feel daunting even if the physical distance between the two is minimal. And uncertainty in parking availability makes it difficult to drive between venues. Chapter 2.0 presents strategies for improving the parking experience and access.

Many cities today are capitalizing on the growing tech industry paired with the start-up culture and the return of the desire to live in walkable urban environments. The Mississippi River Urban Technology Corridor study (2006) identified the growing trend in technology companies and jobs. Since that plan, Moline has seen growth in the start-up culture building on its manufacturing heritage with things like the Quad Cities Manufacturing Innovation Hub sponsored by the Quad Cities Chamber and the Quad City Manufacturing Lab at the Rock Island Arsenal. Paring the tech trend (co-working, tech incubators, and maker spaces) with vacant buildings or underutilized spaces in the Downtown Core presents a great opportunity. The City of Moline can work with current building owners, especially those with vacant commercial space, to encourage creation of co-working spaces, startup culture and incubator projects. Further, the synergy of stakeholders in the community from traditional manufacturing, to educational institutions present the perfect opportunity for supporting an incubator culture.

The City of Moline can continue to incentivize building owners to invest in their properties through incentives such as the Storefront Restoration Grant for Small Businesses, Traditional Commercial Areas Rejuvenation Program, and TIF Financing. The City of Moline should continue to actively work with the QC Chamber to actively recruit business to down-town.

Moline can activate the Downtown Core with special events such as farmer’s markets, festivals, special theme weekends, etc. Local merchants and the Moline Center Main Street Program team can work together to cross promote events and establish incentive programs. Promotions could include discount coupons for dinner or a hotel stay when you attend an event or a loyalty campaign with weekly Moline Center “Night Out” events. Moline can further activate the streets by working with local groups and building owners to plan “pop up shops” in vacant store fronts. These actives can be planned in connection with other events downtown and can be a great cold weather opportunity when outdoor events are not an option.

Active ground level uses are critical to preserving active, welcoming streets. Future development needs to ensure active ground floor uses that engage the street. Particular attention should be paid to developments between 15th / 17th Streets and 4th Ave and River Drive to help bridge the space between the area south of 4th Avenue and the development north of the rail line.



Opportunities for the Downtown Core

Strategies

- A. Activate the Downtown Core with special events, programming, and pop up events.
- B. Pursue long term strategy to convert parking lots to development when existing parking demands can be consolidated.
- C. Re-imagine 4th Avenue as a complete street with bike and pedestrian amenities with adjacent new commercial mixed-use development.
- D. Continue to promote adaptive reuse, facade improvements, and infill along 5th Avenue.
- E. Implement 6th Avenue downtown streetscape.
- F. Mitigate impact of truck traffic on River Drive and freight rail traffic on along 4th Avenue. Enhance pedestrian crossings.
- G. Connect directly to the River through new open space design and sidewalks.
- H. Connect to neighborhoods up the bluff



Special Events Examples: Ice Skating



Special Events Examples: Car Shows

3.1 MOLINE CENTRE NEIGHBORHOOD/ DOWNTOWN CORE



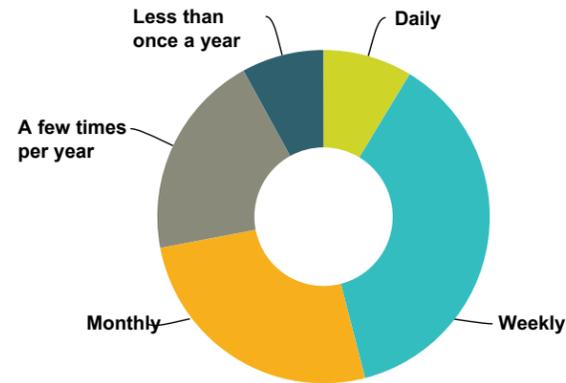
Activate Downtown

Nearly two-thirds of our survey respondents stated that they visit Moline Centre weekly or monthly. Dining was the most popular activity in Moline Centre while many respondents would like more opportunities for shopping. Farmer's markets and outdoor performances top the list of special events respondents would like to see downtown. Together, dining, shopping and special events have the capacity to create a lively downtown district.

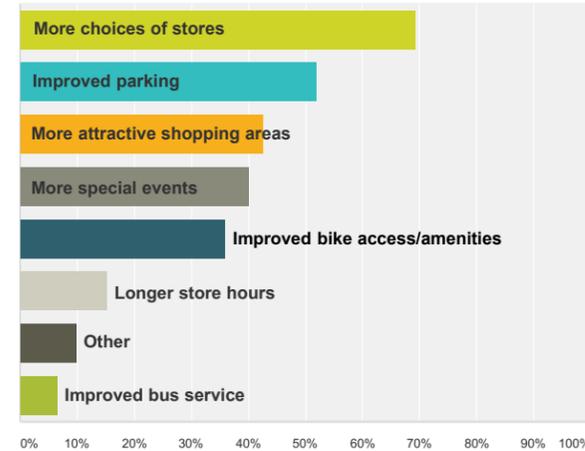
Many events can take place within the current downtown environment on parking lots, through temporary street closures, or within existing open spaces. Temporary events are a great way to showcase the city and to test the potential for more permanent events at these locations in the future.

Over 185 community members responded to a community wide survey seeking input and guidance on the vision and direction of recommendations as part of the 2016 Comprehensive Plan Update. The charts presented here highlight the questions asked about Moline Centre.

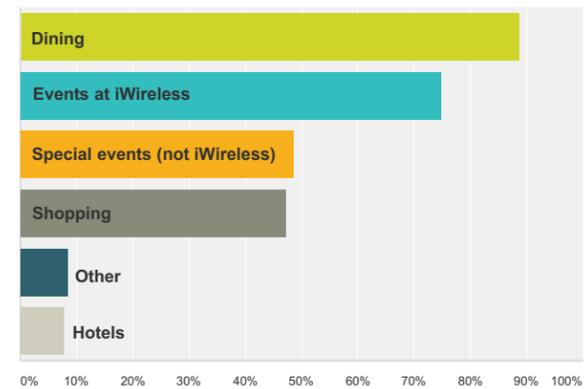
How often do you visit Moline Centre for activities other than work/school?



What would increase your interest in Moline Centre?



What types of leisure activities do you attend in Moline Centre?



What types of special events would you like to see downtown?



Special Events Examples: Food Trucks



Special Events Examples: Farmer's Market



Special Events Examples: Kayaking



Special Events Examples: Food & Wine Weekend

3.1 MOLINE CENTRE NEIGHBORHOOD/ DOWNTOWN CORE



The City of Moline, the QC Chamber, the Moline Center Main Street Program, along with local building owners, businesses, and institutions can work together to encourage a business start-up culture that promotes activation of under utilized spaces, supports new business development, and encourages talent to remain in Moline. Here are a few case studies that illustrates the range of possibilities from co-working spaces to fully funded incubators.

Slingshot Co-Work, Normal, IL

Slingshot CoWork is a space for the startup community. It consists of a workspace for independent workers like entrepreneurs, freelancers, remote employees, consultants, and small business owners with weekly meetings, workshops and events that bring people together.

In order to make a long term and sustainable startup community, Slingshot CoWork has partnered with The Bloomington Normal Investor Network.

Slingshot Co-Work offers different types of memberships depending on the program. It leases its space for a nominal amount from the City and is located in an older store front in a traditional walkable retail district near the Amtrak Station.



Startup Peoria, Peoria, IL

The Nest is the coworking space that is shared by designers, developers, entrepreneurs, intrapreneurs, business investors, etc. in an office environment, that provides the infrastructure, flexibility and connectivity. The Nest also provides training and mentorship, bringing in people with experience in entrepreneurship, business, and service.

The Nest is supported by The Greater Peoria Economic Development Council, which is a non-profit organization governed by people from both public and private sector. Other collaborators include Illinois Small Business Development Center at Bradley University.

1871, Chicago, IL

1871 is a co-working space for digital entrepreneurs, located in the historic Merchandise Mart building in Chicago. This non-profit hub provides Chicago startups with programming, mentoring, educational resources, and access to possible investors and successful entrepreneurs.

Members rent their own desk and storage with access to all conference rooms. 1871 is supported by The Chicagoland Entrepreneurial Center (CEC). The CEC is a non-profit organization focused on promotion and growth of startups in Chicago and is funded by private corporations and entities including other successful entrepreneurs.



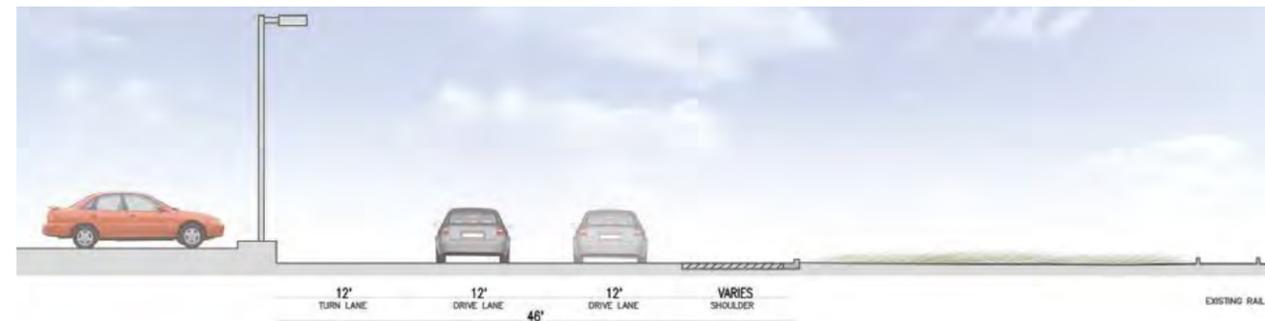
3.1 MOLINE CENTRE NEIGHBORHOOD/ DOWNTOWN CORE



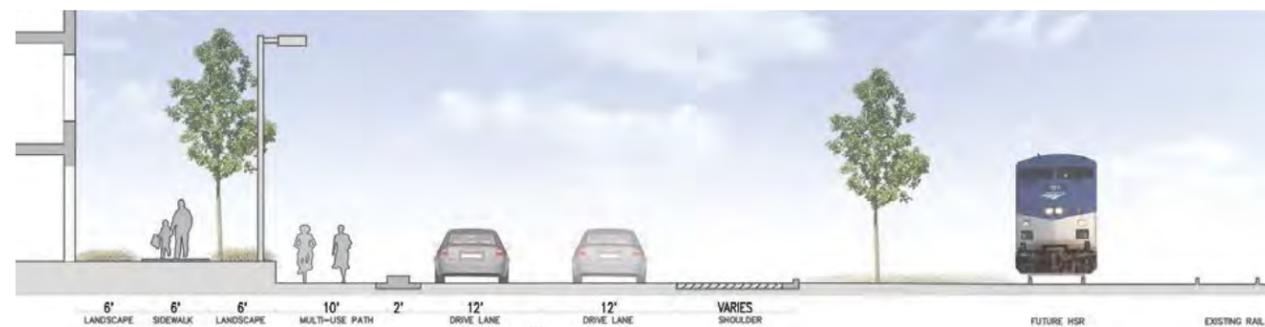
4th Avenue Strategies through the Downtown Core

4th Avenue connects The Q Multi-Modal Station, the Downtown Core and neighborhoods east of I-74. Upon completion of the new I-74 Bridge, 4th Avenue will be the only street that connects the east and west sides of Moline Centre that is not impacted by on and off ramps. In particular all pedestrian paths will have to cross these ramp access points except on 4th Avenue. As a result 4th Avenue has the potential to become something other than a fast moving one-way street that provides access to parking lots. Introduction of a new multi-use path will create a slower more “complete” street and

allow for pedestrian and bicycle access throughout Moline Centre. See Chapter 2.0 for more on the role of 4th Avenue and the Multi-Use Path. Through future consolidation of surface parking lots (as identified in the 2014 Comprehensive Plan Update), improving mobility and improving the character of 4th Avenue, the parcels on the south side of the roadway can become viable development parcels. Filling in the gaps in the urban fabric between the 5th Avenue Downtown Core and the Bass Street Landing development north of 4th Avenue will connect the two areas into one larger walkable district.



Existing 4th Avenue: Current configuration includes block long dedicated left turn lanes with large parking lots abutting the roadway.

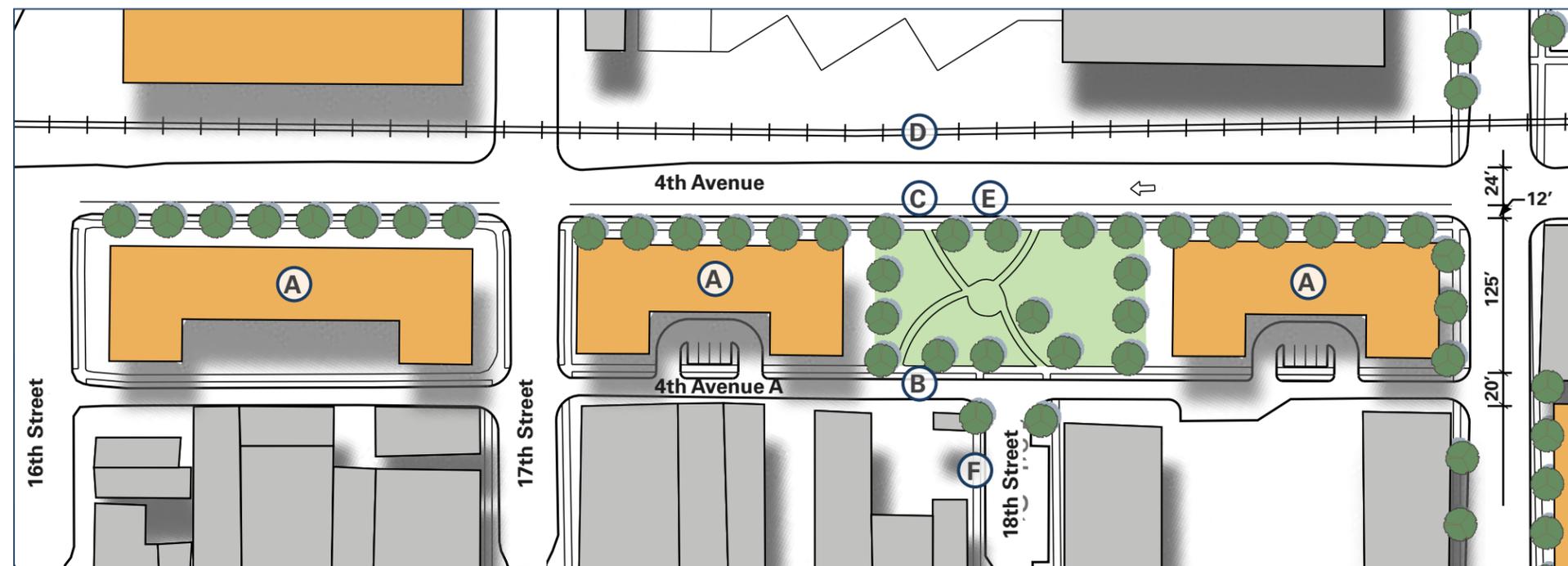


Vision for 4th Avenue: Introduction of the Multi-Use Path and an enhanced landscape can result in viable development parcels.

Recommendations

Re-imagine 4th Avenue as a complete street with bike and pedestrian amenities and new commercial mixed-use development

- A. Consolidate surface parking lots to allow for new development parcels
- B. Narrow 4th Avenue A to allow deeper development parcels
- C. Introduce streetscape with sidewalks and trees along 4th Avenue
- D. Preserve railroad right of way for future passenger rail line. Enhance landscape.
- E. Create east-west pedestrian connections along 4th Avenue
- F. Connect 18th Street to new development through enhanced landscape



4th Avenue Redevelopment Opportunities: Reconfigure parcels along south side of 4th Avenue to allow for redevelopment.

3.1 MOLINE CENTRE NEIGHBORHOOD/ DOWNTOWN CORE

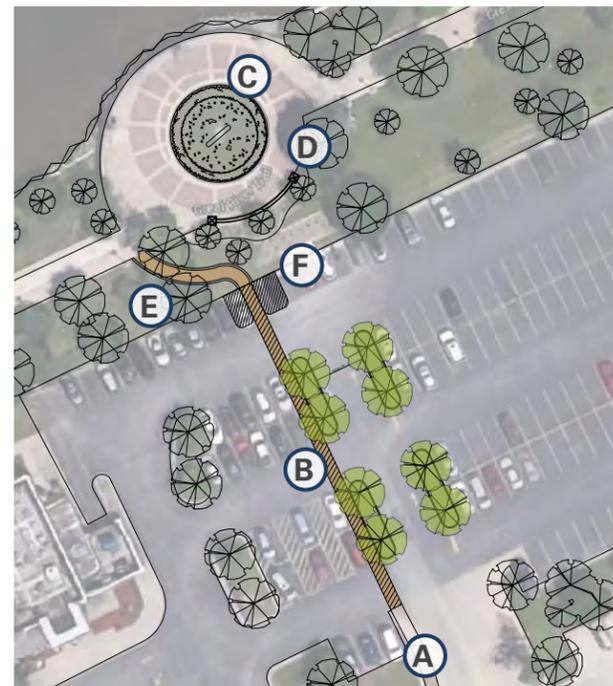
15th Street Streetscape Strategies: Connect directly to River and Great River Trail

15th Street streetscape was identified as a priority in the 2014 Comprehensive Plan Update. This Plan advances the recommendation that the 15th street pedestrian environment be extended all the way to the River, creating a direct access point through the existing Radisson parking lot. This enhanced connection will link the River to the heart of 5th Avenue and to major tourist destinations such as the John Deere Pavilion. This connection will also improve visibility and access to the TGI Friday's restaurant, and improve parking and pedestrian circulation to the iWireless Center during special events with minimal impact to total number of parking spaces.

- A. Install and widen sidewalks from River Drive to the River
- B. Add delineated pedestrian path through parking lot and new planted islands
- C. Introduce sculptural element in landscape bed. Element should have height and visual impact from River Drive and provide wayfinding to the riverfront
- D. Preserve existing seat wall
- E. Create new path connection to Great River Trail, coordinate grade to meet ADA requirements, preserve berm on riverside for flood protection
- F. Remove adjacent parking stalls to create a more visible trail entrance



15th Street: Strengthen the pedestrian connection to River



Pedestrian Path: Create a visible and clear pedestrian path with landscape in the Radisson parking lot



15th Street Connections: Link 5th Ave and Deere Pavilion directly to River through streetscape improvements



View From River Drive: The Mississippi River itself, and access to the River, is not visible



Pedestrian Path: Existing walkway is very narrow and should be widened.



Great River Trail: Landscape berm prevents pedestrian and bike access from trail to downtown and adjacent amenities

3.1 MOLINE CENTRE NEIGHBORHOOD/ DOWNTOWN CORE



Downtown Core Streetscape

Themes from prior documents and studies remain relevant and include calming traffic, enhancing the pedestrian environment, and prioritizing implementation. The success of the recent 5th Avenue Streetscape improvements provides the City with the confidence it needs to pursue upgrades throughout the district. The City of Moline should continue to prioritize streetscape improvements that connect pedestrians to key assets and destinations such as the River, connecting the north and south sides of the district across the rail line, and improving commercial viability of downtown businesses. Strategies include:

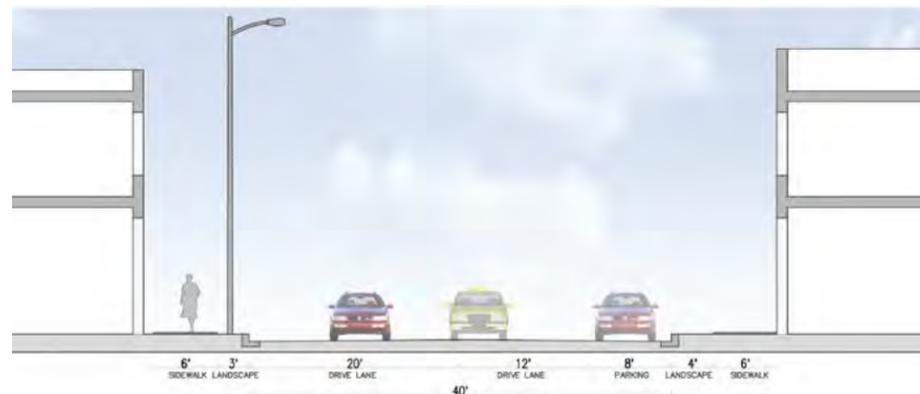
- Improve pedestrian crossings at all streets. Priority intersections include: 4th Avenue and 15th Street; 4th Avenue at 17th Street; River Drive at 17th Street. (See Chapter 2.0)
- Improvements to 6th Avenue through the Downtown Core as illustrated in the image to the right can promote walkability, strengthen connectivity to the area, and improve access to existing and future store fronts. A robust streetscape improvement program would add vitality and encourage pedestrian activity. Recommendations include the potential to narrow roadways, widen sidewalks, increase landscape and add street trees. The 2005 Downtown Streetscape Master Plan and the 2014 Comprehensive Plan update recommend 6th Avenue between 14th Street and 16th Street as a high priority streetscape zone.



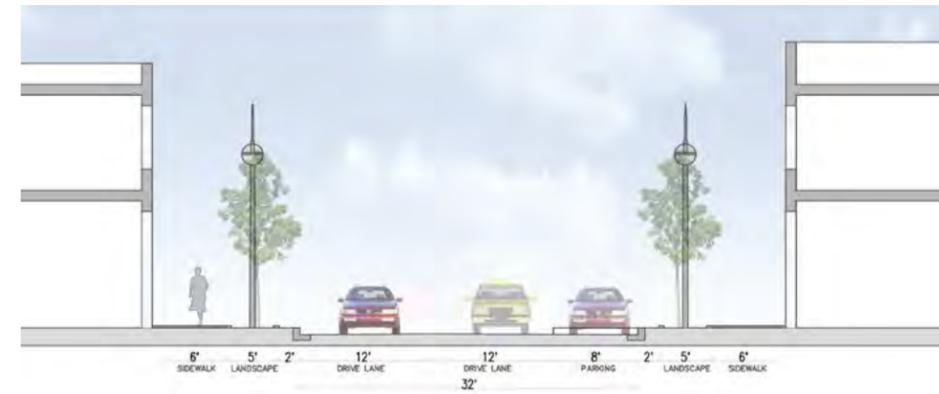
Existing 6th Avenue in Downtown Core: Wide drive aisles with narrow sidewalk, frequent curb cuts, and no landscape



Vision for 6th Avenue in Downtown Core: Narrower drive aisles with widened sidewalks and new streetscape, with new infill development.



Existing 6th Avenue in Downtown Core: Wide drive aisles with narrow sidewalk, frequent curb cuts, and no landscape



Vision for 6th Avenue in Downtown Core: Narrower drive aisles with widened sidewalks and new streetscape

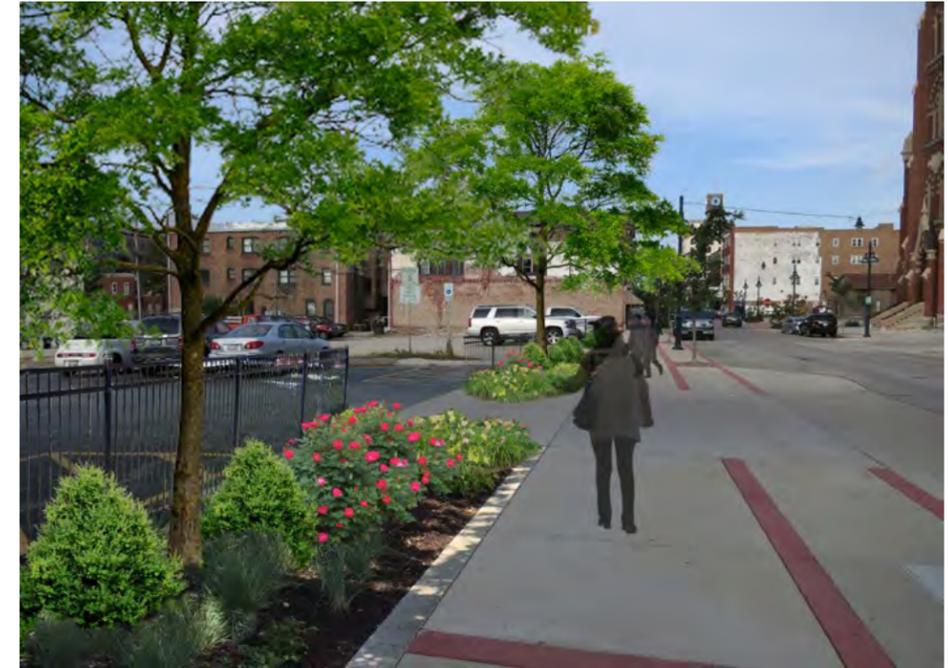
Downtown Core Parking Lot Streetscape

The City of Moline should develop specific parking lot landscape design guidelines for Moline Centre that minimize the negative impact of large parking lots on the pedestrian environment. Additional landscape guidelines can be incorporated to mitigate negative environmental impacts of large parking lots. Negative impacts include, increased and uncontrolled stormwater runoff, urban heat island and reduction of habitat for animals. Design guidelines may include:

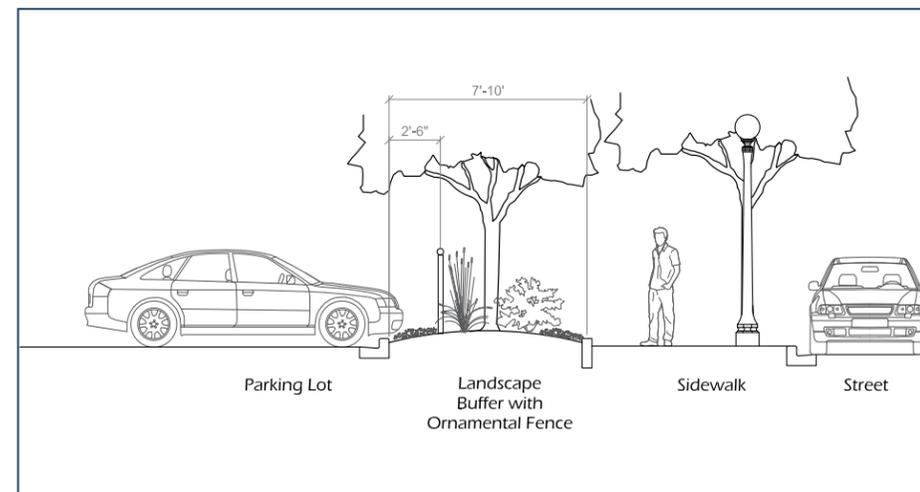
- Landscaped buffers between sidewalks and parking lots. These buffers should include canopy trees if the adjacent streetscape does not already include street trees and sufficient space is available for proper planting.
- Plants selected for the landscape should be native to Moline, should not be invasive species, and should not require excessive watering or maintenance.
- Landscapes can be designed collect and filter rainwater
- Landscape islands with canopy trees within the parking lots
- Incorporating permeable pavements that reduce storm water runoff



Existing Parking Lot: No buffer between parking lot and pedestrian



Parking Lot Landscape Vision: Landscape buffer creates inviting pedestrian environment



Prototypical Design: Parking lot screening in urban environments such as Moline Centre

Recommendations

In order to provide a favorable impression, parking lots should be screened from adjacent streets and sidewalk with low ornamental fencing. The following landscape is recommended for the perimeters of parking lots in both residential and commercial districts.

- Perimeter Landscape: Provide an additional 2' - 6" overhang space at parking stalls
- Continuous hedge 36"-48" tall.
- Shade or ornamental trees planted 25' on center
- Install 4' tall ornamental fence along parking lot perimeter with a 4 foot wide planing zone between the sidewalk and the fence.

3.1 MOLINE CENTRE NEIGHBORHOOD/ WESTERN GATEWAY



Western Gateway Existing Conditions

The Western Gateway is positioned between the predominantly residential neighborhood of Florenciente to the west and the Downtown Core to the east. The Western Gateway is home to a number of Moline's greatest assets, which attract visitors from throughout the region. Today this area lacks cohesion and the ability to capitalize on the synergy between these components.

- Centre Station: The transit hub is served by five transportation carriers, including local/regional bus, paratransit, intercity bus, and the nearby water taxi. It is used by 700,000 persons annually.
- The Q Multi-Modal Station, once completed, will bring regional high-speed passenger rail service to the Quad Cities with a direct connection to Chicago. The redevelopment of the historic building will also include a 95 room boutique hotel.

- iWireless Center/Civic Center: This 12,000 seat indoor arena serves the entire Quad City area hosting professional hockey, concerts and conventions with more than 200 annual events and an 650,000 estimated annual patrons.
- Community service providers include the Boys and Girls Club and Community Health Care, Inc.
- Velie Park is on the southern end of the area, however pedestrian access is limited by lack of sidewalks and a steep bluff.
- Many quality buildings remain. Some have been recently converted to loft condominium buildings while others are currently vacant or are in need of improvement. These buildings are important as they provide a sense of scale and character.
- The area suffers from a high level of vacant parcels and parking lots.



Existing Land Use / Moline Centre Western Gateway (City of Moline GIS 2010)

Land Use

- Commercial
- Mixed-Use
- Heavy Industrial
- Light Industrial
- Office
- Parking
- Open Space
- Institutional/Entertainment
- Low Density Residential
- Multi Family Residential
- Vacant
- Historic District Boundary

Source: City of Moline GIS data 2010



Boys and Girls Club: Teen Center



14th/16th Street Arsenal Ramp: Creates pedestrian barriers and impacts site circulation and wayfinding



13th Street: Looking south from 4th Avenue



Rendering of the new Q Multi-Modal Station

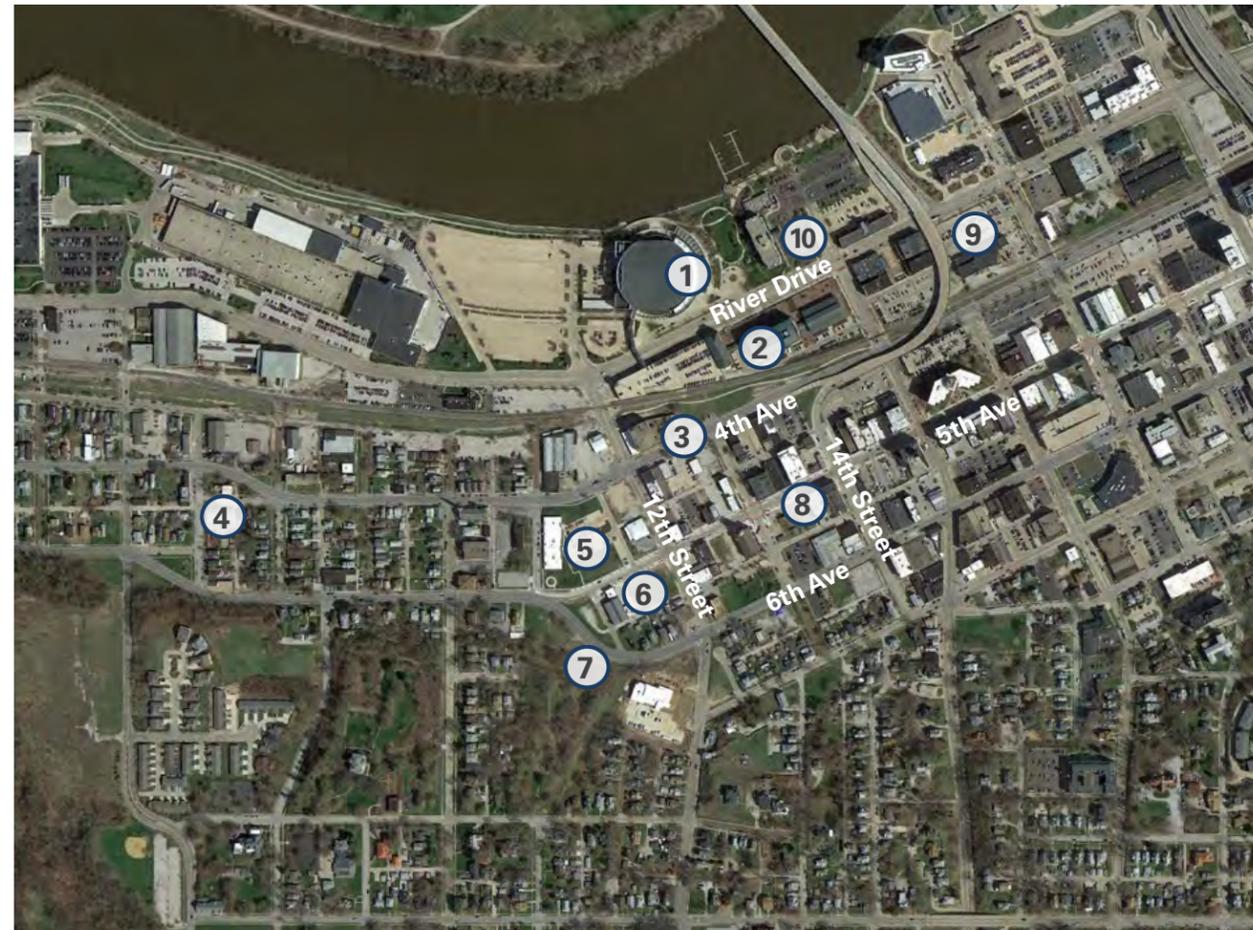
3.1 MOLINE CENTRE NEIGHBORHOOD/ WESTERN GATEWAY



Western Gateway Prior Plans

Prior plans have identified this area as a key gateway and have envisioned mixed-use development that builds upon existing and future transit assets. Many of the prior recommendations are still applicable today. Two key previous planning studies are:

- Quad Cities TOD + Intermodal Plan (2009) outlined a vision for infill mixed-use transit oriented development responding to market demands, in keeping with the downtown scale and character of the area.
- 2014 Comprehensive Plan Update builds upon prior recommendations and introduces the idea of adding more sporting or entertainment venues downtown.



Western Gateway Overview

Key Features

1. iWireless Center
2. Centre Station
3. The Q Multi-Modal Station
4. Boys and Girls Club
5. Community Health Care, Inc
6. Boys and Girls Club Teen Center
7. Velie Park
8. 5th Avenue Mixed-Use Commercial Corridor / Residential Loft Conversions
9. 16th Street / Arsenal Bridge ramp
10. Radisson Hotel



1201 5th Avenue: This 1845 Italianate anchor block building is the oldest in the district.



1404 6th Avenue: Architectural detail on the 1910 Fraternal Order of Eagles No 1112 building at



Community Health Care, Inc



iWireless Center, Moline's Civic Center

3.1 MOLINE CENTRE NEIGHBORHOOD/ WESTERN GATEWAY

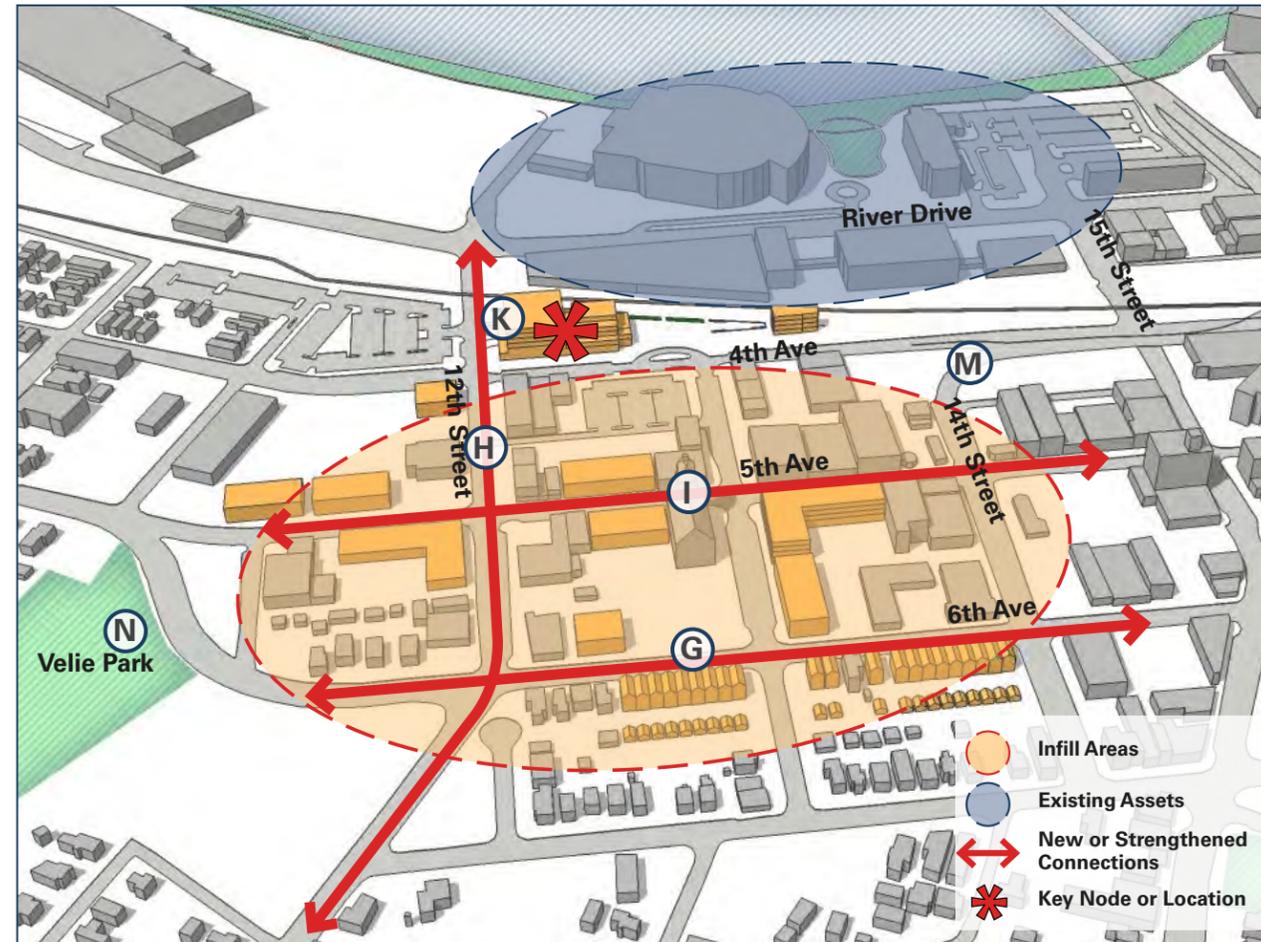


Western Gateway Strategies

A logical extension of the growing Downtown Core, the Western Gateway has the potential to become a thriving area. Today, the area is already home to many elements that contribute to a vibrant urban district and as a result have experienced some recent reinvestment. With the addition of the future Q Multi-Modal Station, the area is poised to become a successful transit oriented development (TOD) by weaving together the urban fabric with enhanced streetscape and infill development. However, large gaps in the urban fabric detract from the area making it fragmented and lacking in clear identity. The City should place a high priority on infill of existing vacant lots and renovation of existing high quality buildings. Particular attention should be paid to buildings identified as contributing properties within the Downtown Commercial Historic Core.

Redevelopment along 6th Avenue in the Western Gateway area presents a unique opportunity to knit together the advantages of urban living within a more traditional residential neighborhood. Located two blocks from the thriving 5th Avenue Corridor and adjacent to residential developments to the south, nearby parks, and three blocks to transit, this section of 6th Avenue is well suited for residential development such as townhomes. Improving the streetscape by widening sidewalks, adding street trees, and slowing down traffic will further strengthen and attract development to this area.

- Reinforce and strengthen connections between current assets such as the iWireless Center, the River, 5th Avenue commercial corridor and parks, the neighborhoods up the bluff, and future assets such as The Q Multi-Modal Station, through enhanced streetscape and strategic infill.
- Prioritize pedestrians and bicyclists, while preserving functionality for trucks and cars. (See Chapter 2.0 for more details.)
- Respect character of existing neighborhood fabric. Future buildings should be similar in scale to existing buildings and should face the street with parking lots behind or next to buildings.
- Create a mix of walkable, active uses near transit and connect to the 5th Avenue corridor.
- Continue to pursue commercial development along 12th Street. Work with local land owners and developers to pursue community serving commercial and retail opportunities that build upon existing land uses.
- Pursue housing opportunities through reuse of existing buildings and infill of vacant lots. Future housing on 5th Avenue should include ground floor retail. 6th Avenue would be ideal for low to medium density residential such as townhomes.



Opportunities for the Western Gateway

Key Recommendations	
A. Infill vacant lots and explore redevelopment opportunities for soft sites.	J. Buildings should face streets with complimentary ground floor uses, creating an active urban street.
B. Maintain and renovate existing buildings .	K. Improve access and wayfinding, especially from The Q Multi-Modal Station to the 5th Avenue commercial corridor and the riverfront.
C. Focus development around new multi-modal station to increased activity and visibility of the area.	L. Strengthen character of pedestrian environment, mitigate obstacles to pedestrian circulation and increase way finding signage.
F. Preserve scale and character of existing neighborhood development.	M. 14th Street and 15th Street streetscape enhancements. Consider strategies to mitigate obstacles crated by the Arsenal bridge and ramp, and the new train tracks for the Multi-Modal Station.
G. Reinforce residential nature of 6th Ave.	N. Strengthen connection to Velie Park and neighborhoods up the bluff
H. Strengthen 12th Street as a north/south street by promoting infill.	
I. Grow 5th Ave mixed-use commercial corridor.	

3.1 MOLINE CENTRE NEIGHBORHOOD/ WESTERN GATEWAY



- Western Gateway Recommendations**
- A. Improve pedestrian crossings over the rail line at 12th Street.
 - B. Provide pedestrian landscape buffer for future Q Station parking lot.
 - C. Pursue neighborhood serving commercial and retail opportunities that build upon existing land uses. Buildings should face the street with parking lots, behind or next to buildings. Opportunities may include: pharmacy; mercado, sports venue and practice facilities.
 - D. Strengthen link between The Q Multi-Modal Station and 5th Avenue through improved streetscape and signage along 4th Avenue, 13th Street and 14th Street.
 - E. Promote adaptive reuse of existing buildings, and infill on vacant lots.
 - F. Create pedestrian friendly streetscape, consider an appropriate scaled roadway, wide sidewalks, and street trees.
 - G. Develop urban style townhouses along 6th Avenue, minimize curb cuts by maintaining garage access from existing alleys.
 - H. Improve pedestrian connection to Velie Park, increase landscape buffer and sidewalks along roadway. Activate park with new playground along 6th Ave. Install new pathway inside park. Coordinate with potential two-way street conversion in Floreciente. (See Chapter 2.0.)



Opportunities for the Western Gateway



Chicago, IL: Townhomes example (Roscoe St & Ravenswood Ave)



Bloomington/Normal, IL: Amtrak station integrated to urban environment with public plaza and shops



Mixed-use Development: Example of mixed-use development designed to preserve a pedestrian scale environment

3.1 MOLINE CENTRE NEIGHBORHOOD/ WESTERN GATEWAY



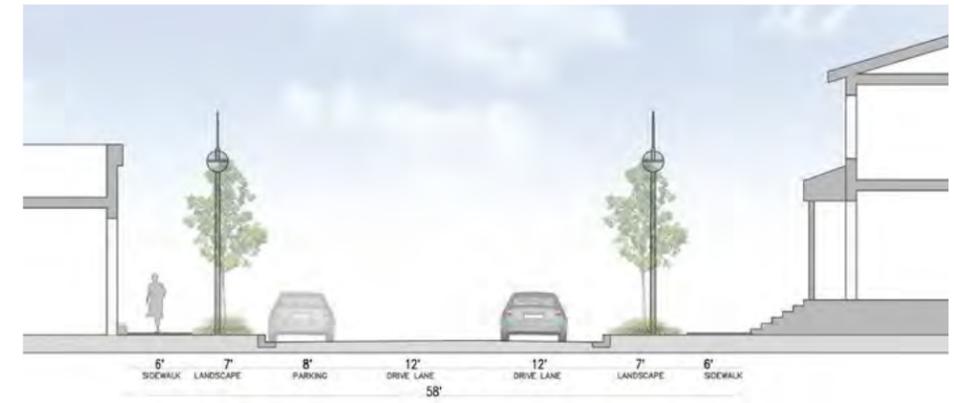
6th Avenue Recommendations

Presented to the right are a series of recommendations that would transform 6th Avenue from a fast moving vehicular thoroughfare into an urban street that completes the southern end of Moline Center in the Western Gateway district.

- Create a walkable neighborhood by upgrading pedestrian street crossings, widening sidewalks and adding landscaped parkways.
- Attached single family homes create a transition to growing loft conversion developments in the Downtown Core.
- 6th Avenue is part of IL-92 and will likely remain a one-way street for the near term. The City of Moline should implement traffic calming and conduct engineering studies to evaluate the possibility of reducing width of traffic lanes.



Existing 6th Avenue near 13th Street: Extra wide lanes, narrow sidewalks and missing streetscape



Vision for 6th Avenue in the Western Gateway area: 12' drive aisles, on-street parking adjacent to retail, widened sidewalks and introduce streetscape

3.1 MOLINE CENTRE NEIGHBORHOOD/ EASTERN GATEWAY



Eastern Gateway Existing Conditions

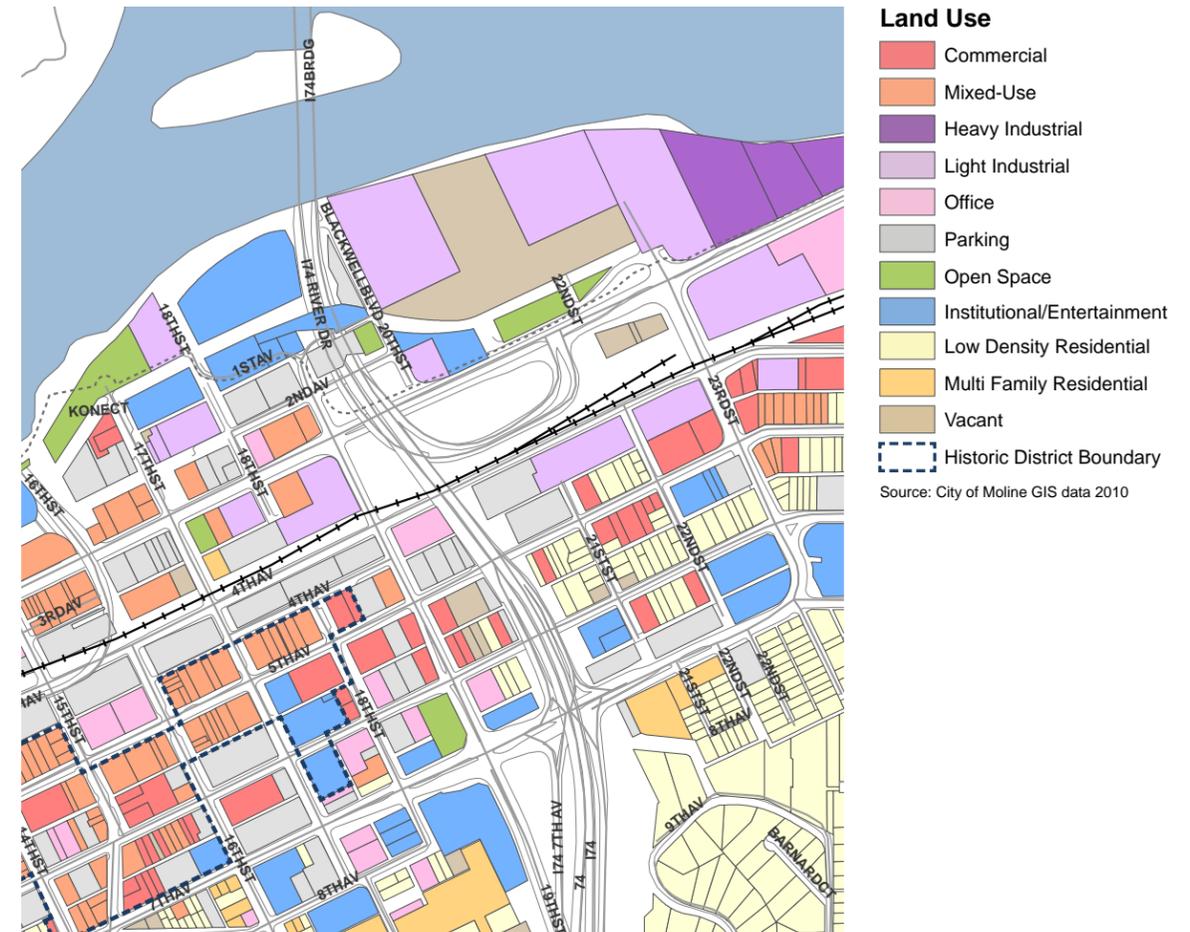
The Eastern Gateway is defined by the area around I-74, and is a primary entrance point into the Downtown Core from the west and from I-74. This area, in general, lacks definition and exists between two better defined sub-districts, the Downtown Core and the emerging WIU-QC District. This area will be significantly impacted by the construction of the new I-74 bridge over the Mississippi River. East of I-74, residential, commercial and industrial uses are intermingled without a sense of cohesion. Auto oriented uses result in excessive curb cuts that diminish the pedestrian environment. West of I-74, the area south of 4th Avenue is defined by the John Deere IT facility and associated parking lots. To the north of 4th Avenue, the current bridge and industrial parcels occupy the land. The Great River Trail moves away from the River and winds through the industrial area. A large portion of this area will become available to redevelopment once the new bridge is completed.

The construction of the new I-74 bridge and removal of the existing one provides Moline with a unique opportunity to embrace the River as a key asset and reorganize development and traffic patterns to support new residential and mixed-use development. The completion of new bridge is scheduled for 2020 with demolition of the old bridge in 2021.

Eastern Gateway Prior Plans

The Mississippi River Urban Technology Corridor Study (2006) envisioned areas east of the I-74 bridge as expansion of the RiverTech district and the area near 19th Street as an IT business zone building upon the Deere IT facility and existing infrastructure.

The River to River Corridor Study (2007) introduced the idea for an urban pedestrian/bike trail connecting the Mississippi River to the Rock River. The Plan recommends routing the trail through the right-of-way vacated by the I-74 project, creating a new road and crossing at the intersection of 20th Street and 4th Avenue. Today, an alternative alignment is proposed due to limited access to land adjacent to the bridge and difficulty in creating a new intersection and rail crossing at 20th Street, because of a second rail line being introduced for the high speed rail.



- Land Use**
- Commercial
 - Mixed-Use
 - Heavy Industrial
 - Light Industrial
 - Office
 - Parking
 - Open Space
 - Institutional/Entertainment
 - Low Density Residential
 - Multi Family Residential
 - Vacant
 - Historic District Boundary
- Source: City of Moline GIS data 2010

Existing Land Use / Moline Centre Eastern Gateway (City of Moline GIS 2010)



19th Street: New John Deere parking lot



19th Street at River Drive: Enterprise Lofts



Existing I-74 Bridge north of River Drive: Vacant, former KONE facility in background

3.1 MOLINE CENTRE NEIGHBORHOOD/ EASTERN GATEWAY



Eastern Gateway Prior Plans (cont.)

The River Drive Corridor Concept Plan (2009) identifies the area west of the bridge as an extension of the Downtown Core and identifies the area east of the bridge as a potential technology corridor or for expansion of the WIU-QC riverfront campus as a neighborhood center.

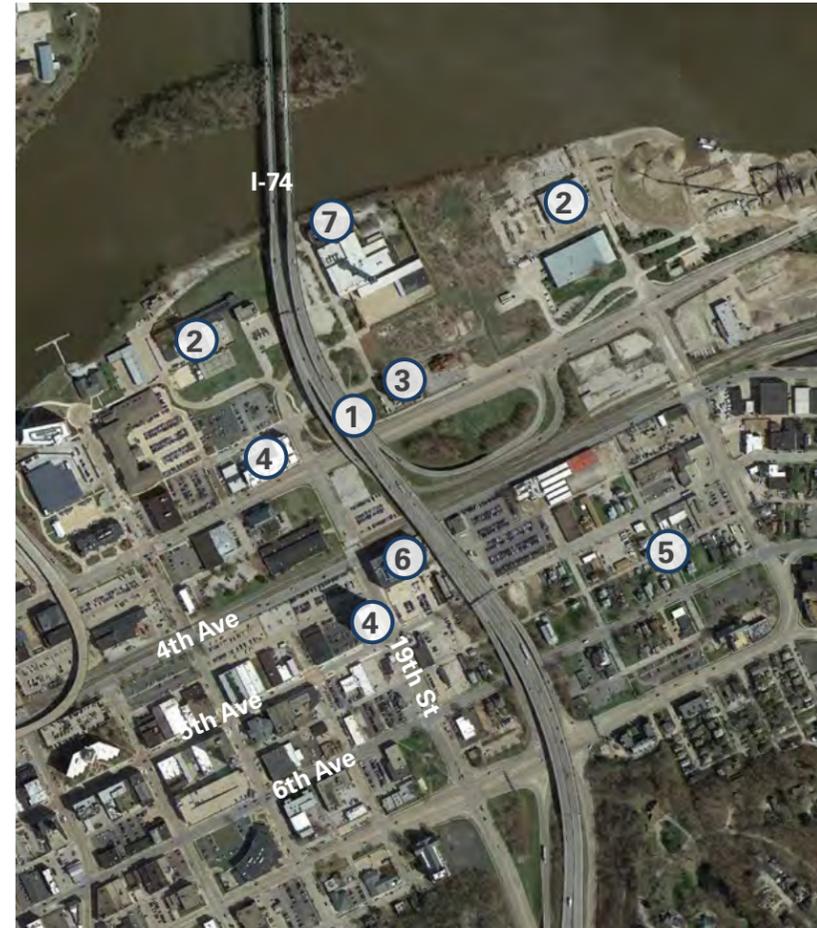
The 2014 Comprehensive Plan Update identifies the area west of the new bridge as either a commercial/office district or a medium density housing development and introduces a new park. The current Plan identifies the area east of the bridge as low to medium density residential.

Today, the technology sector is growing in the Downtown Core, the WIU-QC campus town is growing closer to campus with the RiverBend Commons development and market indicators suggest a modest demand for new office, especially speculative development. The greatest opportunities for this area are to strengthen physical connectivity for pedestrians, bike, car and transit to adjacent districts; bring residential development close to the Downtown Core; and provide amenities that make downtown living attractive.

Eastern Gateway Strategies

Redevelopment of the Eastern Gateway has the potential to transform an area that has long lacked identity and sense of place. Removal of the old I-74 bridge presents unique opportunities to reengage with the River in new and meaningful ways. The rerouting of vehicles through the area and the potential to introduce a multi-use path on 4th Avenue add increased visibility and access to these neighborhoods. Thoughtful and organized development can create a sense of place and identity for these neighborhoods.

A new generously sized riverfront park can create a public space that expands and enhances the downtown experience for visitors and residents. Residents should work together with the City to plan this new park. Themes can include programmable space for community events and areas that provide stormwater and ecologic benefits.



Eastern Gateway Overview

Key Features

1. I-74 roadway, bridge, entrance and exit ramps
2. Riverfront industrial parcels (Riverstone, Moline City Water Department)
3. Spiegel Warehouse
4. Multi-family residential building (LeClaire Apartments, Enterprise Lofts)
5. Older single family homes
6. John Deere IT Center and associated parking lots
7. Mississippi River Floodway



4th Avenue: I-74 Bridge



4th Avenue: Large curb cuts at car oriented businesses impact quality of sidewalks



4th Avenue: Significant pockets of single family homes remain in the district

3.1 MOLINE CENTRE NEIGHBORHOOD/ EASTERN GATEWAY



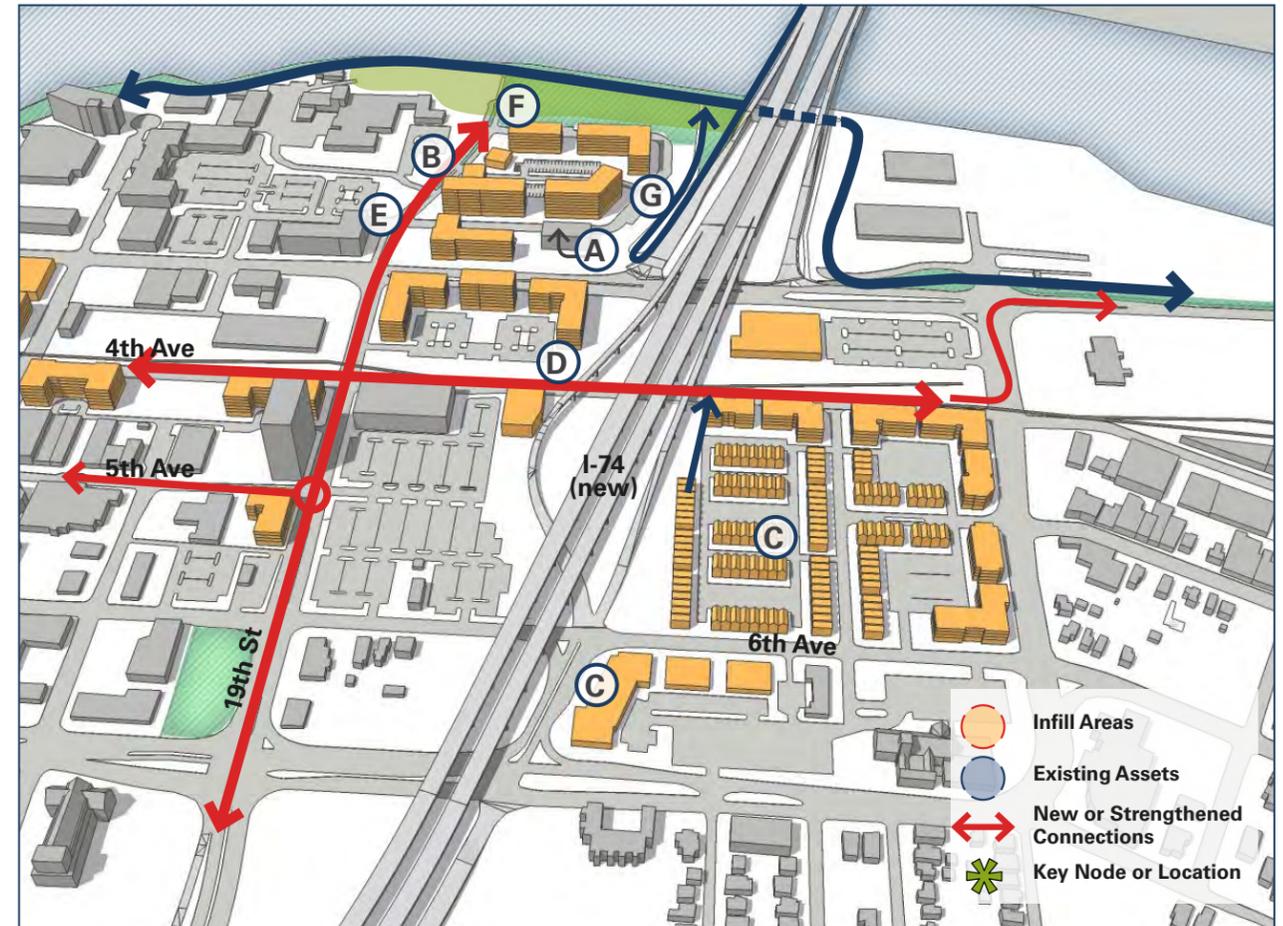
A portion of land adjacent the River is designated as part of the Mississippi River Floodway, meaning it is considered part of the River zone and is prone to flooding. Expansion of open space within the Floodway can provide ecological benefits. Development in this area should employ design methodologies that reduce impervious surfaces, decrease storm-water runoff, and reduce water pollution throughout. The landscape can also introduce vegetation that promotes biodiversity supporting native plants and animals. More information about flood mitigation can be found in Chapter 6.0 of the 2014 Comprehensive Plan Update.

Access and mobility throughout the Eastern Gateway can be improved with new streetscapes, and amenities for pedestrians and bicyclists. The Great River Trail can be relocated to the River's edge and connect to the interstate multi-purpose trail on the new bridge. 19th Street can extend to the River and be configured to create a view corridor to the River from River Drive. Streetscape upgrades along 19th street should be a priority as it takes on a larger role connecting 5th Avenue to 4th Avenue and 6th Avenue, and connecting to the River. The street should be designed using complete street standards (See Chapter 2.0). Walkability can be enhanced by reducing curb cuts, extending the sidewalk network, and adding more landscape.

Demand for development in this area will grow as the old bridge is demolished and circulation

patterns change, making it easier for people to see the development potential. As with the Downtown Core, development should first focus on residential development of a modest scale that doesn't saturate the market or divert development from the Downtown Core. For the area west of the bridge, medium density residential of 4 or 5 stories is compatible with this downtown and riverfront location. There is an opportunity to introduce limited mixed-use retail components. Development east of the bridge will likely lag behind, but should be thought of holistically. There is potential to develop this area as low to medium density multi-family housing such as townhomes. Throughout the district, emphasis should be placed on minimizing the dominance of auto oriented uses, parking lots, excessive curb cuts, especially on eastern side. The potential for noise from I-74 to impact future development should be addressed with developers. Appropriate building design criteria should be put in place to mitigate noise.

The majority of the land north of River Drive is within the 100 year flood plain. The City of Moline can use the redevelopment of the Eastern Gateway as an opportunity to incorporate sustainable and ecological design methods. All development whether open space or building development in this area should incorporate sustainable development principles such as the use of Low Impact Design (LID) strategies that minimize the impact of stormwater and provide environmental and ecological benefits.



Opportunities for the Eastern Gateway

Key Recommendations

- A. Explore redevelopment and/or adaptive reuse opportunities for obsolete and under utilized buildings.
- B. Highlight riverfront access. Connect to the River through new open space amenities.
- C. Use market indicators to guide development type and scale:
 - Mixed-use medium-density residential with neighborhood retail in former bridge right-of-way.
 - Develop low to medium density residential east of new bridge.
- D. Minimize impact of bridge on local circulation by connecting east and west sides along 4th Avenue and the riverfront. Integrate 4th Avenue multi-use path and connect path to neighborhoods.
- E. Strengthen 19th Street as a gateway to Moline Centre through enhanced streetscape. Reinforce urban infill along 19th street to create active street fronts.
- F. Reinforce 19th Street as part of the River to River Pedestrian / Bike Corridor
- G. Minimize and balance the impact of high parking demands on the quality of the urban environment.
- H. Improve wayfinding and strategically locate directional signage.

3.1 MOLINE CENTRE NEIGHBORHOOD/ EASTERN GATEWAY



Recommendations - Development Scenario 1

Consider the opportunity to remove the former KONE facility and create a significant public park along the Mississippi River.

- A. Special design considerations are needed within the Mississippi River Floodway (approximate location).
- B. Create new riverfront park.
- C. Realign Great River Trail to engage the River and the new I-74 Bridge Multipurpose Trail
- D. Extend 19th Street to River, creating a new view corridor, include bike paths on 19th Street north of 4th Avenue as part of River-to-River Trail network.
- E. Extend 4th Avenue multi-use path under the new I-74 bridge and connect to new residential neighborhood.
- F. Develop medium-density residential with some strategic retail sites.
- G. Create low- to medium-density residential with blocks organized to reinforce neighborhood pattern.
- H. Capture adaptive reuse opportunity of the historic Spiegel Warehouse.
- I. Review River-to-River trail alignment from the 2007 Plan for ongoing coordination with bridge traffic and access issues.



Eastern Gateway - Development Scenario 1



Example of waterfront open green space



Example of waterfront active, programmable open space

Open Space Ideas

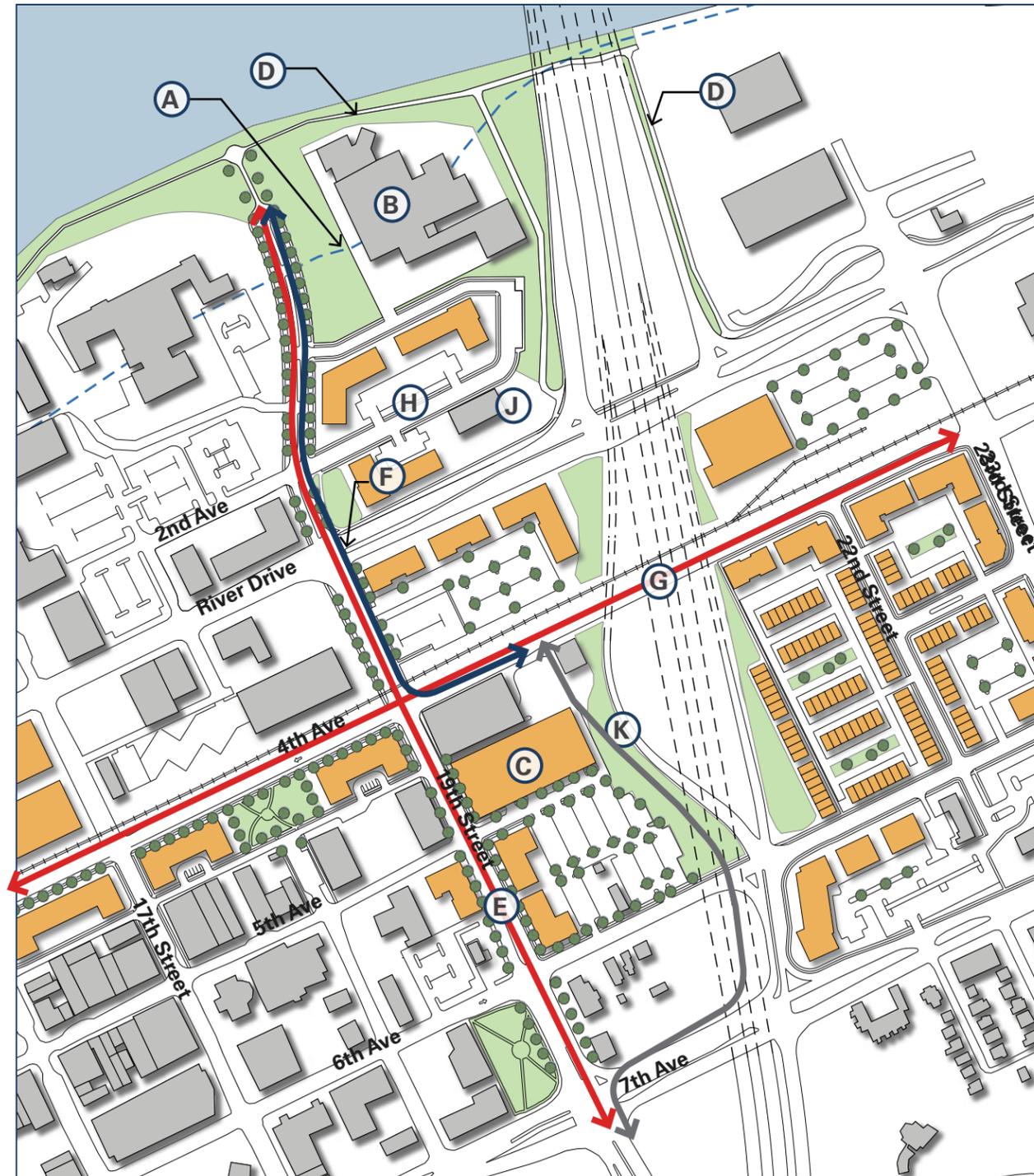
- Specialty Playground or Climbing Area
- Splash Pad
- Community Festival or Event Space
- Open Green Space - Kite Flying, Picnic, & Playing
- Programmable Space
- Boardwalk/ Overlook at River's Edge
- Native Garden Landscape
- Sculpture Garden
- Natural River 's Edge
- Seating Along Trail

3.1 MOLINE CENTRE NEIGHBORHOOD/ EASTERN GATEWAY

Recommendations - Development Scenario 2

Consider adaptive reuse of the former KONE building and opportunities to consolidate parking and develop 19th street frontage into an active urban mixed-use commercial corridor

- A. Special design considerations are needed within the Mississippi River Floodway (approximate location).
- B. Adaptive reuse of the former KONE facility
- C. Consolidate Deere parking into a structured garage, incorporate of active ground floor uses.
- D. Re-align Great River Trail to engage River and the I-74 Multi-Purpose Trail
- E. Develop 19th Street as a gateway to Moline Center with infill development with active ground floor uses
- F. Extend 19th Street to River, creating a new view corridor, include bike paths on 19th Street north of 4th Avenue as part of River-to-River Trail network.
- G. Extend 4th Avenue multi-use path under the new I-74 bridge and connect to new residential neighborhood.
- F. Develop medium-density residential with some strategic retail sites.
- G. Create low- to medium-density residential with blocks organized to reinforce neighborhood pattern
- H. Adaptive reuse opportunities for the historic Spiegel Warehouse.
- I. Review River-to-River trail alignment from the 2007 River for ongoing coordination with bridge traffic and access issues.



Eastern Gateway - Development Scenario 2



Example of waterfront open space connections



Example of waterfront open space connections



Example of medium density residential riverfront development

3.1 MOLINE CENTRE NEIGHBORHOOD/ EASTERN GATEWAY



19th Street Bike and Pedestrian Connections and River-to-River Corridor Impacts

The City of Moline should review the River-to-River Corridor Study (2007) to understand impact of the current bridge design on potential trail connections and alignments. The area between 7th Avenue and the Great River Trail will be especially impacted. Additional detailed studies should be undertaken to identify if it is still possible for the trail to parallel the new bridge alignment from 4th Avenue to 7th Avenue or if alternative routes need to be considered.

The extension of 19th Street north of River Drive has the potential to create connections between the 4th Avenue Multi-Use Path and the Great River Trail as part of creating an interconnected network of trails. This connection should be incorporated in future studies and detailed planning for the area once the old bridge is removed.

The introduction of new I-74 bridge entrance and exit ramps will likely alter traffic patterns on 19th Street. The City of Moline in the future should conduct traffic impact studies to understand the potential to reduce through lanes of traffic and introduce on-street bike facilities along 19th Street between 4th Avenue and 7th Avenue.

Moline can look to other riverfront cities, such as St. Paul, Minnesota, as examples of medium density residential mixed-use redevelopment with integrated public open space and active riverfront trails on former industrial sites.

Saint Paul's Upper Landing, St. Paul, MN

The Upper Landing is a new 22-acre, \$175 million Mississippi riverfront residential neighborhood in Saint Paul. Once a metal scrap yard and Superfund site, the Upper Landing today features seven blocks of riverside housing (home to 1,000 new residents), two new parks, a park pavilion/trail head and interpretive center, a public plaza with fountains and gathering spaces, river overlooks, fishing pier and a barge fleeting area.

Seven blocks of mixed-income and mixed-use housing: Site developers responded to the challenges of river views and access for all residents, existing historic structures, and unique topography and soil conditions. Four different builders delivered a diversity of design, market appeal and price points, ranging from high-end market-rate condos to affordable rental units.

The block adjacent to Chestnut Plaza includes loft style flats and 10,000 square feet of retail space, currently leased by a Caribou Coffee shop and AnyTime Fitness.

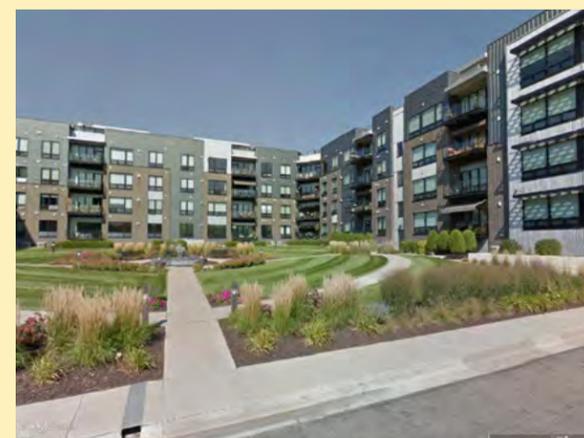
- Saint Paul Riverfront Corporation / <http://www.riverfrontcorporation.com/what-we-do/portfolio-upper-landing/>



Riverfront open space: credit: Tim Griffin / Flickr



Medium Density Residential



Medium Density Residential



Neighborhood Serving Retail

3.1 MOLINE CENTRE NEIGHBORHOOD/ EASTERN GATEWAY

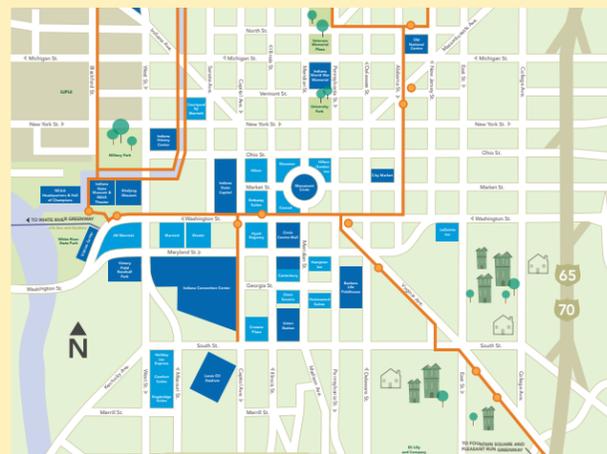


The City of Moline can look to other cities and towns that have successfully implemented mixed-use trails to connect urban neighborhoods and riverfront amenities. These projects can serve as examples on how to build upon the success of the Great River Trail and meet the goals of Asset Connectivity presented throughout this Plan.

Case Study: Cultural Trail, Indianapolis

The Indianapolis Cultural Trails is an 8 mile long urban bike and pedestrian trail located in downtown Indianapolis, Indiana. It connects 5 different cultural downtown Indianapolis districts and neighborhoods, while providing downtown with a greenway system.

The project cost of \$63 million and was funded by private and public funds. Today Indianapolis Cultural Trail, Inc. maintains and promotes the trail. The Cultural Trail promotes interest through different attractions along its length such as seven pieces of public art, a landscaped path, and bike rentals.



Case Study: Sabine Promenade on Buffalo Bayou Park, Houston

The Sabine Promenade project is 3,000 feet of linear park as part of the Bayou Park in downtown Houston. The project converted an eyesore with challenging urban conditions into an inviting urban park.

The project had a cost of \$15 million and was made possible through a historic private/public partnership committed to revitalizing Houston's downtown urban waterfront.

The promenade revitalized a vacant industrial area with an overhead freeway and utilities that suffered from flooding and transformed into a park with ecosystem for wetlands and native landscaping.

The project provides the city with landscaped trails and bike paths, public art, artistic lighting, way-finding and signage. Other attractions include boat tours and kayak rentals.



3.1 MOLINE CENTRE NEIGHBORHOOD/ WIU-QC DISTRICT



WIU-QC District Existing Conditions

The WIU-QC District is located east of the Downtown Core, between 23rd Street and 34th Street. This area has changed dramatically over the last five years from a predominantly industrial area to a growing academic campus and surrounding mixed-use district.

Phase I and II of the Western Illinois University-Quad Cities (WIU-QC) 20 acre Riverfront Campus is complete, with Phase III in the planning stages. WIU-QC has the fastest growing student population (15.4%) of all Illinois state universities with enrollment of 1,198 students in 2014.

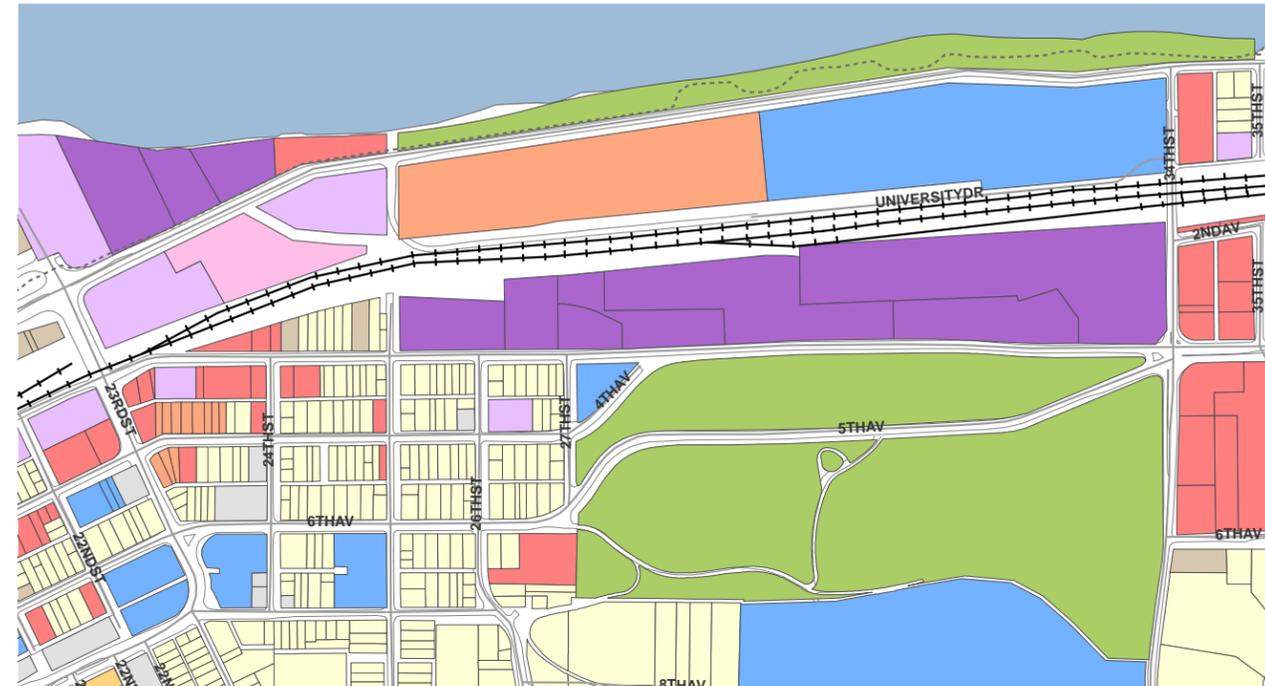
Riverbend Commons is a new 15 acre mixed-use residential development directly west of the WIU-QC campus. Phase I opened in 2015 with The Mills at Riverbend Commons. In 2015 the Mills provided housing for students from WIU-QC, Augustana and other schools in the region, as well as interns from Deere and Alcoa and trainees from KONE. The Mills includes two restaurants on site. Phase II is in the planning stage and will bring more residential and commercial development to the area.

Remaining industrial land to the west and the south create physical and physiological barriers to other nearby assets including Riverside Park and the Downtown Core. Trucks and other fast moving traffic dominate the area and creates a barrier along River

Drive between new development and the riverfront. The rail line creates another barrier between the riverfront and the community to the south of tracks.

WIU-QC District Prior Plans

- The Mississippi River Urban Technology Corridor Study (2006) proposed a 'RiverTech' concept for this area, including the new WIU-QC Campus, a Midwest Intellectual Property Management Institute, and promoted public-private partnerships.
- River Drive Corridor Concept Plan (2009) incorporates recommendations from the prior RiverTech Plan.
- 2014 Comprehensive Plan Update - identifies the growing WIU-QC campus and Riverbend Commons. It identifies the industrial parcels south of the rail line as future expansion of the WIU-QC campus and further development of university supportive uses. It also identifies an opportunity to create open space and pedestrian routes from the campus, across the rail line to Riverside Park. The long term vision in this Plan is currently being implemented and is still relevant moving forward.



Existing Land Use / Moline Centre WIU District (City of Moline GIS 2010)

Land Use		
Commercial	Office	Institutional/Entertainment
Mixed-Use	Parking	Low Density Residential
Heavy Industrial	Open Space	Multi Family Residential
Light Industrial	Vacant	Historic District Boundary

Source: City of Moline GIS data 2010



5th Avenue: Potential new bike / pedestrian connection



34th Street Pedestrian improvements needed to connect the WIU campus to the community

3.1 MOLINE CENTRE NEIGHBORHOOD/ WIU-QC DISTRICT



WIU-QC District Strategies

The development of the Western Illinois University – Quad Cities is a great asset to the community. It brings people, resources and amenities to the City. The campus is quickly transforming from a part-time commuter campus to a four year institution with students living near campus.

The Mills at Riverbend Commons is the first building under the Riverbend Commons Master Plan. It is a housing project developed through a public private partnership with the City of Moline geared to both students and the community. The Mills is designed to complement WIU-QC’s on-going conversion to a 4 year institution and to catalyze an entirely new neighborhood within the City of Moline. The Mills serves the University’s needs and will be an integral part of a remarkable mixed-use high-density urban neighborhood. Today the Mills consists of 240 student beds in 90 student living suites, in addition to

providing food, lifestyle, fitness and healthcare within the 20,000 square foot of retail space along its River Drive frontage.

The Mills, a private development, represents an ongoing shift for campus communities like WIU-QC by supporting the school and the community with housing, retail and dining for both students and non-students. This ongoing development will have a substantial impact not only on the anticipated growth of WIU-QC but on the growth and vitality of the City of Moline and the Quad Cities as a whole.

Creating a strong connection between the Downtown Core and WIU-QC is desired and mutually beneficial but the physical connections need to be strengthened. This can be done through shared programming - providing incentives for people to go back and forth as well as enabling the physical connections that make movement between the two subdistricts easier.



WIU District Overview

Key Features		
1. Riverside Park	4. Western Illinois University-Quad Cities (WIU-QC) Riverfronts Campus	7. Celebration Belle River Boat Dock
2. River Drive	5. Riverbend Commons /The Mills	8. New Channel Cat (water taxi) dock
3. IL-92 (4th / 5th Avenue)	6. Large industrial parcels	9. Great River Trail
		10. Existing commercial node



The Mills at River Bend Commons



Great River Trail between River Drive and existing industrial land uses



Great River Trail and the Mississippi River near WIU



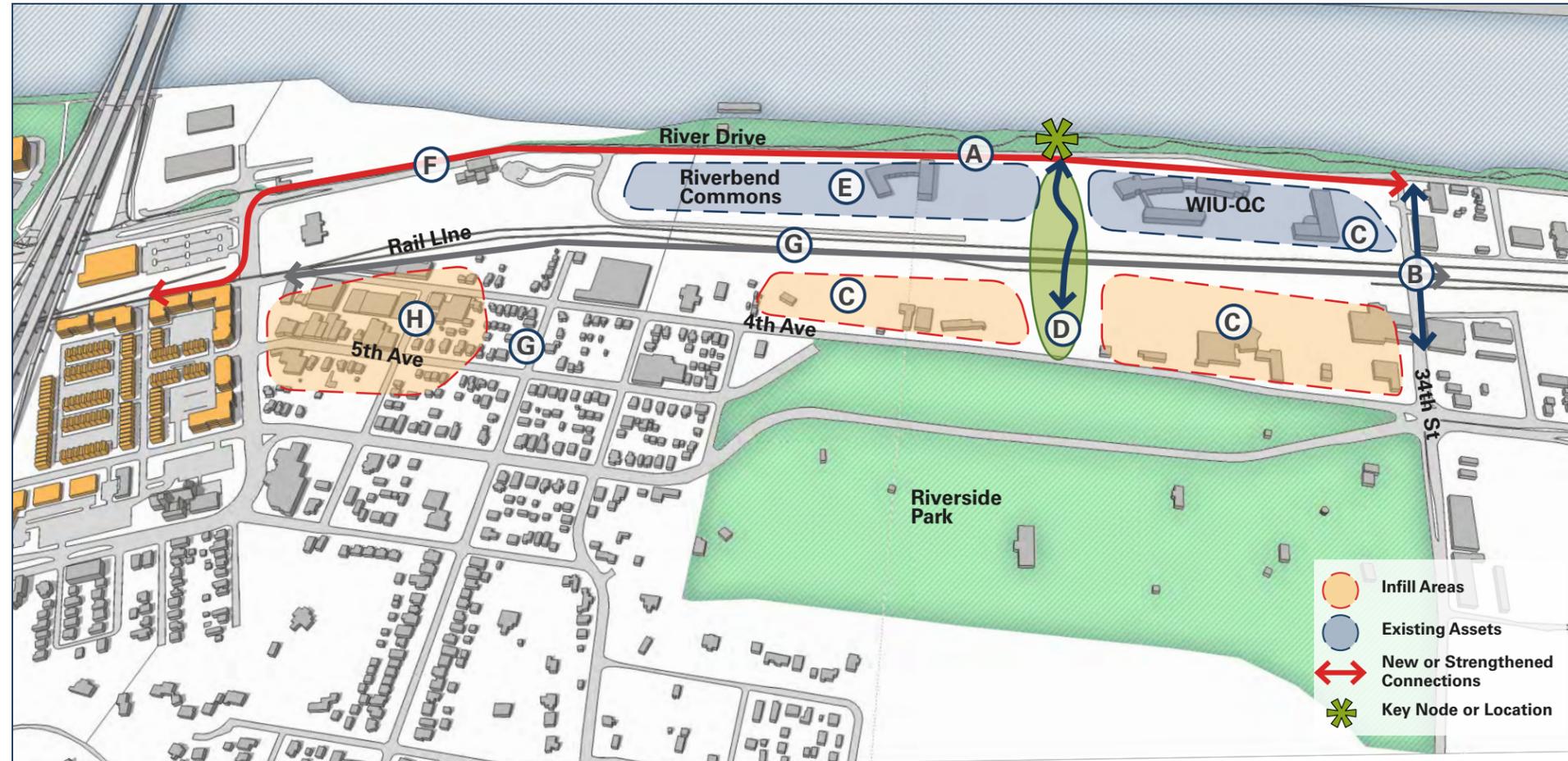
River Drive at 34th Street

WIU-QC District Strategies (cont.)

The existing industrial land uses create an environment that makes commuting by bike or foot undesirable. The City of Moline and current land owners may explore long term opportunities to relocate heavy industrial users. However in the near term, stakeholders should work to create an environment that is pleasant for all users. Frequent bus service is available between WIU-QC, the Downtown Core and the rest of the Quad Cities. Connections are currently being strengthened with the introduction of the new Channel Cat dock that will provide additional connection to the Great River Trail and the Metro bus system.

Create a Campus Community

- Support continued growth of the WIU-QC campus and related mixed-use development including residential and retail as outlined in the 2014 Comprehensive Plan Update.
- Strengthen connections to the Downtown Core by improving pedestrian and bike network and overall quality of the urban environment:
 - Employ truck routing alternatives and traffic calming strategies as outlined in Chapter 2.0
- Support high quality public realm:
 - Include complete network of sidewalks on both sides of all streets
 - Screen industrial uses from public view
 - Strengthen connections to Riverside Park



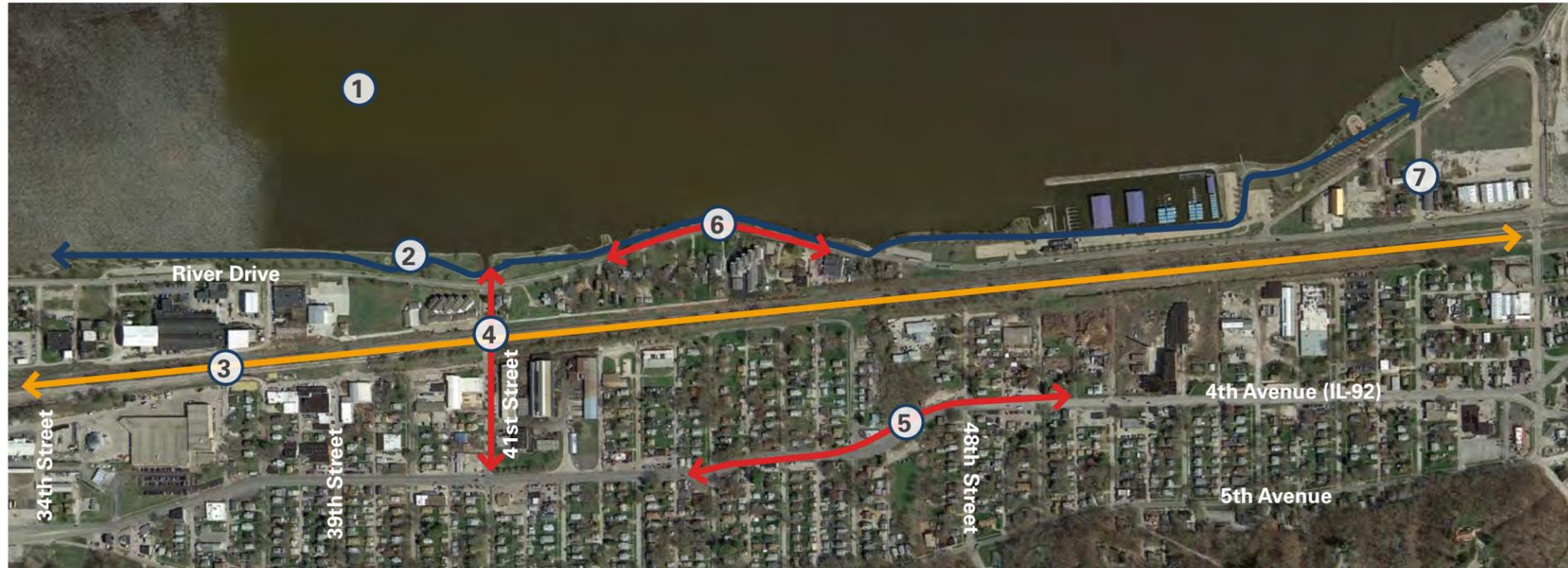
Urban Design and Planning Recommendations for the WIU District

- Continue to explore the long term feasibility of a new connection over the rail line (Riverside Park Greenway) identified in the 2014 Comprehensive Plan Update.
- Incorporate sustainable landscape strategies that address the Mississippi River Flood Plain and look for ways to manage stormwater on development sites in a sustainable manner.

Key Recommendations

- | | |
|--|---|
| <ul style="list-style-type: none"> A. Implement River Drive Traffic Calming, see Chapter 2.0. B. Improve neighborhood connections across rail line along 34th Street. Upgrade crossing, add sidewalks. C. Support long term expansion opportunities for WIU-QC and University supportive uses. D. Create new open space and pedestrian connection over rail line from the WIU-QC Campus to Riverside Park. | <ul style="list-style-type: none"> E. Support continued expansion of Riverbend Commons. F. Screen pedestrian and bike paths from large industrial parcels. G. Reinforce existing residential neighborhood feel and structure. H. Support commercial use for neighborhood and WIU-QC. Consider new "Campus Town" |
|--|---|

3.2 EDGEWATER NEIGHBORHOOD



Edgewater Neighborhood Overview

- Key Features**
1. Mississippi River
 2. River Drive / Great River Trail
 3. Freight Rail
 4. 41st Street Rail Crossing
 5. IL-92 / 4th Avenue "S" curve
 6. River Drive "S" Curve
 7. Mixed-use or Business Park Redevelopment Opportunity

Edgewater Prior Plans

- River Drive Corridor Concept Plan (2009) identifies the eastern edge of the neighborhood as having the potential to develop as a mixed-use center or business / industrial park
- Recommendations from the 2014 Comprehensive Plan Update remain valid and should continue to be utilized going forward:
 - Implement River Drive traffic calming.
 - Pursue 4th Avenue corridor improvements.
 - Improve neighborhood street connections.
 - Concentrate industrial uses and job generating activities on the east end and west end.

- The 2014 Comprehensive Plan Update identifies future development as opportunities for adaptive reuse, renovation and upgrades of existing buildings for restaurants, offices, shops and residential, noting few vacant lots. The recommendations are still relevant and should continue to be considered moving forward.

Edgewater Strategies

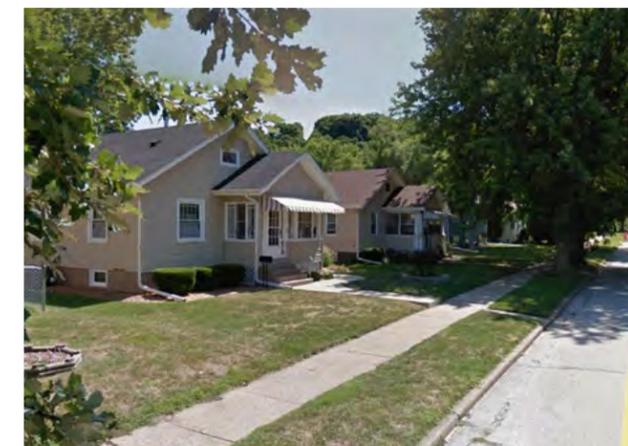
Transportation goals presented in Chapter 2.0 are to: calm traffic on River Drive; promote 4th Avenue as the preferred truck route; and place emphasis on important upgrades to the roadway infrastructure. The Urban Design and Planning recommendations in this

chapter build upon these strategies with specific land use and streetscape concepts.

Future development should encourage the creation of neighborhood nodes with commercial development at strategic intersections located approximately ¼ to ½ mile intervals along Illinois Route 92. The locations of these nodes can be identified and categorized by the through streets that connect to other neighborhoods. Several streets connect Edgewater to the neighborhoods to the south including 39th Street, 43rd Street and 48th Street. 41st Street is the only connection from the neighborhood to River Drive and the Great River Trail.



Typical Light Industrial: River Drive



Typical Residential Block: 45th Street in Edgewater

3.2 EDGEWATER NEIGHBORHOOD

- Key Recommendations**
- A. Install or upgrade sidewalks throughout.
 - B. Improve pedestrian and bike connections along 41st Street from the neighborhood, across the rail line, and to the River.
 - C. Install high-visibility pedestrian crossing with signalization at 41st Street and River Drive.
 - D. Introduce signalized intersections and protected pedestrian crossings along 5th Ave every 1/4 mile to 1/2 mile.
 - E. Augment neighborhood commercial / retail nodes
 - F. S-Curve at IL-92: Study geometric improvements and traffic calming enhancements to accommodate trucks and calm traffic. Coordinate with adjacent land uses and land owners.
 - G. River Drive: Study geometric improvements and traffic calming enhancements to slow traffic, improve parkway experience, and mitigate flooding.



Urban Design and Planning Recommendations for Edgewater Neighborhood

4th Avenue Pedestrian Strategies

The current design of 4th Avenue through the Edgewater Neighborhood encourages fast moving traffic and deters pedestrian and bike activity. As part of the overall roadway hierarchy outlined in Chapter 2.0, truck traffic will be encouraged to utilize 4th Avenue (IL-92) in lieu of River Drive through this section of town. To ensure a balanced “complete” street that meets the needs of the residents, the local business and the industrial users along the riverfront, the City of Moline should take a comprehensive look at the design of 4th Avenue with a complete street standards in mind.

The quality of the pedestrian environment can be strengthened throughout the neighborhood by installing lighting, sidewalks, signage, and landscape consistently that enhance walkability and the sense of place. Improve the streetscape along IL-92 to create a more inviting and safe pedestrian experience. Currently the sidewalks and roadway edges of IL-92 lack definition. Sidewalks are discontinuous and in most cases sidewalks are immediately adjacent to the roadway pavement, creating an uncomfortable walking environment. Roadway curbs sometimes blend into large asphalt areas. A defined streetscape of continuous sidewalks with a parkways and street trees would significantly improve the neighborhood character and sense of place.

Consider developing streetscape guidelines specific to Edgewater to create a consistent street edge that will strengthen the neighborhood character:

- A defined parkway and sidewalk dimensions.
- Regulations to restrict the creation of large paved parking areas in the set back of properties fronting on to IL-92.
- Parking lot screening, landscape and fencing requirements.
- Define signage design and installation requirements

- Prohibit fencing materials that negatively impact the neighborhood image including chain-link fences and barb wire.

To enhance pedestrian mobility across 4th Avenue, and to connect the north side of the neighborhood to the south side of the neighborhood, the City of Moline should consider the potential to add signalized pedestrian crosswalks or rapid flash beacons at pedestrian crossing locations. This recommendation would require further engineering studies.

3.2 EDGEWATER NEIGHBORHOOD



Existing Conditions along 4th Avenue



Visualization of an Enhanced Streetscape along 4th Avenue with Traffic Calming Measures in place



Visualization of an Enhanced Streetscape along 4th Avenue with Traffic Calming Measures in place

4th Avenue Traffic Calming Strategies

To better accommodate trucks on IL-92 / 4th Avenue the City of Moline should engage in a detailed study on the impact of trucks along the entire route and develop strategies to accommodate trucks, passenger cars, bikes and pedestrians in one street design. Stakeholders expressed concern as it related to the current design of the “S-curves” between 44th Street and 47th Street which should be the focus of detailed engineering studies. Additional traffic calming measures, such as the ones outlined below, should also be considered:

- Reducing the number of through lanes.
- A landscape median or curb extensions in some areas to allow pedestrians to move safely across 4th Avenue.
- Introduce dedicated left turn lanes to increase access to local businesses.
- Increase the number of signalized intersections.

41st Street Strategies

As part of the River Drive Traffic Calming strategy, detailed in Chapter 2.0, trucks are encouraged to move to 4th Avenue from River Drive at 55th Street and 34th Street. 41st Street is not ideal for moving truck traffic between River Drive and 4th Avenue due to adjacent residential development and the close proximity to a grade level rail crossing. This condition suggests that 41st Street could be a priority for pedestrians and bikes. Connections between the neighborhood and the Great River Trail on 41st

Street should be enhanced through installation of continuous sidewalks from River Drive to 5th Avenue, a signalized intersection at 4th Avenue, and highly visibility pedestrian crossings at 4th Avenue and at River Drive.

River Drive

Stakeholders expressed concern over the curve on River Drive near the 4300 block. The City of Moline should collect data to better understand the conditions at this juncture, including safety, flooding, traffic calming, and open space needs. The City of Moline should engage adjacent land owners about the potential to adjust the roadway in this area. This could also present an opportunity to widen a particularly narrow section of the parkland along the Great River Trail and assist in flood mitigation.

Future residential development along River Drive, such as the potential redevelopment of the International Harvester site in East Moline, will continue to transform the character of the neighborhood and should be considered in future plans for River Drive.

Neighborhood Character Improvements

Encourage gardening and other beautification efforts to instill community pride and neighborly cooperation.

Encourage building rehabilitation and renovations through programs like “Habitat For Humanity” or the City of Moline façade improvement programs.

3.0 Urban Design & Land Planning

3.3 FLORECIENTE NEIGHBORHOOD

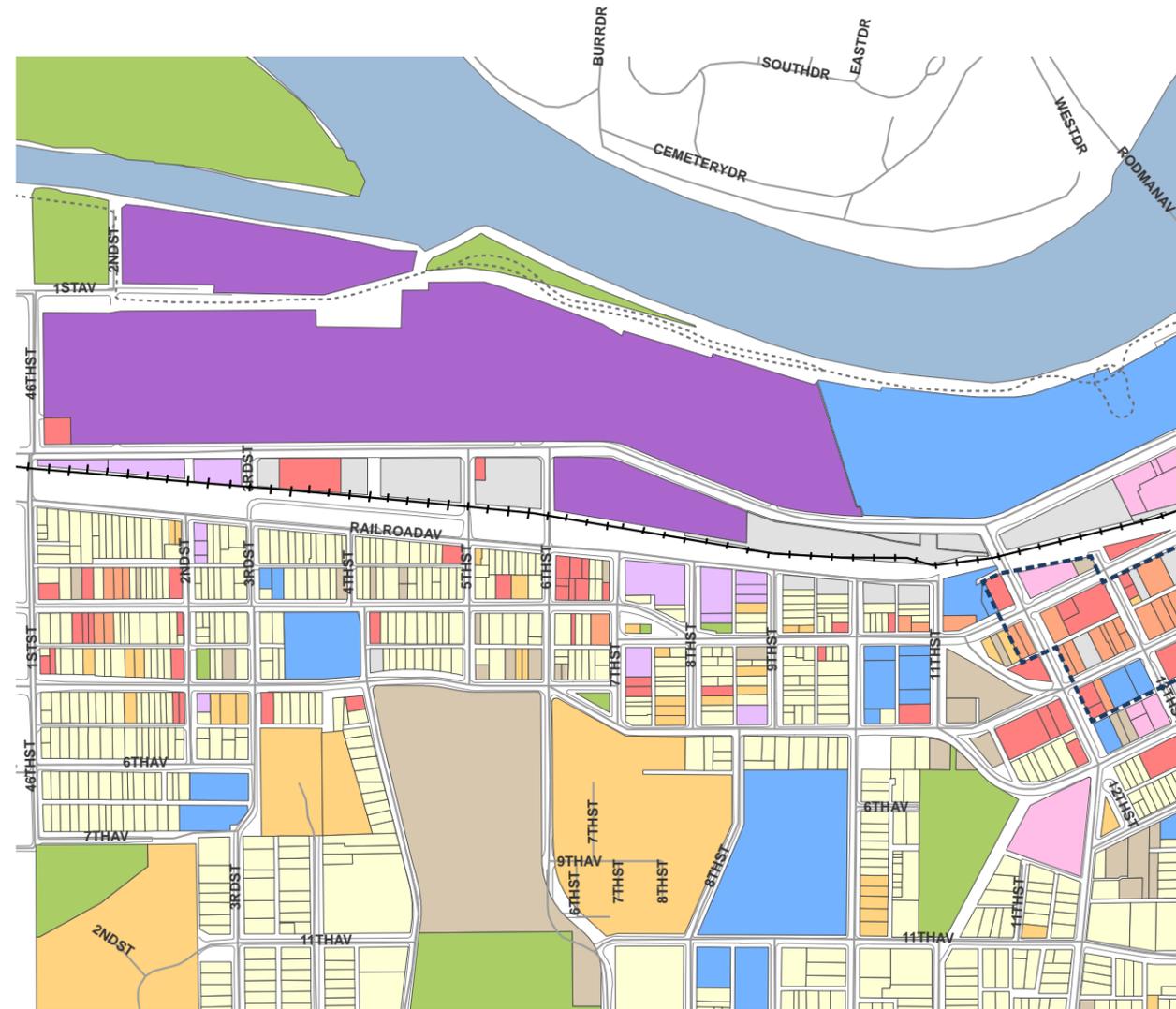
Florecente Existing Conditions

Florecente is a neighborhood with disparate and often incompatible land uses. The northern portion is industrial while the southern portion is a traditional residential area composed of older single family homes. Florecente is the western boundary of Moline, with the City of Rock Island to the west. The eastern edge of the neighborhood abuts the Western Gateway transit hub.

The combination of industrial land uses with wide, unrestricted one-way streets creates an environment that promotes fast moving trucks through the residential neighborhood. This fast moving traffic deters biking and pedestrian activity.

There are several parks in close proximity to Florecente including Velie Park, Stephens Park, McCandless Park, Sylvan Gateway Park, and the temporarily closed Sylvan Island. These parks are great assets however many of the parks are difficult to see and access. Pedestrian and bike connectivity is limited and not visible. Access to Velie and Stephens Parks are complicated by being located up the bluff.

Florecente is home to many large families with young children. During stakeholder interviews, residents expressed the need for near-by high-quality pocket parks and tot lots that are easily accessible to the neighborhood. Most of the existing playgrounds are on the grounds of community organizations and need modernization.



Existing Land Use / Florecente

Community organizations are working together to empower residents to help the neighborhood thrive and grow. Many of these organizations are led by local residents or have significant resident involvement including Global Communities/Deere Foundation; Habitat For Humanity; Palomares Social Justice Center; Boys and Girls Club; Project Now; and Heritage Church's Esperanza Center and Vida Nueva Campus.

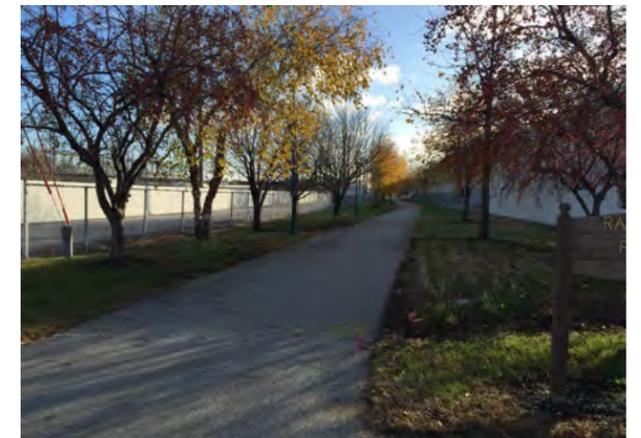
Land Use

Commercial	Open Space
Mixed Use	Institutional/Entertainment
Heavy Industrial	Low Density Residential
Light Industrial	Multi Family Residential
Office	Vacant
Parking	Historic District Boundary

Source: City of Moline GIS data 2010



Bridge to Sylvan Island: Currently closed, the City of Moline is working to reopen the park



Great River Trail Connection: at Sylvan Park weaves through industrial area.



Stephens Park: includes ball fields and playgrounds, but is difficult to access from neighborhood due to quality of sidewalks and location on bluff.

3.3 FLORECIENTE NEIGHBORHOOD



Florecente Prior Plans

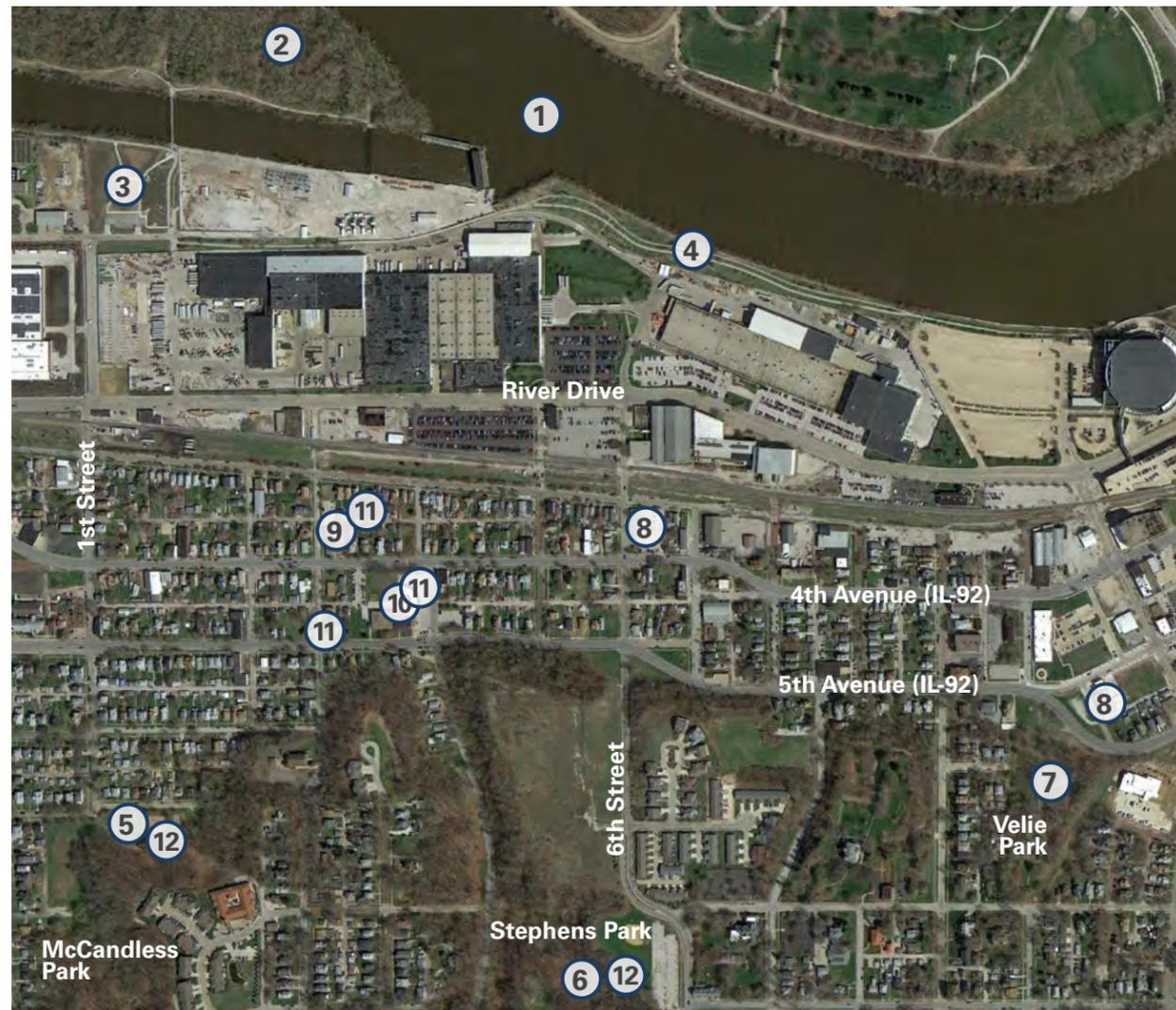
Several recent studies, in addition to Chapter 2.0 of this document, have highlighted the negative impact of high vehicle speeds on the neighborhood. Recent studies include the idea of converting 4th Avenue and 5th Avenue (IL-92) from one-way streets to two-way streets. These studies also outline, in detail, traffic calming and pedestrian enhancements that should be implemented regardless of one-way or two-way traffic. The City of Moline should continue to pursue implementation of these recommendations.

Relevant studies include:

- The Illinois Highway 92 Corridor Study
- River Drive Corridor Study
- 2014 Comprehensive Plan Update

The 2014 Comprehensive Plan Update also includes recommendations to:

- Improve the landscape along Railroad Avenue
- Improve connections to Sylvan Island
- Develop neighborhood centers at 4th Avenue and 7th Street and along 4th Avenue between 1st Street and 2nd Street
- Support neighborhood improvement through a series of strategies and specialized programs
- Enhance Industrial Areas



Florecente Neighborhood Overview

Key Features		
1. Mississippi River (Slough)	5. McCandless Park	10. Heritage Church / Esperanza Community Center
2. Sylvan Island	6. Stephens Park	11. Playgrounds and Tot Lots (Private)
3. Sylvan Island Park	7. Velie Park	12. Playgrounds and Tot Lots (Public)
4. Great River Trail	8. Boys and Girls Club	
	9. Project Now / Headstart	



Typical Residential Block with Corner Commercial



Typical Residential Character: 5th Avenue



Signalized pedestrian crossing: at 4th Avenue and 4th Street

3.3 FLORECIENTE NEIGHBORHOOD



Neighborhood Improvement Goals

- Improve Access to Green Spaces
 - Add sidewalks to connect to parks
 - Improve existing walks and crosswalks
 - Add wayfinding signage
- Enhance Streetscape
- Create Neighborhood Identity
- Preserve established economic and employment generators
- Pursue partnership to enhance riverfront trail connections and aesthetics along River Drive Corridor.
- Connect neighborhood to Multi-Modal Station and Downtown

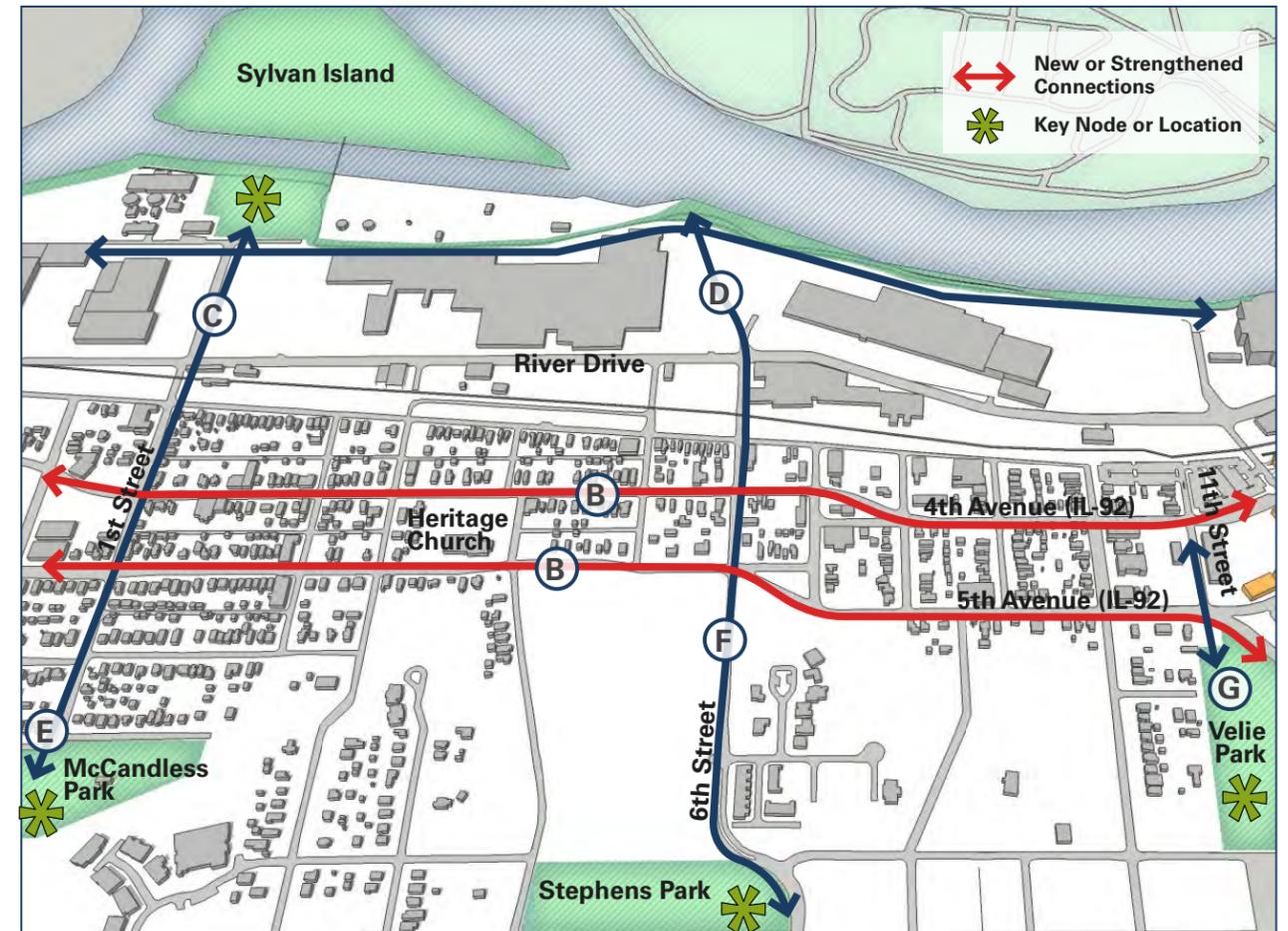
Florecente Strategies

Florecente is a traditional neighborhood with a strong sense of identity and strength of community. These assets can be celebrated and expanded through improvements to the quality of the public realm, strengthening of connections to existing amenities, and added community resources such as tot lots and play grounds.

Through active community engagement, the City of Moline can empower residents, in collaboration with local leaders and neighborhood organizations, to advocate for their community priorities. The arrival of the Heritage Church's Esperanza Center and the Iglesias Vida Nueva Campus at the former Ericcson School Site is a great example of collaboration within the community. For example, to accommodate the former Ericcson School students, MetroLINK is providing bus service from the site to the new school further afield. Moving forward, as this location shifts from a school to a community center, the need

to provide safe access and a welcoming physical environment remains. Heritage Church, MetroLINK and the City of Moline should continue to work together to provide a safe welcoming environment for everyone at the site.

Residential neighborhoods thrive when the public realm, such as sidewalks and parks, become part of the shared space where neighbors gather and share common experiences. The experience of living in Florecente is diminished by fast moving traffic, the poor condition of the sidewalks and the lack of easily accessible public parks. The potential for 4th Avenue and 5th Avenue to become two-way streets simplifies access making wayfinding easier and has the potential to significantly slow down traffic and improve pedestrian safety.



Urban Design and Planning Recommendations for Florecente

Key Recommendations

<p>A. Improve Walkability:</p> <ul style="list-style-type: none"> - Install or upgrade sidewalks throughout. - Improve intersection design to increase vehicular awareness of crossings, slow traffic and improve north/south pedestrian connections throughout. <p>B. Calm Traffic</p> <ul style="list-style-type: none"> - Increase the number of signalized intersections to slow and control traffic on IL-92. - Work with community, IDOT and Rock Island to transition to two-way streets. 	<p>C. Improve visibility and quality of Sylvan Island Connection through enhanced signage.</p> <p>D. Work with John Deere to explore potential Great River Trail Connection at 6th Street.</p> <p>E. Strengthen gateway to McCandless Park and improve internal network of paths within park.</p> <p>F. Widen sidewalks and upgrade streetscape between Stephens Park and community.</p> <p>G. Upgrade Velie Park at 5th Avenue - Install pocket park or tot lot, improve internal network of paths, install traffic calming and pedestrian crossing at 11th Street.</p>
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3.3 FLORECIENTE NEIGHBORHOOD



Vision for 5th Avenue in Floreciente: Introduce complete streetscape with trees and traffic calming strategies.



5th Avenue at 46th Street: Existing Conditions

Streetscape Improvements

- A. Consider Two-Way Traffic
- B. Add Street Trees
 - 25' to 45' on center
 - Understory/Ornamental Trees at Overhead lines
 - Shade Trees at all Other Locations
- C. Repair and Replace Existing Sidewalks
- D. Add Sidewalks at Missing Connections
- E. Add Accessible Crossings with Truncated Domes Tactile Warning



5th Avenue at 45th Street: Existing Conditions



5th Avenue at 5th Street: Existing Conditions



5th Avenue at 46th Street: Proposed streetscape with trees

Pedestrian Amenities Strategies

The City of Moline should conduct a sidewalk audit to identify areas most in need of improvements. Improvements may range from enhanced maintenance to installing curb ramps and missing sidewalks. Priority should be given to improving access to transit resources such as bus stops and open space amenities. The community should work with the City of Moline to identify additional priority areas.

As discussed in detail in Chapter 2.0, fast moving traffic along wide roads with very few traffic signals negatively impacts the quality of life within the neighborhood and limits local mobility. One strategy to improve conditions is to convert the one-way streets to two-way traffic and introduce additional traffic calming measures such as enhanced landscape, pedestrian bump outs at intersections to

shorten crossing distances and alert drivers to the presence of intersections and signalized pedestrian crossings (such as the one near the Heritage Church at the intersection of 4th Avenue and 4th Street). If the City of Moline chooses not convert 4th and 5th Avenues to two-way streets, traffic calming is still critical to improving the quality of life and promoting

walkability with the neighborhood. There are only two signalized intersections along 5th Avenue (at 4th Street and 10th Street) and one along 4th Avenue at 4th Street. This makes crossing the street very difficult and contributes significantly to the lack of pedestrian mobility, even more than conditions of the existing sidewalks.

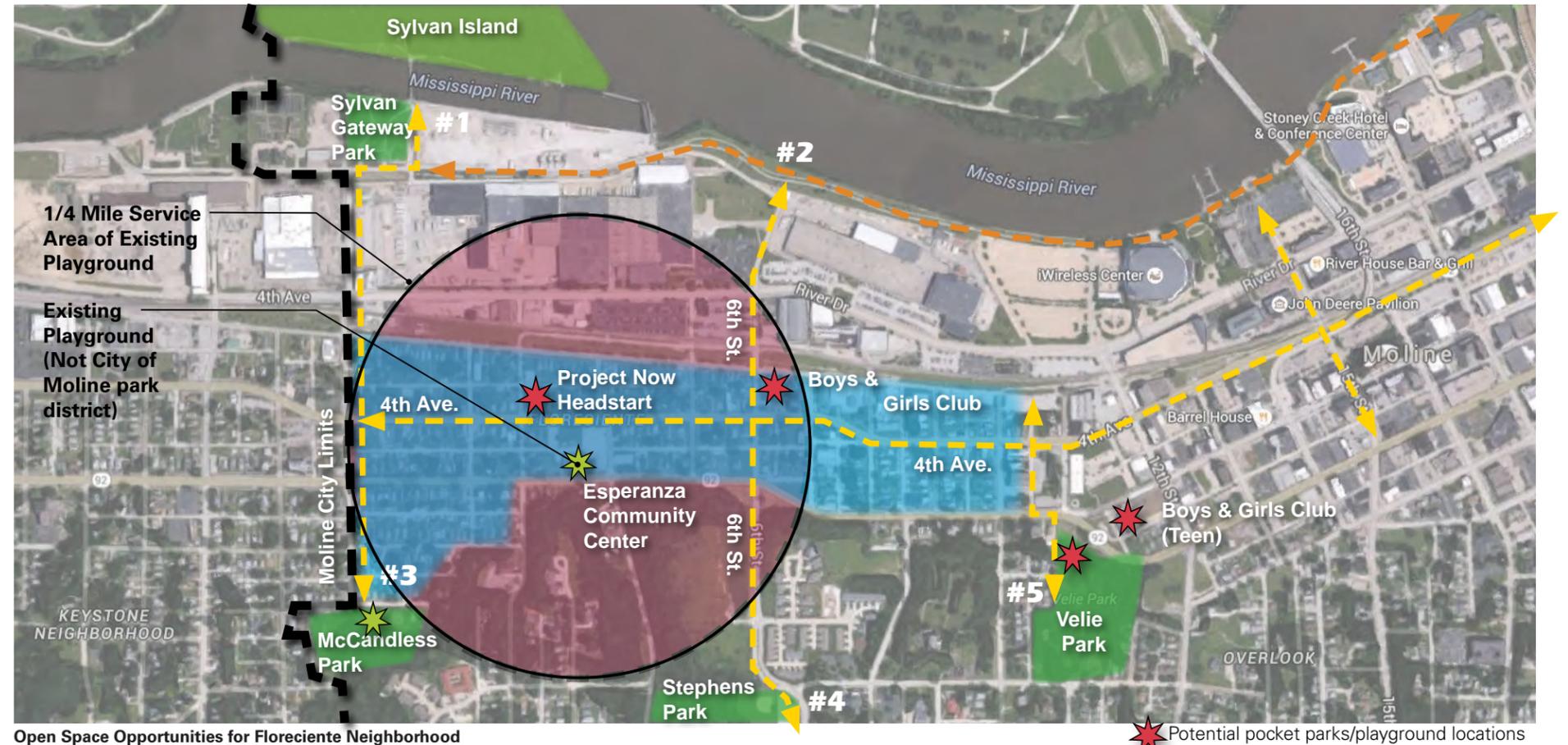
3.3 FLORECIENTE NEIGHBORHOOD

Florecente Open Space Strategies

There are extensive open space amenities located near the Florecente neighborhood, however they remain largely inaccessible due to either barriers created by riverfront industrial uses to the north or the bluff to the south. Florecente is home to many families, and access to open space including recreational amenities such as bike trails and play grounds are essential to the health of the community. The City of Moline should work with residents and community leaders to identify potential locations for new parks or upgrading of existing parks. Based on neighborhood characteristics including number of children, the City should review the 2014 Moline Park & Recreation Plan in conjunction with standards established by the National Park and Recreation Association Guidelines to guide development of or enhancements to tot lots and neighborhood parks. A general rule of thumb is that residents should be within a 1/4 mile walk of a playground or tot lot. Shared open space with community organizations should be encouraged.

Riverfront Access

Sylvan Island, Gateway Park and the Great River Trail, are near by, but are difficult to find and access. Access requires passing through industrial sites, rail crossing without sidewalks, and unsignalized street crossings on 1st Street at River Drive, 6th Avenue, and 5th Avenue. Strategies to improve access to



Open Space Opportunities for Florecente Neighborhood

★ Potential pocket parks/playground locations

these areas include adding sidewalks, upgrading rail road crossing, adding marked or signalized intersections, and adding signage to identify routes to the park, Sylvan Island and trails.

The City should establish an overarching goal of creating riverfront access every 1/2 mile throughout the City of Moline, including in Florecente. An ideal location for access from the residential neighborhood is along 6th Street. The City should engage in conversations with John Deere to explore the possibility

of creating a public access point to the Great River Trail through this portion of the site. The City should work with industrial users to screen existing sites and require all new development to provide proper screening and sidewalks.

Parks and Playgrounds

There are partnership opportunities for community organizations to help bring playgrounds to the neighborhood. "Community Built Playgrounds" are

designed in collaboration with design professionals and equipment manufactures and installed by volunteers. Companies such as Landscape Structures and Kaboom also support community built playgrounds. Other organizations that offer assistance to communities include: Kiwanis International, National Association of Elementary School Principals (NAESP) Community Build Projects, and Project Fit America (PFA). The Community Built Association is collaboration between professionals and community volunteers to design, organize and create community projects.





4.0 IMPLEMENTATION

MOVING MOLINE FORWARD: URBAN DESIGN & TRANSPORTATION UPDATE TO
THE 2014 COMPREHENSIVE RIVERFRONT PLAN

JUNE 20, 2016

4.0 Implementation

4.1 IMPLEMENTATION STRATEGIES



This 2016 City of Moline Comprehensive Plan update focuses on the three Riverfront Neighborhoods. The Plan creates a framework to prioritize transportation and district goals, organize stakeholders, apply resources, and connect local assets so that all neighborhoods can continue to be vital, grow economically and support a high quality of life. The Riverfront Neighborhoods have many cultural, economic and regional assets that make this area a valuable resource to the City of Moline and the Quad Cities. The intricate urban fabric of Moline’s Riverfront communities emerges from embracing the new I-74 bridge with its new ramps and roadway connections, the expanding WIU campus and its transformation of the eastern Riverfront; The Q Multi-Modal Station, Western Gateway development and many other local attractions. These elements are at the forefront of the recommendations of this plan.

The issues and ideas that have emerged in this plan are the products of numerous dialogues, community meetings and public open houses with the diverse community of the City of Moline and the Quad Cities. At its core, the basic premise of this planning effort is to enhance the unique assets of the Riverfront communities and build on the transportation, cultural, social, economic, and physical infrastructure that exists today.

The goals, strategies and recommendations in this plan outline opportunities for improving the major transportation corridors; connecting the critical

new bridge and rail station infrastructure; infilling strategic development sites; improving streetscape, catalytic projects provide a working framework for the development of the Riverfront Neighborhoods. The discussions and ideas of stakeholders have served as the foundation for recommendations that serve as a basis for future actions. The strong network of community organizations and individual citizens supported by local government agencies will help facilitate next steps for the implementation of this plan. Although no one organization or body is responsible for the implementation of specific tasks, it is important to take advantage of the momentum generated by these ideas to form responsible partnerships regarding the resources needed as well as scheduling goals for specific milestones.

A summary of action items generated throughout this 2016 Comprehensive Plan Update is presented in the following pages in an effort to provide a simplified checklist of implementation goals for individual projects or areas. Projects should be prioritized in order of importance, availability of funding sources, synergies with other projects and ease of implementation. Funding sources should be identified, and detailed drawings should be developed for concepts presented in the plan.

This action item “to do” list will help government agencies, community organizations and individual citizens develop a plan of action for individual recommendations. The City of Moline, local

Implementation Policies	
1.0 Establish Priority Development Areas. Identify key sites that are well poised for development and will have a catalytic effect to attract new development	10.0 Reconfigure the IL-92 couplet to better serve the Floreciente Neighborhood
2.0 Actively pursue opportunities to program and activate downtown and the riverfront	11.0 Upgrade railroad crossings to improve pedestrian safety and connectivity, consider rail quiet zones
3.0 Improve streetscapes throughout the neighborhoods	12.0 Support existing transit services with streetscape and signage improvements
4.0 Create new open space and roadway connections	13.0 Consider Bus Rapid Transit (BRT)
5.0 Establish a new City of Moline street hierarchy map to guide design decisions	14.0 Encourage on-street bike facilities
6.0 Identify and implement traffic calming alternatives along River Drive	15.0 Continue and expand programs and policies to increase visibility of cycling and safe cycling
7.0 Identify and implement traffic calming alternatives along IL-92	16.0 Support user choice to take alternative modes by providing bike amenities and fostering a biking community
8.0 Define east/west pedestrian and bike access routes through new I-74 bridge zone	17.0 Improve pedestrian network and accessibility within the riverfront neighborhoods
9.0 Improve pedestrian and bike access surrounding The Q Multi-Modal Station	18.0 Support growth of downtown businesses by understanding parking demands and improving access to parking
	19.0 Improve wayfinding and signage

transportation agencies and Renew Moline can serve as a central communication hub for citizens and be able to assist in the continued transportation enhancement efforts.

Through website, e-mail and other communication

efforts, the City of Moline, key stakeholders and community representatives can collectively help organize around common goals and gather information needed to accomplish specific tasks. The following is a summary of recommendations for policy decisions, project definitions and next steps.

4.1 IMPLEMENTATION STRATEGIES



Implementation Partners

- City of Moline
- Renew Moline
- Moline Community Development Corporation (MCDC)
- Illinois Department of Transportation
- Bi-State Regional Commission
- City of Rock Island
- MetroLINK
- Quad Cities Chamber of Commerce
- Moline Centre Main Street Commission
- Business Property Owners Group
- Moline Project Management Team
- Private Developers and Investors
- Financial Institutions
- Corporations and Industries
- Foundations and Community Organizations

Local Funding Sources

- Bi-State Revolving Loan Fund Program
- Capital Improvement Budget
- Community Development Block Grant (CDBG)
- Facade Improvement Programs
- General Revenue Bonds
- Moline Revolving Loan Fund Program
- Motor Fuel Tax
- Neighborhood Stabilization Program
- Quad Cities Enterprise Zone
- Quad City Regional Economic Development Authority (QCREDA)
- Special Service Areas (SSA)
- Storefront Restoration Grant for Small Businesses
- Tax Increment Financing (TIF Districts)
- Traditional Commercial Areas Rejuvenation Program

Consider Adding

- Business Improvement Districts (BID)

State Funding and Financial Resources

- Illinois Department of Commerce and Economic Opportunity (DCEO).
- Illinois Capital Access Program from DCEO
- Revolving Line of Credit PLP Program (RLOC PLP) from DCEO
- Illinois Finance Authority Participation Loan Program
- Illinois Transportation Enhancement Program (ITEP)
- Open Space and Land Acquisition Program (OSLAD)
- Land Water Conservation Fund (LWCF)
- Park and Recreational Facility Construction Program (PARC)
- Public Water Supply Loan Program
- Illinois Rail Program (ITP)

Federal Funding and Financial Resources

- Historic District Tax Credits
- Certified Local Government Grants
- Low-Income Housing Tax Credit Program
- Historic Preservation Tax Credit Program
- Brownfield Assessment Grants
- U.S. Small Business Association (SBA)
- American River Floodplain and Side Channel Restoration Project, US Fish and Wildlife Service
- Water Resources Development Act (WRDA) US Army Corps of Engineers
- Congestion Mitigation and Air Quality Program (CMAQ)
- Transportation Investment Generating Economic Recovery (TIGER)
- Surface Transportation Program (STP)

Other Funding Sources

- Venture Fund/ Community Supported Financing
- Business Plan Competitions
- Federal Home Loan Bank of Chicago (FHLB)

4.1 IMPLEMENTATION STRATEGIES



1.0 Policy: Establish Priority Development Areas. Identify key sites that are well poised for development and will have a catalytic effect to attract new development	
1.1	Design Guidelines: Review future development in context of the 2007 Moline Centre Design Guidelines and the historic district. Continue to promote reinvestment in existing building stock. Define how new construction should occur within the appropriate and sensitive context of neighboring properties.
1.2	Promote residential loft conversion in Moline Centre
1.3	Prioritize development around new Multi-Modal Station
1.4	Create co-working and incubator spaces. Identify potential spaces and organizations, facilitate collaboration.
1.5	Review development plans to ensure active ground floor uses and walkable streetscapes on north-south connecting streets
1.6	Develop south side of 4th Avenue in the Downtown Core. Consolidate surface parking lots and narrow 4th Avenue A (alley) to create development parcels from 17th Street to 19th Street.
1.7	Create neighborhood commercial nodes on 4th Avenue at strategic intersections such as 39th Street, 43rd Street and 48th Street, and 41st Street.
1.8	Promote adaptive reuse of existing buildings and infill on vacant lots
1.9	Consolidate Deere parking along 19th Street into a structured garage, incorporate active ground floor uses.
1.10	Develop parcels along 19th Street with active ground floor use
Work with land owners and developers to pursue redevelopment of key parcels	
a.1	Develop historic Spiegel Warehouse
a.2	Determine viability of reuse of former KONE facility. Conduct physical and market assessment
a.3	Develop 6th Ave from 12th Street to 14th Street with urban style townhouses
a.4	Work with land owners to pursue adaptive reuse of the Chase Bank Building
a.5	Work with land owners to pursue adaptive reuse of the Carnegie Library
a.6	Review emergency response plan with appropriate agencies as major residential developments are planned, especially for areas north of the rail line
2.0 Policy: Actively pursue opportunities to program and activate downtown and the riverfront	
2.1	Plan and promote events such as farmer's markets, festivals, special theme weekends, etc.
2.2	Work with existing community groups, Chamber and local business owners to create a committee focused on programming, promotion of events, and cross promotion with businesses.
2.3	Use Bass Street Landing as well as temporary locations such as parking lots, parking garages, and streets for outdoor & seasonal events.

3.0 Policy: Improve streetscapes throughout	
3.1	Continue to implement the 2009 Downtown Streetscape Master Plan.
3.2	Develop parking lot landscape design guidelines for Moline Centre
3.3	Require sidewalks to be installed along all public right-of-ways for all future developments.
3.4	Develop Streetscape Master Plan for Floreciente and Edgewater based on complete street policies
3.5	Develop new or updated wayfinding system. Engage wayfinding consultant to conduct detailed strategy and signage system. Coordinate with adjacent communities.
3.6	Work with industrial users to effectively screen sites from public view
4.0 Policy: Create new open space and roadway connections	
4.1	Extend 19th Street to the River in former I-74 bridge right-of-way
4.2	Floreciente: Identify locations for new tot lots and pocket parks.
4.3	Create new riverfront park along the Mississippi River in former I-74 bridge right-of-way
5.0 Policy: Establish a new City of Moline street hierarchy map	
5.1	Floreciente: Support pedestrian and bike activity in residential neighborhoods along 4th and 5th Avenues (IL-92) by promoting River Drive as a quality east/west truck route.
5.2	Moline Centre: Consolidate truck traffic to River Drive to free up downtown streets for pedestrian oriented uses.
5.3	Edgewater: Encourage trucks to use IL-92 (4th Avenue) as an alternative to River Drive. Allow River Drive to become a local street that serves the neighborhood.
Engineering / Roadway Geometric Analysis Study key intersections for redesigning and directing truck traffic to preferred routes.	
a.1	Analyze River Drive at 55th Street intersection at East Moline border
a.2	Analyze River Drive at 23rd Street with connections to 4th and 6th Avenues
a.3	Analyze 6th Avenue (IL-92) and 23rd Street
a.4	Analyze S-curve transition from 5th Avenue to 6th Avenue (IL-92) at 27th Street
a.5	Analyze River Drive intersection at 34th Street
a.6	Analyze 4th Avenue (IL-92) at 34th street
a.7	Analyze River Drive intersection at 41st Street
a.8	Analyze 4th Avenue S-curve between 44 Street and 48th Street
a.9	Analyze River Drive/12th avenue (E. Moline) at 55th /1st Street (E. Moline)

6.0 Policy: Identify traffic calming alternatives along River Drive	
6.1	Analyze River Drive for traffic calming from 23rd Street to 55th Street.
6.2	Consider Phase 1 River Drive for traffic calming from 34th Street adjacent to WIU Quad Cities Campus, to 23rd Street.
6.3	Consider a "pilot approach" for Phase 1 with paint and bollard strategy. Improvements to include high visibility crossings, pedestrian signage to alert drivers and hatching / transitions zones where lanes are being narrowed. Allow for design flexibility and driver /pedestrian feedback on what works.
6.4	With success of pilot approach develop a final phase to install concrete raised curb-protection pedestrian crossings at strategic location
6.5	Collect data to better understand the conditions, including safety, flooding, traffic calming, and open space needs near the 4300 block of River Drive.
7.0 Policy: Identify traffic calming alternatives along IL-92	
7.1	Edgewater: Encourage trucks to use IL 92 (4th Avenue) as an alternative to River Drive. Improve intersections and sidewalks to increase pedestrian safety and comfort in the context of larger vehicle or turning movements
7.2	Moline Centre; Support the local residential and retail land uses with improved pedestrian amenities, intersection design and streetscape. Narrow roadway width on 6th Avenue and incorporate multi-use path on 4th Avenue.
7.3	Floreciente: Support the local residential neighborhood character by slowing traffic and providing improved pedestrian amenities, intersection design, sidewalks and streetscape. Consider returning 4th and 5th Avenues to two way streets.
Engineering / Roadway Geometric Analysis	
a.1	Analyze 4th and 5th Avenues for two way conversion in Floreciente.
a.2	Analyze roadways for developing on-street bike routes and network.
a.3	Analyze 4th Avenue for location of new multi-use path
a.4	Analyze potential to convert 4th Avenue to two-way traffic from 23rd Street to 34th Street.

4.1 IMPLEMENTATION STRATEGIES



8.0 Policy: Define east/west pedestrian and bike access routes through new I-74 bridge zone	
8.1	Study creating a multi-use path along 4th Avenue consider both on and off street alternatives. Consider alternative alignments if necessary
8.2	Initiate access plan for connecting to the Great River Trail under the new I-74 Bridge
Engineering / Roadway Geometric Analysis	
a.1	For the 4th Avenue multi-use path and on-street concept evaluate the option to remove a lane of traffic. Use pilot program to test the concept with temporary installations such as bollards blocking vehicular access on to the path, increase visibility through paint and ground level design at the intersections).
a.2	For the 4th Avenue Multi-use Path and off street solutions, evaluation expanding the existing sidewalks to 10 feet width.
a.3	Initiate access plan for connecting to the Great River Trail under the new I-74 Bridge, explore options to work with the bridge landscape strategy. Realign trail to river's edge where possible.
a.4	Study connecting the proposed multi-use path to the Great River trail at 23rd Street and identify any necessary intersection improvements.
9.0 Policy: Improve pedestrian and bike access surrounding The Q Multi-Modal Station	
9.1	Study connecting the multi-use path along 4th Avenue with The Q Multimodal Station development, via high visibility pedestrian and bike connections in all directions between 12th and 15th Streets.
Engineering / Roadway Geometric Analysis	
a.1	Define sidewalk connections and streetscape improvements to unify the district
a.2	Establish a bicycle activity hub with rack and other bike amenities at 15th Street
a.3	Create a bike share station at the Q Multi-Modal Station. Coordinate with City of Moline Bike Share Study

10.0 Policy: Reconfigure the IL-92 couplet to better serve the Floreciente Neighborhood	
10.1	Review traffic calming and on-street bike routes on 4th and 5th Avenues in Floreciente.
10.2	Consider conversion of IL-92 Couplet from one way to two way traffic in Floreciente
10.3	Partner with Rock Island on two way conversion study
Community Engagement	
a.1	Engage the residents of the Floreciente neighborhood to provide community insight into the design solutions.
a.2	Partner with Rock Island on two way conversion study and collectively pursue funding of the joint project
a.3	Utilize information from the 2003 IL-92 Corridor Study
Engineering / Roadway Geometric Analysis	
a.4	Study conversion of IL-92 Couplet from one-way to two-way traffic in Floreciente in partnership with Rock Island
a.5	Improve roadway design to increase vehicular awareness of intersections, slow traffic and improve north/south pedestrian connections.
11.0 Policy: Upgrade railroad crossings to improve pedestrian connectivity	
11.1	Consider improving pedestrian rail crossings at all intersections
11.2	Consider creating a "Quiet Zone" in Moline Centre with appropriate rail crossing improvements.
Engineering / Roadway Geometric Analysis	
a.1	Study strategic rail crossing for potential improvements
12.0 Policy: Support existing transit services with streetscape and signage improvements	
12.1	Improve bus amenities in Floreciente and Edgewater neighborhoods
12.2	Improve access to bus stops and signage in Floreciente and Edgewater neighborhoods
Engineering / Roadway Geometric Analysis	
a.1	Prepare sidewalk and bus amenity inventories
13.0 Policy: Consider Bus Rapid Transit (BRT)	
13.1	In context of other downtown improvements goals and investment, review Bi-State Regional Commission east-west transit service corridor concept for BRT along IL 92 couplet
Engineering / Roadway Geometric Analysis	
a.1	Assess Opportunity for BRT Transit System Design

14.0 Policy: Encourage on-street bike facilities	
14.1	Grow a more comprehensive bike network by increasing more on-street bike facilities. Coordinate the Moline Bikeways Plan with an updated Moline Street Hierarchy map.
14.2	Estimate a specific annual goal of on-street bike facility miles for the next 10 years and coordinate those goals with roadway resurfacing and reconstruction projects.
14.3	Partner with adjacent communities to create on-street connections that extend outside the municipal boundary.
14.4	Implement catalyst projects such as 4th Avenue Multi-use Path with public outdoor events and publicize widely.
14.5	Update signage and establish new connections under I-74 via 4th Avenue as an alternative to the closing of 5th Avenue.
14.6	Insert bike parking within 100 feet of front entrance to buildings with 25 employees or more.
14.7	Improve north-south bike connection to Great River Trail
14.8	Encourage multi-unit residential developments to integrate covered bike parking facilities.
Engineering / Roadway Analysis	
a.1	Moline Centre: Develop 15th Street Sharrow from 5th Avenue to Great River Trail
a.2	Floreciente: Develop 1st Street/46nd Street connecting from 5th Avenue to the Sylvan Island Bridge.
a.3	Edgewater: Sharrow facility and bike boxes on 34th Street, connecting north and south to 5th Avenue, Riverside and Peterson Park. Build sidewalk and multi-use path along 5th Ave. between 27th and 34th Streets in Riverside Park.
15.0 Policy: Continue and expand programs and policies to increase visibility of cycling and safe cycling	
15.1	Continue to support 'Safe Routes to School' program
15.2	Continue to support Moline Police Department bicycle patrol
15.3	Continue to work towards implementation of 2007 River to River Corridor Plan. Update trail alignment to coordinate with new I-74 bridge. Consider 19th Street as potentially part of the River to River Pedestrian / Bike Corridor.
15.4	Establish bike supportive policies for riverfront neighborhoods.
16.0 Policy: Support user choice to take alternative modes by providing bike amenities and fostering a biking community	
16.1	Increase visibility of bike routes through consistent signage
16.2	Engage public in determining bike parking locations.
16.3	Expand bike rental options along the Great River Trail.
16.4	Incentivize outdoor activity organizations to relocate to riverfront areas.
16.5	Update and distribute bikeway network maps to local businesses and institutions
16.6	Improve public access information about bike facilities and programs on City's website.
16.7	Feature local and Quad cities bike community activities and programs on City's website.

4.1 IMPLEMENTATION STRATEGIES



17.0	Policy: Improve pedestrian network and accessibility within the riverfront neighborhoods
17.1	Prepare an area -wide sidewalk audit to identify the pedestrian needs and gaps. Review conditions of sidewalks especially in residential areas and in proximity of parks and open space. Create a strategy for sidewalk improvements.
17.2	Build legible and linear pedestrian connections between neighborhood assets
17.3	Fill-in gaps in the sidewalk network
17.4	Focus pedestrian improvements around transit stops.
17.5	Create new connection to the River and the Great River Trail
17.6	Strengthen connections to neighborhood parks
Engineering / Roadway Analysis	
a.1	Install high visibility crosswalks at along River Drive at 12th Street, 17th Street and 23rd Street.
a.2	Consider intersection pedestrian upgrades on River Drive at 23rd and 34th street intersections
a.3	Improve sidewalk connections from The Q Multi-Modal Station, in all directions. Use aesthetic elements similar to 5th Avenue to visually unify the streetscape into downtown. Design for maximum pedestrian safety.
a.4	Install pedestrian crossing markers across all curb cuts.
a.5	Create plaza with enhanced streetscape at 15th Street and 4th Avenue at eastern edge of Multi-Modal Station.
a.6	Create new pedestrian connection along 15th Street to the Great River Trail.
a.7	Install speed table at 13th street and 4th Avenue approaching Multi-Modal Station.
a.8	Consider pedestrian impacts of potential tie in with two way traffic in Floreciente in the design of 12th Street intersections at 4th and 6th Avenues.
a.9	Establish maximum daily vehicle counts and speeds for determining priorities for intersection improvements.
a.11	Consider installing Leading Pedestrian Intervals at strategic crossings. Priority intersections include all intersections along River Drive and along 4th Avenue.
a.12	Formalize mid-block crossing connecting to the iWireless Entrance with pedestrian signal.
a.13	Install sidewalks on 4th Avenue between 15th Street and 19th Street (reconfiguring parking lots)
a.14	Widen sidewalks and add landscape on 6th Avenue between 12th Street and 16 th
a.15	Add sidewalk on south side of River Drive near WIU
a.16	Install continuous sidewalk on 41st Street from River Drive to 5th Avenue
a.17	Complete sidewalk on 34th Street from River Drive to 5th Avenue.
a.18	Improve pedestrian connection over the rail line from WIU campus to Riverside Park.

a.19	Create connection to Great River Trail at 6th Street. Coordinate with private land owner / Deere & Company
a.20	Widen sidewalks and upgrade streetscape between Stephens Park and community.
a.21	Strengthen gateway to McCandless Park and improve internal network of paths within park.
a.22	Upgrade Velie Park at 5th Avenue - Install pocket park or tot lot, improve internal network of paths, install traffic calming and pedestrian crossing at 11th Street.
a.23	Improve visibility and quality of Sylvan Island Connection through enhanced signage
18.0	Policy: Support growth of downtown businesses by improving access to parking
18.1	Simplify the parking system to improve the user experience through clear wayfinding, signage and messaging on parking regulations.
18.2	Provide accurate and up-to-date information regarding parking availability to all users.
18.3	Reconsider options for leased parking downtown.
18.4	Maximize enforcement so drivers obey parking requirements and parking spaces turnover where desired.
18.5	Focus on efficient enforcement, setting priorities and using data to inform decisions.
18.6	Address the issue of lot jumping through effective enforcement.
18.7	Pilot automatic enforcement measures at strategic locations with a phased approach.
18.8	Establish a comprehensive event parking strategy, with traffic management coordination, consistent communication between parking stakeholders and incorporating additional resources on an as-needed basis.
18.9	Improve wayfinding to public parking and signage to event facilities.
19.0	Policy: Improve wayfinding signage
19.1	Engage a wayfinding/signage consultant to audit the existing wayfinding system, sign design and locations.
19.2	Update and replace signs to incorporate a systems approach for readability and visibility at strategic locations and wayfinding to key assets. Incorporate a multi-mode approach for vehicular, pedestrian and bicycle oriented signs.
19.3	Revisit the signage system developed in 2001 Moline Centre Master Plan.
19.4	Improve directional signage to key destinations such as the Moline Riverfront between 15th street and 19th street, WIU-QC and the Q Multi-Modal Station.

Reference Documents

The following documents were used in developing the recommendations within this document and may be used to provide guidance as the next level of design studies are conducted.

- Designing Walkable Urban Thoroughfares: A Context Sensitive Approach: An ITE Recommended Practice. Institute of Transportation Engineers. 2010.
- Illinois Bureau of Design and Environmental Manual (BDE). Illinois Department of Transportation, Division of Highways. July 2015, Revised March 2016.
- Manual on Uniform Traffic Control Devices for Streets and Highways, 2009 Edition. U.S. Department of Transportation. Federal Highway Administration. 2009.
- Separated Bike Lane Planning and Design Guide. A.S. Department of Transportation, Federal Highway Administration. March 2013.
- Urban Street Design Guide. National Association of City Transportation Officials (NACTO). October 2013
- Urban Bikeways Design Guide. National Association of City Transportation Officials (NACTO). March 2014.



NEIGHBORHOOD VITALITY: Downtown Parking & Access

GOALS

- Streamline parking system to:
 - Enhance user experience
 - Provide signage to make the system more intuitive for user
 - Update public information and signage
 - Reconsider the structure of transit parking locations
- Maximize environment
 - Increase availability of existing parking infrastructure
 - Increase public information and signage
 - Increase public information and signage
 - Increase public information and signage

Existing Signage System



Parking Utilization



Time	Utilization
8:00 AM - 9:00 AM	Low
9:00 AM - 10:00 AM	Medium
10:00 AM - 11:00 AM	High
11:00 AM - 12:00 PM	Very High
12:00 PM - 1:00 PM	High
1:00 PM - 2:00 PM	Medium
2:00 PM - 3:00 PM	Low
3:00 PM - 4:00 PM	Medium
4:00 PM - 5:00 PM	High
5:00 PM - 6:00 PM	Very High

APPENDIX

MOVING MOLINE FORWARD: URBAN DESIGN & TRANSPORTATION UPDATE TO THE 2014 COMPREHENSIVE RIVERFRONT PLAN

JUNE 20, 2016

Appendix SURVEY RESULTS



Survey

As one way of gathering insight into the community's perspective on a variety of topics related to transportation and urban planning ideas for the riverfront neighborhoods, a survey was distributed in via electronic and paper distribution. 185 participants responded to the survey and the results were cataloged and analyzed using a digital database. Survey results confirmed a desire for greater variety of shopping and entertainment options downtown; and a connected sidewalk network, increased opportunities for bicyclists throughout.

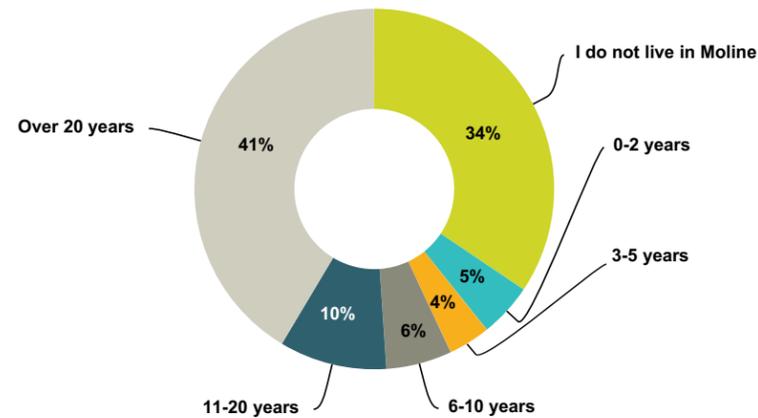
Respondents were a mix of residents, business owners, employees, and shoppers. Of the respondents 67% live in Moline; 42% have lived in Moline for over 20 years; and 11% live within the Riverfront Neighborhoods. Over half of the respondents work in Moline, while 15% work in Iowa.

Bilingual Engagement

In recognition of Moline's diverse community, meeting invitations, flyers, surveys, and meeting materials were distributed in Spanish as well as English. Additionally, spanish speaking representatives were available as translators at the public open house.

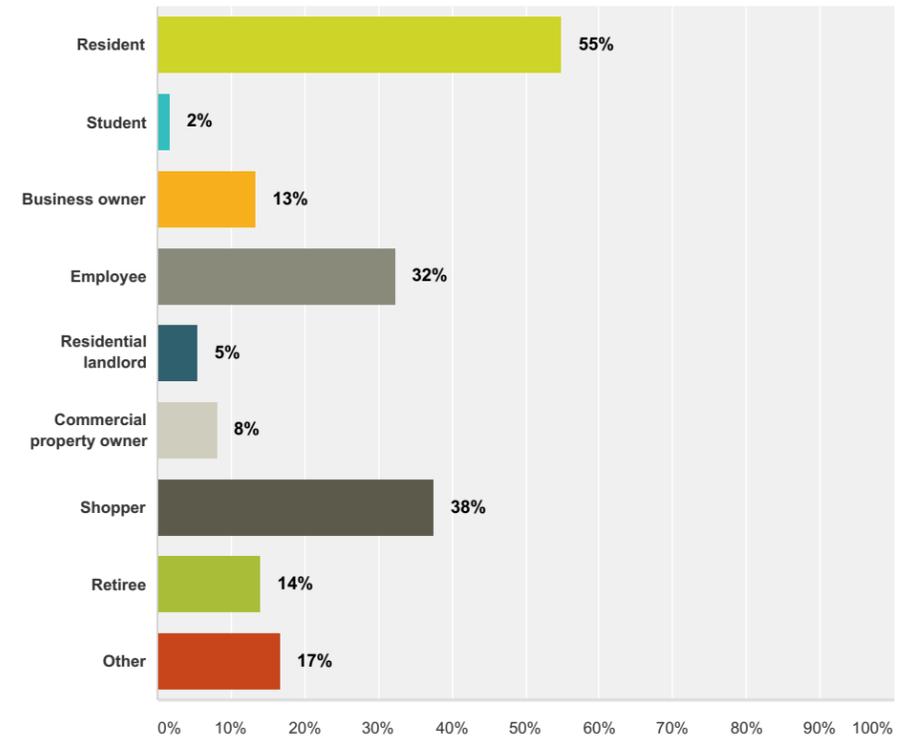
Q1 How many years have you lived in Moline?

Answered: 186 Skipped: 1



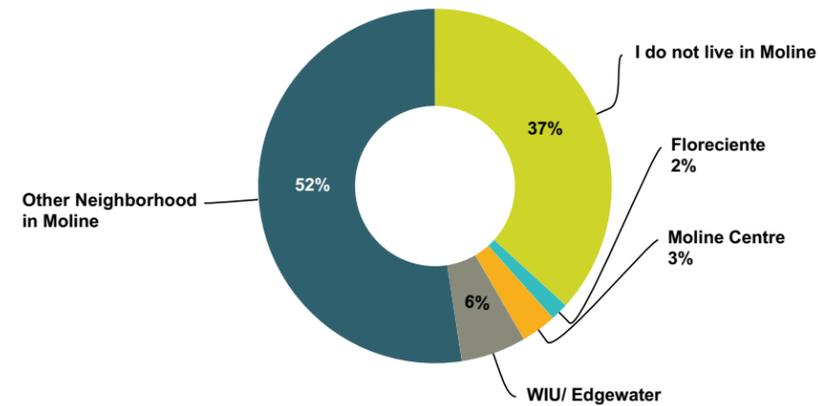
Q3 Please describe your role within Moline (select all that apply)

Answered: 186 Skipped: 1



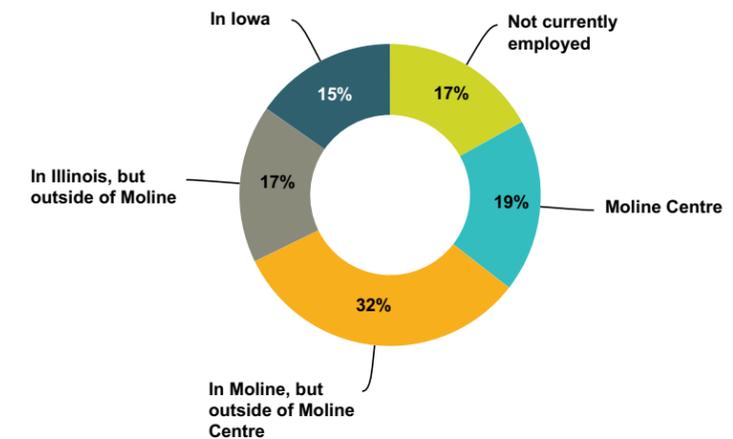
Q2 In which neighborhood do you live?

Answered: 185 Skipped: 2



Q4 Where do you work?

Answered: 183 Skipped: 4

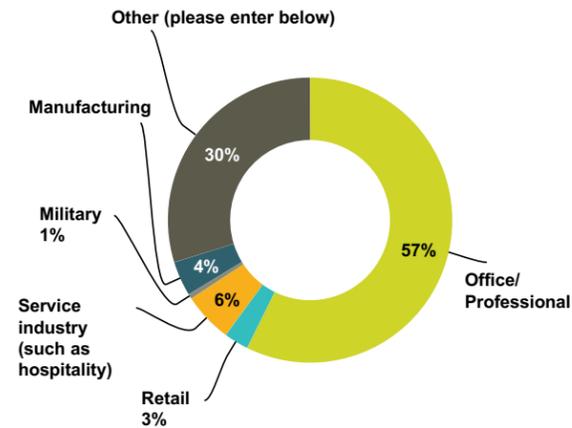


Appendix SURVEY RESULTS



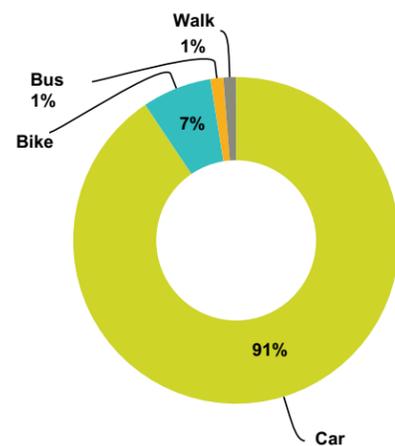
Q5 How would you describe your type of work?

Answered: 178 Skipped: 9



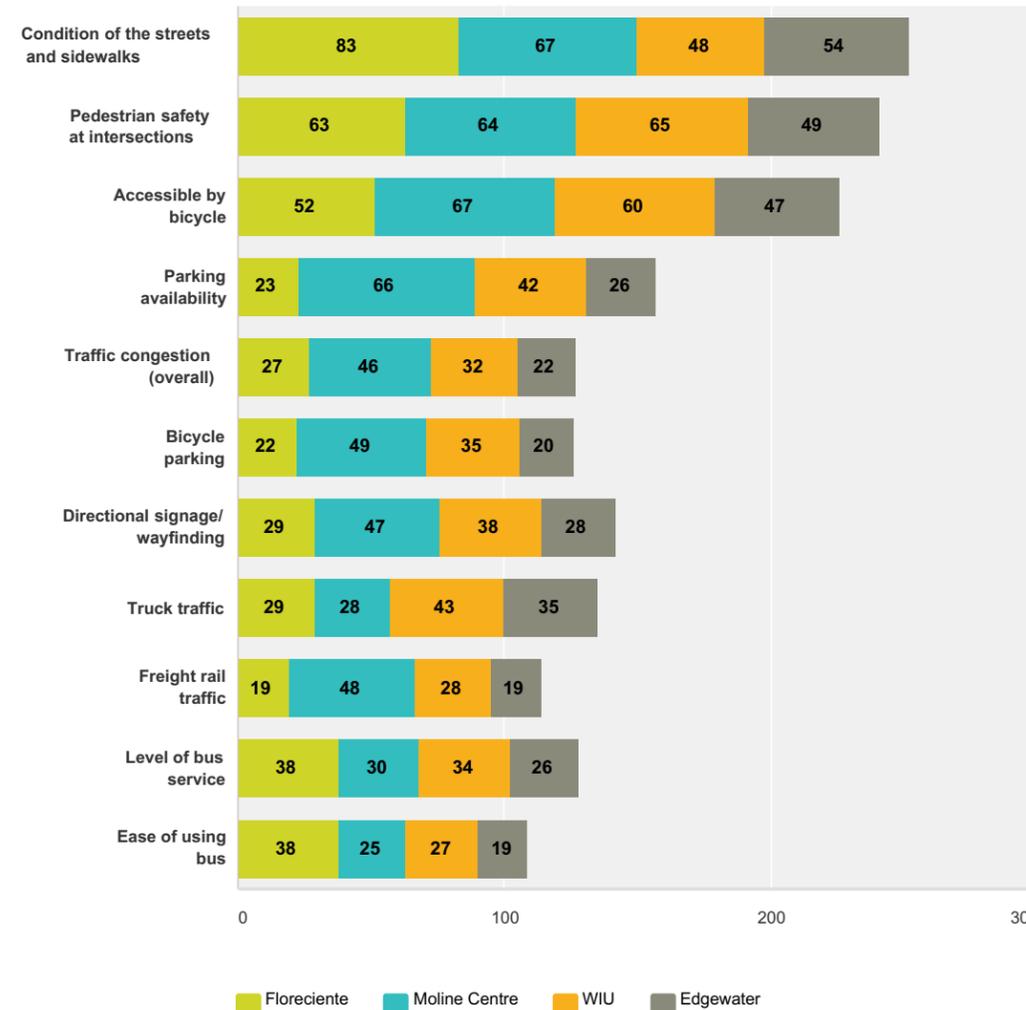
Q6 What is your primary mode of transportation?

Answered: 159 Skipped: 28



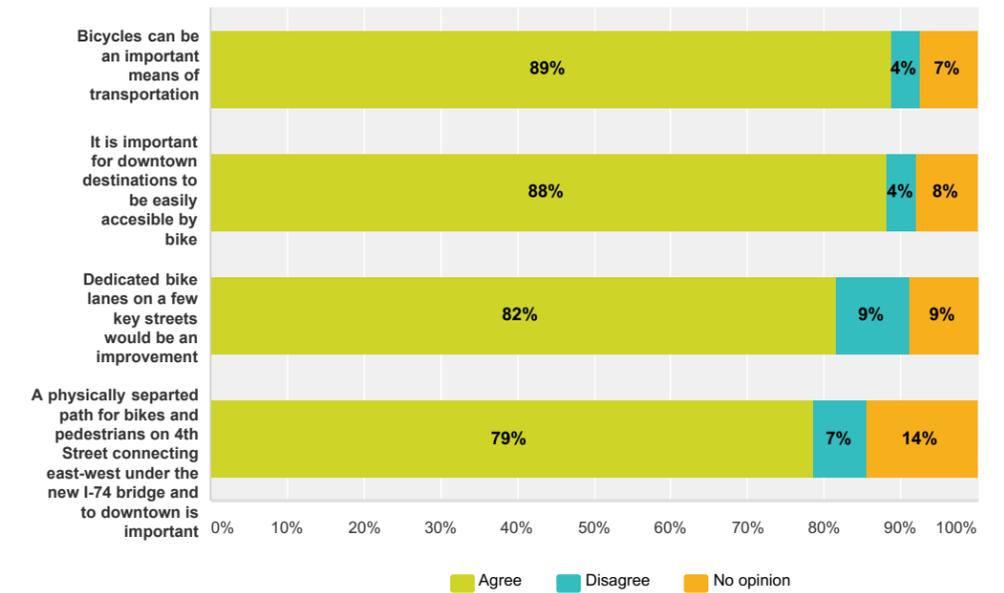
Q7 Which of the following transportation-related items are the most important to improve? (choose up to 4 for each area)

Answered: 156 Skipped: 31



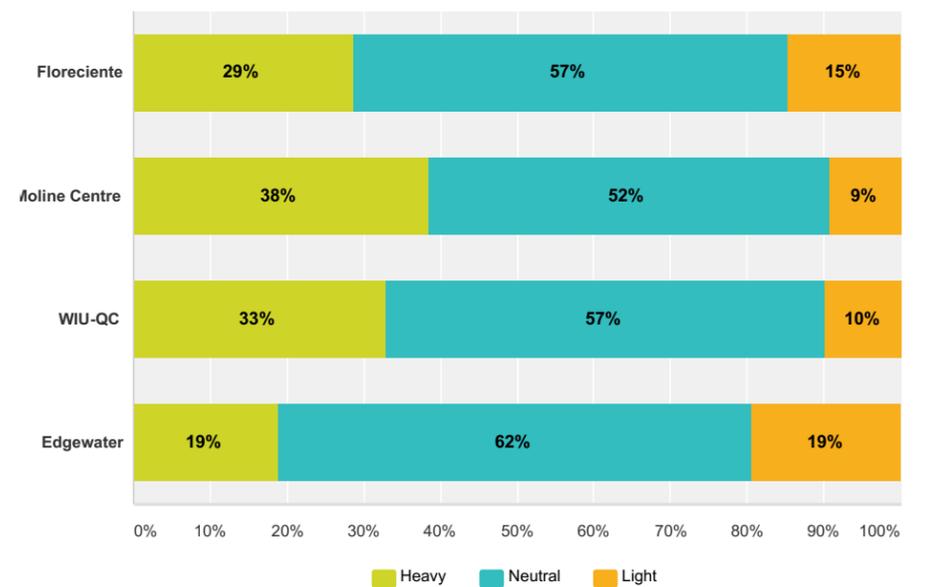
Q8 For the following statements please select agree, disagree, or no opinion

Answered: 161 Skipped: 26



Q9 How would you rate traffic congestion in the following neighborhoods?

Answered: 155 Skipped: 32

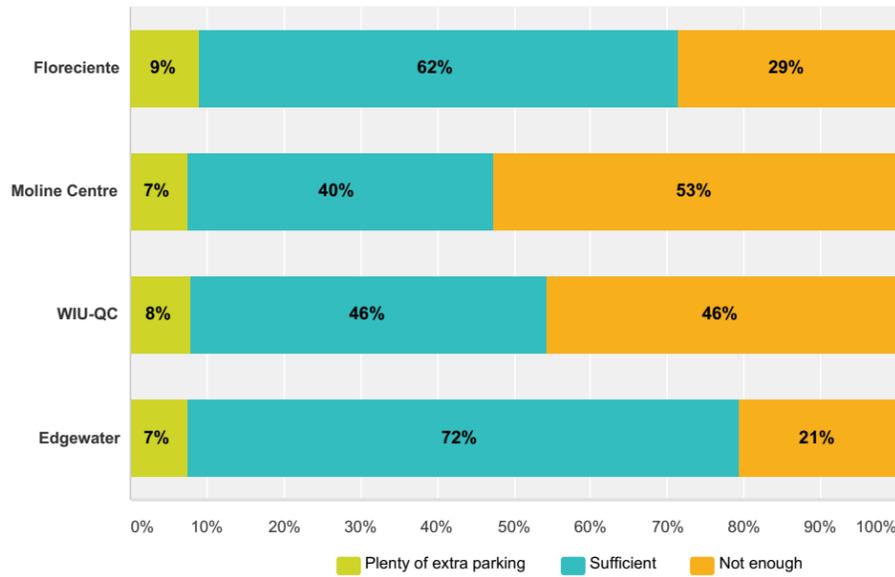


Appendix SURVEY RESULTS



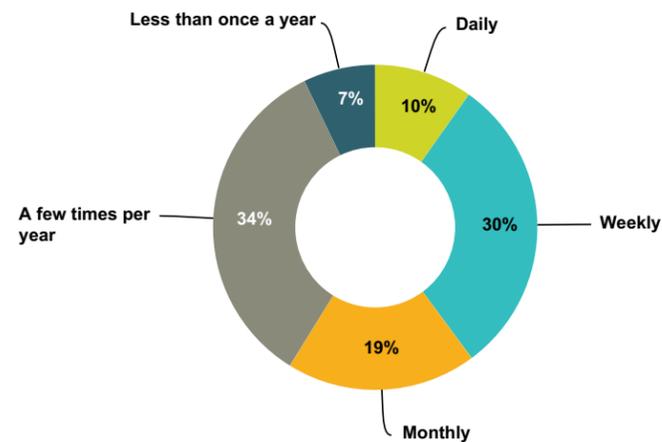
Q10 How would you rate the availability of parking in the following neighborhoods?

Answered: 153 Skipped: 34



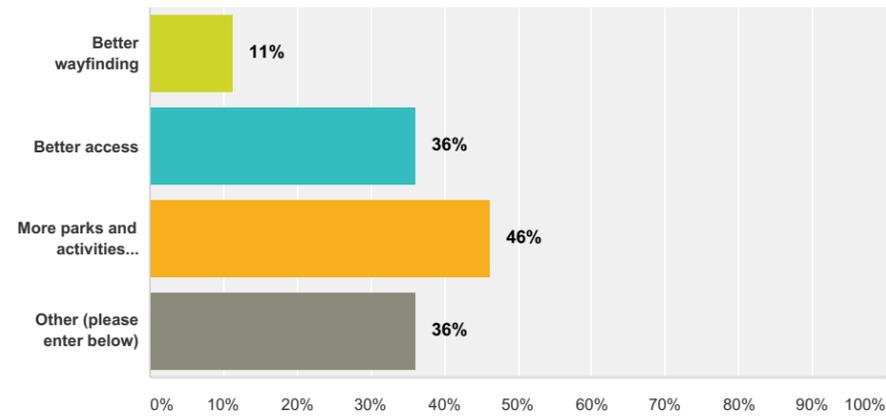
Q11 How often do you access to the riverfront trail?

Answered: 153 Skipped: 34



Q12 What would increase your frequency of use of the riverfront trail?

Answered: 141 Skipped: 46



Comments below are taken directly from survey responses and have not been edited

#	Other (please enter below)
1	More police presence
2	I know the Mississippi is muddy, why not build a public beach (with sand). It could be a draw to Moline to build a beach on the riverfront. People like to layout in the sun. Also, build a water park.
3	Connecting bike lanes or trails
4	Mor bicycle friendly
5	surface improvement
6	Kept free of debris
7	smoother and wider trail
8	Making time over other activities
9	Music on the trails
10	Time
11	More free time
12	Waterfront Restaurant
13	More trees, I would like to run there but not much shade
14	les bicycle traffic
15	More maintenance. Resurfacing
16	I use the path as a means of transportation but the ease of access to it traveling north and south is lacking.
17	Mire green space along the river front. Possibly a facility or park like LeClaire park in Davenport. River view restaurant a must.
18	Make sure it remains open, especially by iWireless
19	Personal time/priority
20	Fewer pedestrians on the bicycle path
21	need to fix poor drainage along the riverfront trail

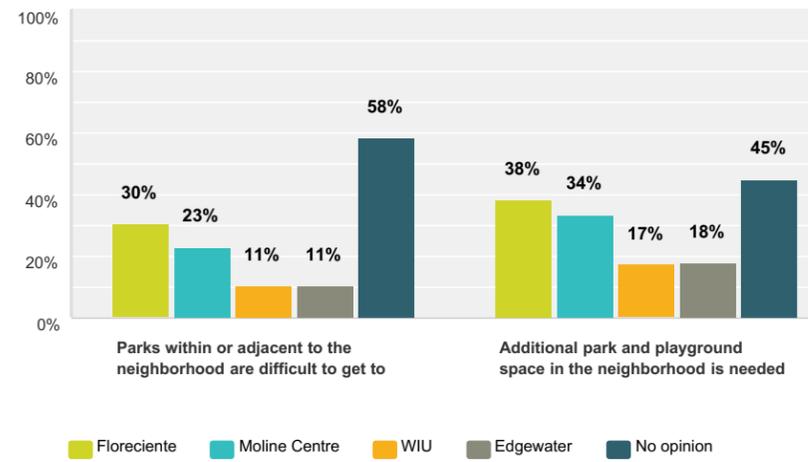
22	better attention to trail conditions--poor condition of path leads to major puddles
23	Become part of a grander Illinois QC Bicycle Loop
24	Less congestion bikes vs pedestrians. Also water over the bike path is a problem at times. Feeling safe is some areas, mostly going west from the downtown area. Some areas seem a bit isolated to me.
25	Better connections across the Mississippi River
26	Improving the surface on the area near the college other wise it is well maintained and a great trail.
27	became a caregiver for wife cutting my rides on the path
28	Butterworth parkway walk/bike path needs to be redone.
29	Don't have to do anything, we love it there!
30	consistent winter snow/ice removal
31	Less traffic
32	bike path across new i-74 bridge
33	More bike paths/lanes to and from the actual bike path along the riverfront to other parts of the city.
34	EVENTS
35	parking
36	Resurface and widen when possible
37	Time
38	I love the riverfront - more options to get to the path via bike would be great as well as more parking areas to get there by car
39	Measures taken to feel safe if walking alone.
40	Improved bife trail - trail getting very rough in places
41	More restrooms
42	Get Sylvan Island accessible, could be great Tourism
43	Committing myself to use it.
44	More food vendors
45	Connection to a passage across the river at I-74
46	Time!
47	More time off
48	Better river shoreline access. Too many fences.
49	bike rental service like Chicago has with Stations and a subscription for it
50	Lighting along trail...emergency call boxes...aura of safety
51	More time :)

Appendix SURVEY RESULTS



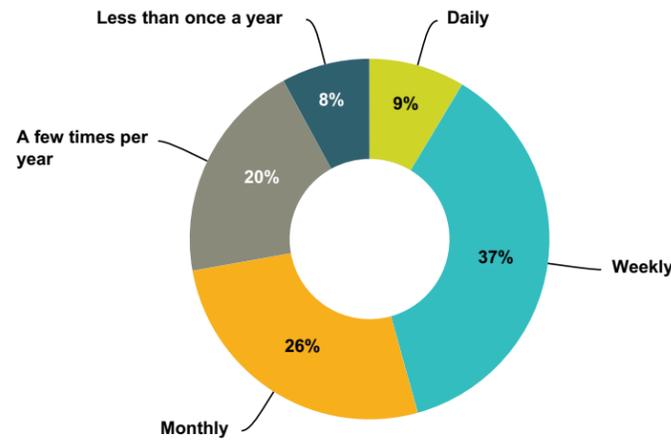
Q13 For the following statements, select the neighborhood or neighborhoods that you agree with the statement or select no opinion.

Answered: 146 Skipped: 41



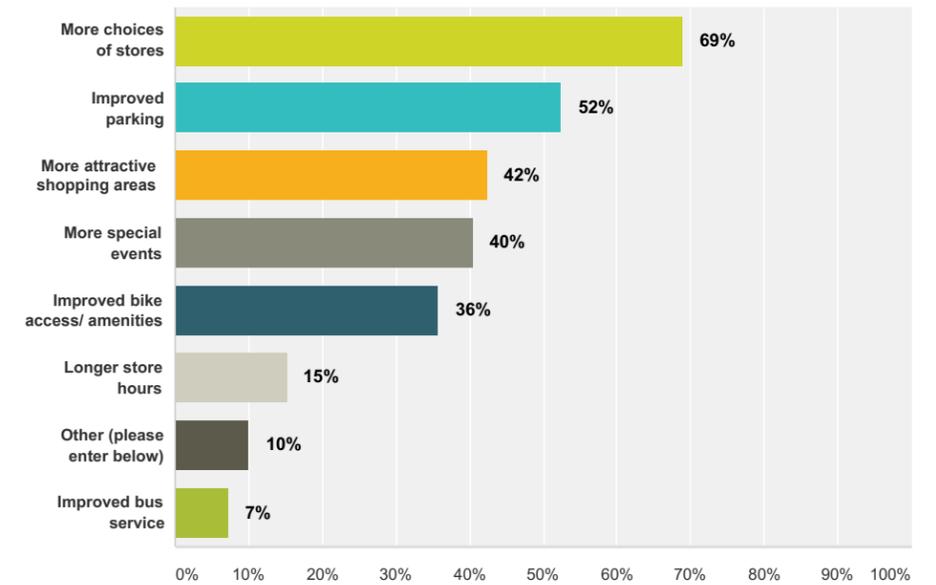
Q15 How often do you visit Moline Centre for activities other than work/school?

Answered: 151 Skipped: 36



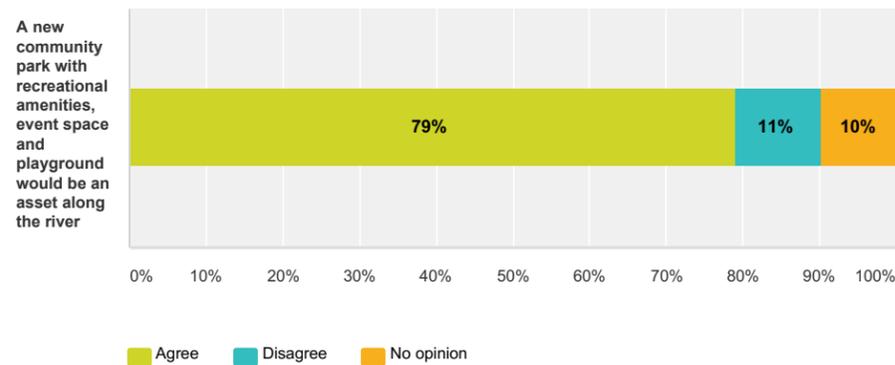
Q17 What would increase your interest in Moline Centre? (select all that apply)

Answered: 151 Skipped: 36



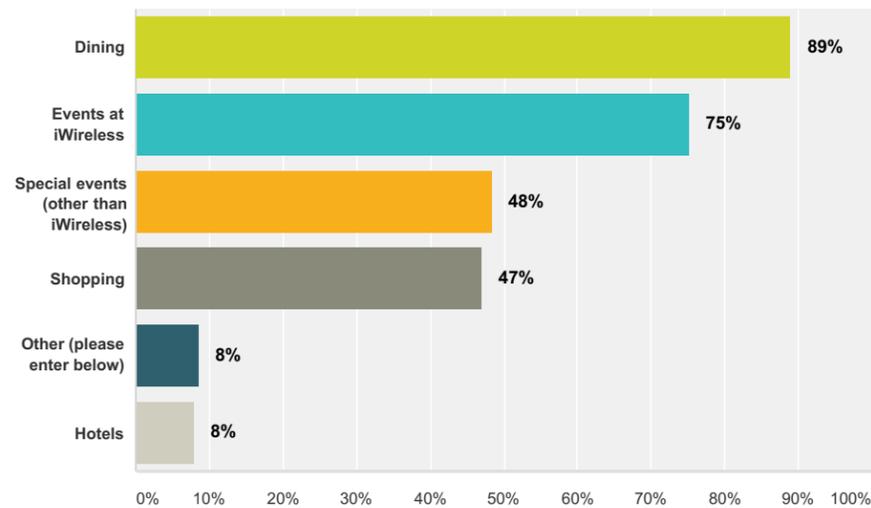
Q14 For the following statement, select if you agree, disagree or no opinion

Answered: 152 Skipped: 35



Q16 What types of leisure activities do you attend in Moline Centre? (select all that apply)

Answered: 153 Skipped: 34



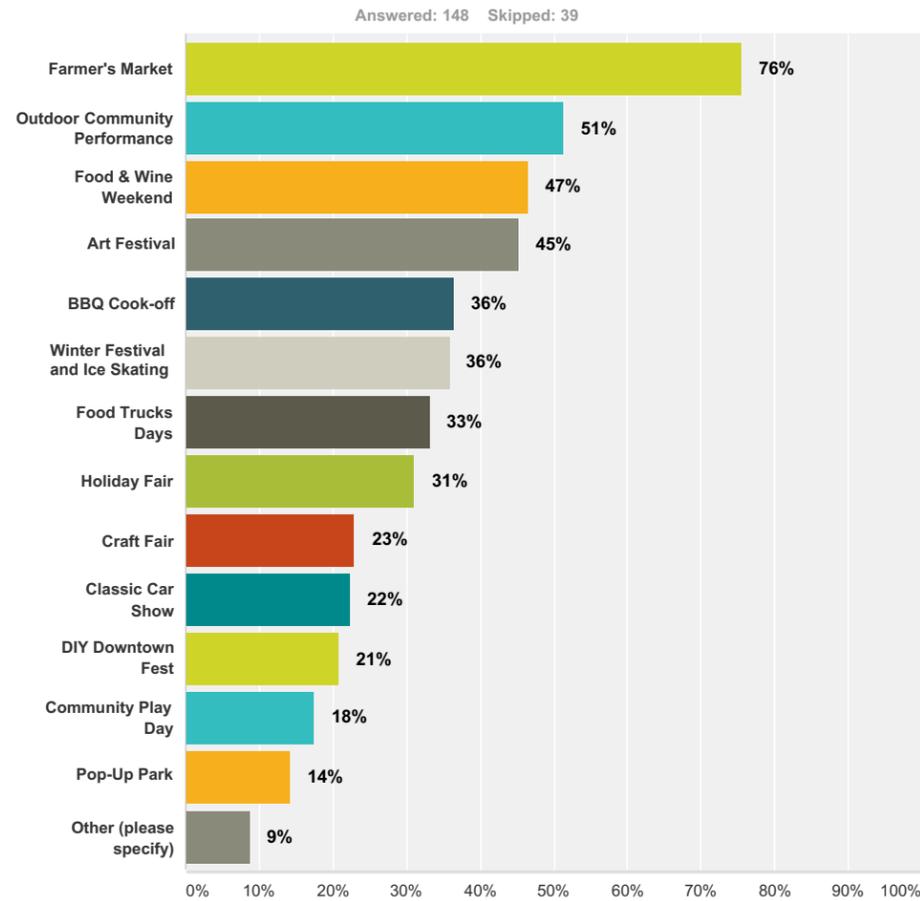
Comments below are taken directly from survey responses and have not been edited

#	Other (please enter below)	Date
1	Need to continue on the same path of attracting good restaurants and bars.	12/8/2015 2:08 PM
2	restaurants & bars	12/3/2015 2:16 PM
3	none	12/2/2015 9:50 AM
4	No comment	12/1/2015 9:04 AM
5	nice grocery store!	11/29/2015 5:12 PM
6	none as homebound so much of the time	11/25/2015 6:09 PM
7	I think this will come with the new rail station	11/25/2015 3:07 PM
8	more bike lanes to get there from other parts of the QCA	11/25/2015 8:29 AM
9	RAIL SERVICE	11/25/2015 8:12 AM
10	IMPROVED PARKING!!!	11/24/2015 9:34 AM
11	More family oriented restaurants.	11/23/2015 5:25 PM
12	there is a need for a grocery store/service	11/23/2015 4:47 PM
13	Grocery store. La Primavera is only viable choice at the moment.	11/23/2015 4:09 PM
14	Added variety; slow traffic flow, add parking to one-ways	11/23/2015 4:00 PM
15	more food options	11/23/2015 3:29 PM

Appendix SURVEY RESULTS

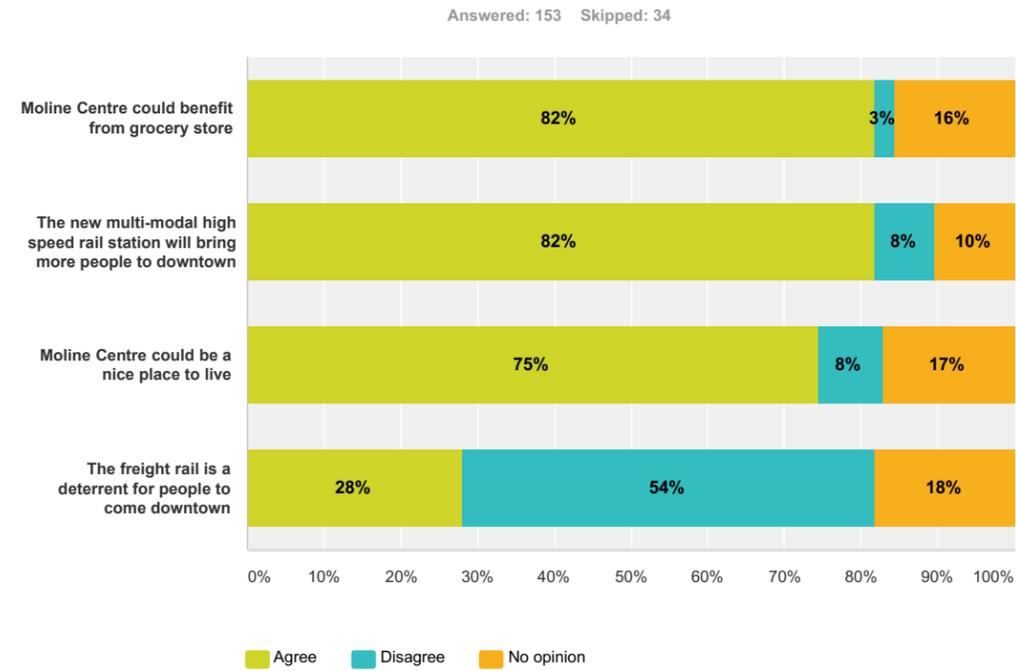


Q18 What types of special events would you like to see downtown? Select up to 5

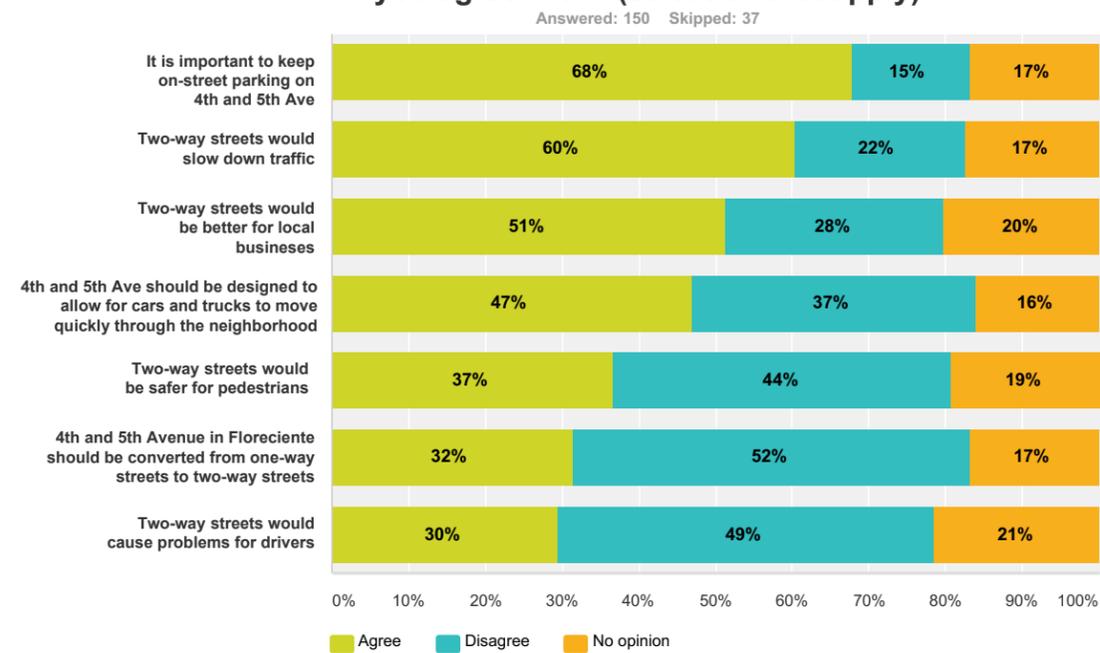


#	Other (please specify) Comments below are taken directly from survey responses and have not been edited	Date
1	The replicating the farmers market that Davenport had would be perfect for Moline.	12/8/2015 2:08 PM
2	sidewalk dining. Moline killed it just as it was taking off.	12/7/2015 11:34 AM
3	Diverse music entertainment (Latin/Hip Hop/House)	12/3/2015 3:20 PM
4	none	12/2/2015 9:50 AM
5	Bike Moline Activity in three areas on survey	11/30/2015 8:44 AM
6	walking tours of historic places	11/29/2015 11:54 AM
7	RAIL SERVICE	11/25/2015 8:12 AM
8	More outdoor music, including all genres.	11/24/2015 9:34 AM
9	Sidewalk/street Chalk Festival	11/24/2015 9:10 AM
10	outdoor movie showings	11/24/2015 7:18 AM
11	Steam Railroad Engines	11/23/2015 4:22 PM
12	Lighted Recreation areas under new bridge	11/23/2015 4:00 PM
13	Currently have enough special events	11/23/2015 3:29 PM

Q19 For the following statements please select agree, disagree, or no opinion



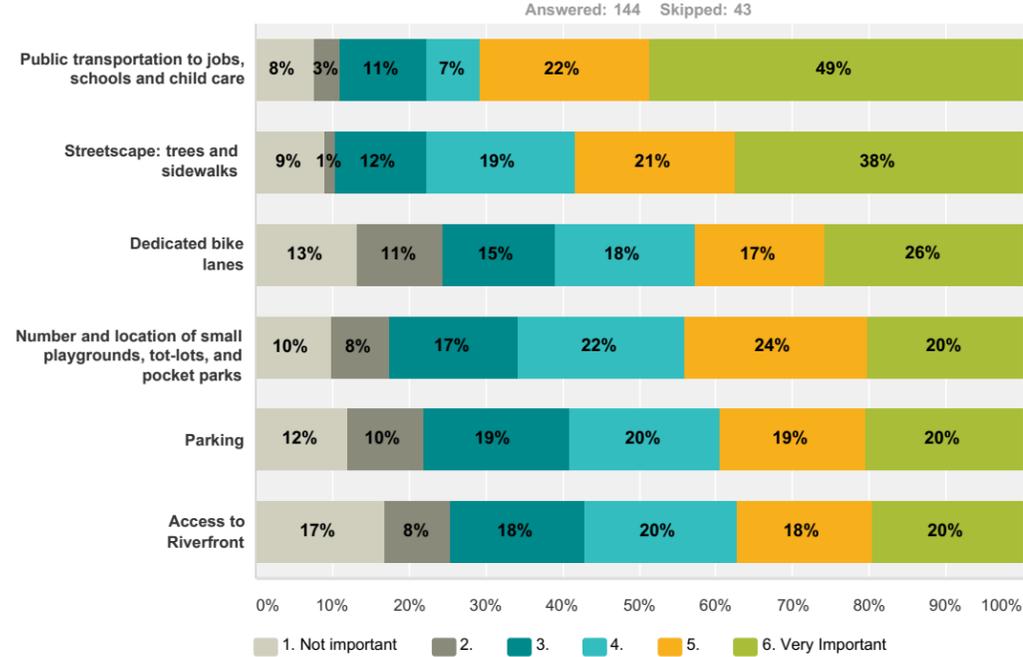
Q20 . Which of the following statements do you agree with? (select all that apply)



Appendix SURVEY RESULTS

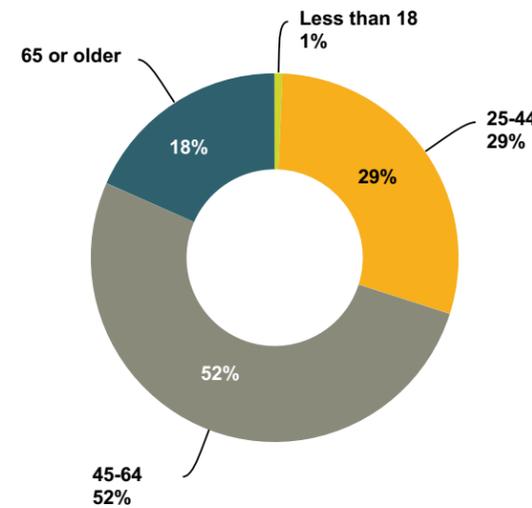


Q21 Which of the following items are most important to improve in Florencia?
(Rank 1 to 6, 6 being the most important)



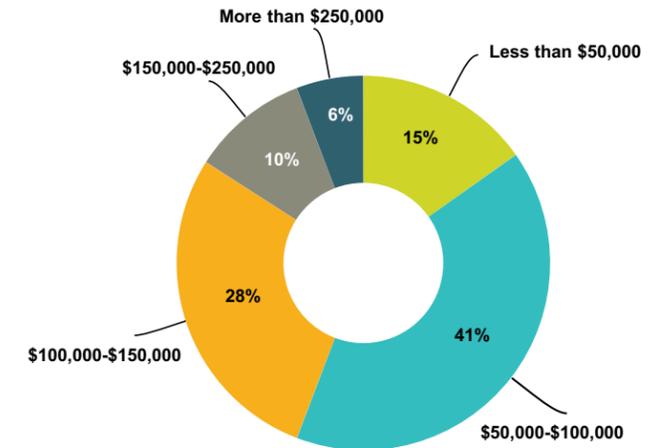
Q23 What is your age?

Answered: 147 Skipped: 40

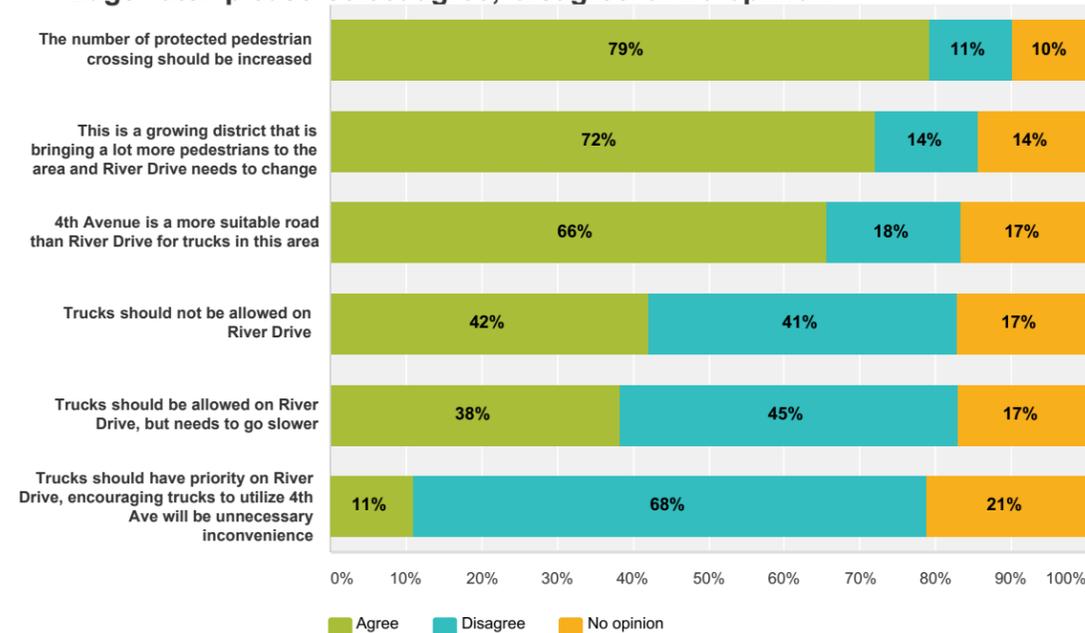


Q25 What is your annual household income?

Answered: 138 Skipped: 49

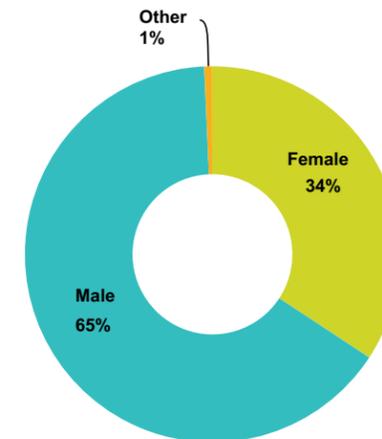


Q22 For the following statements related to River Drive through WIU-QC and Edgewater please select agree, disagree or no opinion



Q24 What is your gender?

Answered: 146 Skipped: 41



Appendix SURVEY RESULTS



Q26 Is there anything else you would like to tell us about land use and transportation planning in Floreciente, Moline Centre and Edgewater? (Please fill in the blank)

Answered: 49 Skipped: 138

Comments below are taken directly from survey responses and have not been edited

#	Responses	Date
1	Need buses to go later at night. Especially if they go to downtown activities cannot get home by bus.	1/5/2016 10:03 AM
2	Get Sylvan Island opened, possible use of aluminum or composite materials for repairs?	12/17/2015 9:20 AM
3	Most land space is wasted. I can barely bike to school.	12/16/2015 2:02 PM
4	Mas juego o parque para ninos y bilancia.	12/16/2015 1:49 PM
5	None.	12/16/2015 1:41 PM
6	Improve existing parks and focus on things that school aged kids would enjoy. Also, add in more outdoor exercize stations near the parks.	12/16/2015 1:34 PM
7	WE NEED A GROCERY STORE!!!	12/8/2015 2:11 PM
8	moving truck traffic off river drive can only be done if you get rid of tight curves on 4th av. tear down some of the blighted houses on points and expand the lanes.	12/7/2015 11:39 AM
9	Access to Milltown coffee from bike path should be improved	12/4/2015 9:43 AM
10	I suggest that a map depicting these three areas of the City should be included in the survey, as not all respondants may be properly informed on their locations.	12/3/2015 1:50 PM
11	Need a parking garage at 12th and River Dr.	12/3/2015 1:39 PM
12	Make the speed limit on River Drive 25mph and enforce it!	12/2/2015 9:53 AM
13	It would be nice to have better access to river front trails in the moline center area. and restraunts with a river view in moline center areacor florentine area to boost neighborhood.	12/1/2015 10:23 AM
14	I think edge water has great potential and we need to invest in it.	12/1/2015 9:04 AM
15	The new I-74 bridge will channel many more people into Moline Center and Edgewater both from all around the QC and throughout the region. The bridge will be a destination bridge and will require trail head parking close by. May be an opportunity to have a community park close to that trailhead, much like Scweibert Park in Rock Island. Families, fun, and food and the rest will follow! Thanks for the survey opportunity!	11/30/2015 9:12 AM
16	I thought the consultants could have been better prepared. The ideas were nice and most of all I thought most were affordable.	11/29/2015 5:18 PM
17	enhance pedestrian street crossings	11/29/2015 11:57 AM
18	we need to use smaller buses for in city usage more economical	11/27/2015 4:04 PM
19	need to get rid of abandoned property. Fix streets	11/27/2015 12:14 PM
20	Need more affordable housing for lower income family's ... Since middle class and above are being catered too but retirees on fixed or low income families have very little access or housing with access	11/27/2015 11:03 AM
21	There are too many poorly placed curbs by the old depot.	11/25/2015 8:36 PM
22	We need to be able to take bicycles on Amtrak when it comes.	11/25/2015 8:36 PM
23	no	11/25/2015 8:03 PM
24	keep tree planting away from path to stop heavs by the roots	11/25/2015 6:16 PM
25	Fixing the horrible roads at the east end of Moline!	11/25/2015 4:00 PM

26	The River is the big draw for the area. Do not need bike lanes on streets, people want to ride on trails as they are safer.	11/25/2015 3:13 PM
27	Edgewater needs to be the focus of development.	11/25/2015 2:41 PM
28	Better connections between Avenue of the Cities and Downtown are needed. Hard to get bus/bike connections from uphill to downhill parts of Moline.	11/25/2015 9:32 AM
29	TIF district for Edgewater for high rise condos and restaurants some great undeveloped land on west River Drive.	11/25/2015 9:12 AM
30	Downtown Moline has good bones. The City should encourage density and compactness with small-scale infill development. The city should consider a form-based code or overlay, so new development fits the character of the historic downtown. Better crosswalks and more parks (which cost money to maintain) alone will not make the city a more active place to be. The urban design, the connection between land-use and transportation, is incredible important to get right.	11/25/2015 9:04 AM
31	I understand this is for targeted neighborhoods, but the city could use more bike lanes throughout the city and connecting parts of the city with the riverfront.	11/25/2015 9:02 AM
32	parking, parks, paths	11/25/2015 8:36 AM
33	Do NOT need sidewalks there is a parkway!	11/25/2015 8:32 AM
34	The Rail Service Project Should Speed Up	11/25/2015 8:18 AM
35	no	11/25/2015 8:04 AM
36	Defenitely need a large riverfront amenity like Shwiebert Park or Davenport Levy, etc with food vending, band shell, etc	11/24/2015 4:14 PM
37	Save the Moline Depot if possible.	11/24/2015 11:20 AM
38	More designated free parking that is clearly marked. Better roadway markings with paint and signs for turn lanes.	11/24/2015 11:09 AM
39	Better bicycle and pedestrian accomodations in all of Moline with an emphasis on a better bicycle network. Also, Moline needs to use the Bikeways Plan as guidance on every project in Moline with a Complete Streets focus on design.	11/24/2015 10:17 AM
40	A dedicated signed pathway from Moline Centre to the Mississippi River walkway	11/24/2015 9:15 AM
41	Keep community input, working well.	11/24/2015 5:47 AM
42	Traffic should be designed to move people faster. Consider turnabouts. There are lots of people who travel to IA simply because it is faster to get around over there.	11/23/2015 10:38 PM
43	This is a very intellectually dishonest survey. Increase business for more tax dollars. Stop worrying about bikes paths and parks until there is money to spend on them.	11/23/2015 8:31 PM
44	Changing the one ways to two way streets will increase air pollution as cars, trucks & busses will be stopped more often.	11/23/2015 5:29 PM
45	Remember existing businesses along River Drive	11/23/2015 4:25 PM
46	Extend Riverdrive improvements through Edgewater	11/23/2015 4:16 PM
47	As rentals become more popular and increase in number in Downtown Moline, the parking capacity will be reached. Many of the buildings in which tenants live are renovated or refurbished buildings with no dedicated parking. Where will all the tenants park? This is especially a problem now from 6-8pm during dinner hours.	11/23/2015 4:15 PM
48	Figure out how to use railroad right-of-way through these areas	11/23/2015 4:07 PM
49	The Floreciente area is in DIRE need of a public library and the City closed down the nice Carnigie one we had - bad decision.	11/23/2015 4:05 PM

PARKING MEMORANDUM



Memorandum

To: Jeff Anderson, City of Moline
 Date: April 27, 2016
 Re: Parking in Moline Centre
 Project Name: Planning and Transportation Update to the Comprehensive Plan/Riverfront Sub-Area Plan

Moline's downtown is a local and regional draw because of the number and variety of destinations located within its borders. It is vital for Moline to provide easy access to available parking to support the needs of residents, visitors, and vested businesses. Construction of a new I-74 Bridge, the Multi-modal Station, and other developments in Moline Centre emphasize the need for the City of Moline to manage the demands on their current parking inventory, and to better understand how to most effectively use their resources in the future. As development continues in Moline Centre, parking will become a more complex issue. As existing parking lots serve more residents and a growing business center, ensuring the availability of convenient spaces will be more and more difficult.

Sam Schwartz Engineering (SSE), as part of our work as a part of the Solomon Cordwell Buenz team to complete the Planning and Transportation Update to the Comprehensive Plan, conducted a parking study for Moline Centre. Parking in downtown is an issue discussed in the Moline Centre Master Plan Update, Illinois Highway 92 Corridor Study, and the Moline Comprehensive Plan Updates. Each of these plans identified similar needs and challenges, but contain little consensus on recommended improvements. Stakeholder interviews confirm the issues identified in all plans, which range from insufficient supply of parking in close proximity to destinations, to a lack of wayfinding and signage on existing facilities.

This memorandum summarizes our team's understanding of the existing conditions of parking facilities, operations and management in downtown and provides preliminary recommendations. These recommendations address the needs of the user and streamlined operations for city-owned parking assets. This memo serves as one part of a larger effort to develop a transportation update to the City of Moline's Downtown Neighborhoods Comprehensive Plan, to better position Moline to manage its assets in the context of future development and growth in downtown.

Existing Parking Supply and Use

The City of Moline is responsible for approximately 1,500 on- and off-street parking spaces within the Moline Centre neighborhood in the area bounded by 12th Street to the west, I-74 Bridge to the east, 7th Avenue to the south, and north to the Great River Trail. While parking in this area is comprised of on-street spaces, public and private surface parking lots, and public parking ramps, the primary focus of this study concerns the use and regulation of off-street, city-owned parking lots and ramps. All downtown parking facilities, visualized by ownership and type are, shown in **Map 1** in the Memorandum Reference Materials. The City's off-street public parking options include facilities with varied time restrictions including "Free Parking 24 Hour," "Free Parking 3 Hour," "Free Parking 1 Hour," as well as leased parking.

5th Avenue, between 12th and 19th Streets, is the commercial heart of downtown, with a variety of dining destinations mixed with local businesses. The John Deere Commons and Bass Street Landing, located along River Drive between 15th and 17th Streets, offers outdoor entertainment venues as well as a mix of office, residential, retail and dining options. The City's individual public parking lots have capacity ranging from 13 to over 200 spaces. Along with the free on-street spaces on most streets, parking options are distributed throughout downtown and are convenient to all destinations.

Additionally, many businesses own and operate private lots scattered throughout the City. Many of these are located to the rear of each business with access from the network of alleys bisecting downtown blocks. The largest private lot in the study area is owned and operated by the iWireless Center located between the Mississippi River and River Drive. This lot provides over 900 spaces and parking is pay-to-park during the 70+ major events the iWireless Center hosts each year. The price of parking in the iWireless lot varies, but is typically \$10.00 per car. This price is determined by the iWireless Center for each event.

Currently, the naming convention the City uses for its public parking assigns a letter to each lot with the exception of the three lots adjacent to the John Deere Commons called "visitor." The parking ramps are referred to by name: Centre Station Ramp, Heritage Ramp, and Midtowne Ramp.

For the purpose of this study, SSE assigned a unique reference number to each public lot and ramp, and the private lots of particular interest. The maps included in this study show facilities labeled with both the SSE reference number, and the City's assigned letter, where possible, shown as "Letter - Number". All maps and reference materials can be found in the Memorandum Reference Materials at the end of this document.

On-Street Parking Supply

The majority of streets within the downtown study area allow for on-street parking amounting to over 550 on-street parking spaces. All parking spaces in downtown are free. As with the City's

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off-street lots, the majority of on-street parking spaces are designated with time-specific signage similar the one shown to the right. Approximately 90% of street parking is “Free Parking 2 Hour.” Other on-street parking restrictions include free 15 minute, 1 hour, and 3 hour parking¹. All on-street options are enforced 8:00 am – 5:00 pm except weekends and holidays and prohibit “space jumping.”² **Map 2** in the Memorandum Reference Materials shows street segments within the study area designated as on-street parking.



Existing On-Street Parking Occupancy

Parking time restrictions are used to encourage the turnover of on-street parking spaces. To better understand the availability of street parking in Moline Centre, the City of Moline conducted an on-street occupancy study of 13th, 14th, 15th, 16th, and 17th Streets, and 5th and 6th Avenues. Three counts of occupied spaces were taken on a Saturday [in the morning, midday and late afternoon] and averaged. This average represents a typical number of cars parked on each street on any given weekend day. The mean divided by the street’s total capacity yields the average daily occupancy.

Table 1: Weekend On-Street Parking Occupancy

Street Name	Space Count	Average Daily Occupancy	Peak Occupancy Surveyed
13th Street	8	50%	63%
14th Street	32	31%	47%
15th Street	22	59%	91%
16th Street	26	32%	42%
17th Street	44	42%	61%
5th Avenue	62	56%	84%
6th Avenue	16	60%	81%

The data shows that 15th Street and 5th and 6th Avenue have the three highest occupancy rates ranging from 50% - 60%. Higher occupancy is to be expected in light of 5th Avenue’s concentration of retail and dining options and 15th Street’s position as a central north-south connection, with access to both 5th Avenue and the John Deere Commons. Industry standards suggest that occupancy over 85% is nearing capacity as people would likely need to circle the block to look for spaces. Despite showing the highest daily occupancy rates of the streets surveyed, all occupancy rates show that on-street parking is relatively easy to find along streets in the study area. On a block with on-street parking utilization between 50-85%, the street would appear busy, but spaces a driver would likely be able to find a space without circling the block. This is in line with the observations conducted by the project team.

¹ The few 1- Hour spaces on-street were acknowledged as a City oversight; to be removed/replaced.

² Space Jumping is defined as any person moving any vehicle from any designated zone parking space to any other space within a similarly designated zone.

Off-Street Parking Options

The City is responsible for enforcing a total supply of 21 surface parking lots and 3 ramps within the study area, with an approximate capacity of 1,040 spaces. There are four different types of off-street parking facility uses in Moline Centre: public lots and ramps – within which there are spaces leased by the City of Moline, and private lots and ramps. The City is not responsible for enforcement in private ramps or lots.

The City of Moline website provides basic information regarding public and leased parking in downtown, as well as a map showing facilities color-coded by type and time restriction. Information about the use and time-restrictions are also posted at each lot. The City has created a website that maps these lots and details their regulations and approximate capacity, and includes an aerial image of each lot. The City’s parking map is included as **Reference Map 03**.

Public Surface Parking Lots

Of the 21 off-street public surface parking lots, there are six different regulation types: “Free Parking 24 Hour,” “Free Parking 3 Hour,” “Free Parking 2 Hour,” “Free Parking 1 Hour,” Discounted Leased Parking, and Leased Parking. Lots labeled T and W on the City’s map are mixed use, combining regulations “Free Parking 3 Hour” with spaces reserved for “Discounted Leased Parking”. Signs posted at individual lots display the regulation and times enforced. On-site verification found several inconsistencies between the parking map posted on the City’s website, and the uses posted at the lots themselves. All public parking facilities are free for public use after 5:00pm on weekdays and weekends. **Map 04** included in the reference materials shows the city-owned parking lots and ramps by current use, confirmed on site.

The “Visitor Lots” shown on the City’s parking map - adjacent to the John Deere Commons - are “Free Parking 24 Hour.” These lots were given no time restriction through an agreement with John Deere to facilitate longer visitor stays at the John Deere Museum and other nearby activity centers. Signage within these lots states “Parking for Customers of John Deere Commons.” Lots with these time restrictions can be challenging to enforce.

Parking Ramps

In the City Centre there are three existing parking ramps: Centre Station, Heritage Place, and the Midtowne Ramps. All ramps have regulations that are enforced Monday – Friday 8:00 am – 5:00 pm with the exception of holidays. The Midtowne and Heritage Ramps are free public parking outside the weekday, daytime enforcement. **Table 2** shows the stall counts and posted regulations in each of these parking ramps.

The Centre Station parking structure houses Moline’s bus depot which occupies the first floor of the structure and is home to MetroLINK bus service. The second and third floors are reserved for employee parking on Monday through Friday, 8:00am – 5:00pm, for various major nearby employers. Over holidays, weekends, and weekdays (only after 5:00pm) spaces are open to the public. During major events at the iWireless Center, spaces within this lot are either reserved for arena suite-holders or pre-sold with event tickets. The ramp parking is connected to iWireless via an enclosed skywalk and the John Deere Commons. Centre Station is not highlighted as a

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key facility supporting commerce and retail in the downtown parking supply because of the ramp's use as weekday employee parking under a long-term contract with MetroLINK and the Illinois Quad City Civic Center Authority, parking only available on upper floors and specific times of day, and unique use to support event parking.

Like the Centre Station Ramp, the Heritage facility operates as a private parking ramp Monday – Friday 8:00 am – 5:00 pm, except holidays. This ramp is currently leased to the current owner of the Heritage Building. Through this agreement, all spaces within the Heritage Ramp are leased privately, predominantly to tenants in the Heritage Building for employee parking. Signs posted state that the lot converts to public parking after 5:00 pm weekdays, and on weekends.

The Midtown Ramp is owned and operated by the City of Moline, and is currently the only parking ramp in downtown in which spaces are offered for lease to the public. The City offers discounted leased spaces on the 4th floor, and regular leased spaces on the 2nd and 3rd floors. The ground floor is free 3 hour public parking. Once leased, spaces in the Midtown Ramp are first-come-first serve. Some lease-holders pay a premium to have dedicated sign per space.

Table 2: Ramp parking space counts and regulation

Ramp Facility	Space Count	Posted Regulation
Centre Station Ramp	434	Leased Parking
Heritage Ramp	320	Leased Parking
Midtowne Ramp	Ground Floor	Free Parking 3 Hour
	2nd, 3rd, Roof	Leased Parking

Existing Off-Street Parking Occupancy

SSE conducted a weekday utilization study to evaluate the typical parking conditions of all the public facilities during peak occupancy times³. Observations were conducted from 11:00 am – 2:00 pm, representing the 'daytime' utilization, and from 5:30 pm – 7:00 pm, representing the 'evening' utilization. Details of the survey are displayed in the tables below. This information is visualized in **Map 5** included in the reference pages.

The data collected shows that most public parking lots in the downtown area maintained sufficient available spaces with the exception of T (13), U (14), V (6) and the Visitor Lots (John Deere Commons parking), which were at higher capacity for both periods. It is expected that these lots would show higher utilization during both daytime and evening counts due to their lack of parking restriction – these lots are "Free Parking 24 Hour" – and close proximity to both activity and dining destinations in the John Deere Commons. Lot U (14) shows extremely high evening utilization, 106%. This lot was over capacity during the evening counts as many cars were parallel parked illegally along the western edge of the lot. This lot illustrates the high premium drivers put on convenient parking in close proximity to downtown dining destinations. Lot U (14) is located

³ SSE Public Off-Street Parking Survey conducted on

adjacent to the highest density of restaurants along 5th Avenue, with direct access to outdoor patio seating for several dining establishments from the adjoining alley.

Leased lots, on the other hand, which are in many central areas of the downtown, showed low utilization during business hours, dropping to nearly empty after 5:00 pm.

Table 3: Off-Street Parking Occupancy

SSE Reference Number	City of Moline Name	Posted Regulation	Space Count	AM Utilization	PM Utilization
1	A	Leased Parking	31	20%	13%
2	L	Leased Parking	85	44%	18%
3	B	Leased Parking	73	32%	22%
4	K	Leased Parking	13	50%	69%
5	I	Free Parking 24 Hour	25	25%	24%
6	V	Free Parking 24 Hour	45	79%	76%
7	S	Leased Parking	28	24%	14%
8	M	Free Parking 3 Hour	32	20%	34%
9	Y	Free Parking 3 Hour	26	19%	15%
10	Police Department Lot	Free Parking 1 Hour	17	45%	18%
12	O	Free Parking 24 Hour	30	40%	43%
13	T	Free Parking 3 Hour & Discounted Leased Parking	49	50%	73%
14	U	Free Parking 3 Hour	71	58%	106%
15	Visitor	Free Parking 24 Hour	35	83%	31%
16	Visitor	Free Parking 24 Hour	136	81%	53%
17	W	Free Parking 3 Hour & Discounted Leased Parking	100	36%	15%
18	Visitor	Free Parking 24 Hour	145	55%	37%
19	G	Leased Parking	31	3%	16%
20	X	Free Parking 3 Hour	55	16%	15%
101	Z	Free Parking 24 Hour	49	11%	6%
102	R	Leased Parking	35	50%	51%

Leased Parking

Leased lots have a mix of sign types. There are eight facilities (A, B, G, I, K, S, T, W) designated as leased lots or lots with leased spaces on the City's parking map. Lots T and W are mixed lots including both discounted lease parking and "Free Parking 3 Hour" with specific signage denoting where each regulation is enforced. Refer to **Maps 3** and **6** for visualization of current leased parking locations and shared use lots.

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Residents of Moline can purchase leased parking spaces in the aforementioned off-street lots or the Midtowne Ramp. Within these ten available parking facilities prices range from \$10.00/month to \$55.00/month. **Table 4** details the current parking rates for all leased spots as of August 2015. A 10% discount is offered to businesses leasing more than 10 spaces. In order to acquire a leased space, a person must visit the City of Moline Finance Office. Leased spaces are billed quarterly.

Additionally, residents can also purchase Rover Permits for \$80.00/month. These rover passes permit drivers to park in any timed lots for excess of posted time limits.

Visitors have the option to purchase an All-Day Parking Permit for \$3.00/day. The permit allows users to park in any public parking space within the off-street lots and Midtowne Ramp exceeding the time posted. The All-Day Parking Permit does not apply to leased parking spaces.

All leased spaces in city-owned facilities are free public parking after 5:00 pm.

Table 4: Leased Parking Rates

Facility Name	Typical Rate per Month
All-Day Parking Permit	\$3.00*
Midtowne Parking Facility 4th Floor	\$10.00
Lots T and W	\$30.00
Resident parking permit (in three hour lots)	\$30.00
Lots B, G, and S	\$30.00
Midtowne Parking Facility 2nd & 3rd Floor	\$40.00
Midtowne Parking Facility Reserved Space, 2nd & 3rd floor	\$55.00
Rover parking permit	\$80.00

*per day

Based on the City's record of billing, only lots B, S, T and W have current leased spaces. Of the other lots shown on the City's public parking map as leased parking, some are dedicated for City of Moline Employee Parking (L), City Hall Employee Parking(B), and parking for Police Department vehicles(K). The current actual percentage of leased spaces within each lot is shown in Table 5, below. At present, Lots A and G have no active leases.

Table 5: Leased Parking Occupancy*

SSE Reference Number	City of Moline Name	Space Count	Leased Spaces (July2015)	Percent Leased	Typical Rate per Month
1	A Lot	31	0	0%	Undetermined
2	L Lot	85	0	0%	N/A
3	B Lot	73	19	26%	\$30.00
4	K Lot	13	0	0%	N/A
7	S Lot	28	16	57%	\$30.00
13	T Lot	49	14	29%	\$30.00
17	W Lot	100	41	41%	\$30.00
19	G Lot	31	0	0%	\$30.00

* Data included here is based on information provided by City of Moline Parking Enforcement

Map 6 included in the reference materials shows the locations of current leased spaces in the City of Moline, as well as specific uses of lots L and K. This map also shows the relationship between lots where a single individual or business holds multiple leases, and the location of the business address listed with the lease records. This illustrates typical walking distances between a leased space and the lessee's place of work.

Private Parking

Over 40 private parking lots are located throughout the Moline Centre as shown on **Map 01**. These lots are used for patrons of corresponding businesses or employees, and have signs that denote the ownership of each lot and its intended use. With the exception of lots 201, 207, 220, and 221, private lots were not included in this study. Lots 201, 207, 220, and 221 were included in the utilization survey due to their potential or present use as additional public parking for major iWireless events or as key development parcels in the larger study of Moline Centre.

Enforcement

Current enforcement for all public on- and off-street public facilities and spaces leased by the City of Moline is conducted by a single parking enforcement agent between Monday – Friday, 8:00 am – 5:00 pm, except holidays and weekends. Tickets are issued with the aid of a handheld computer (Autocite by Duncan Solutions).

Fines

Any vehicle violating a timed space by exceeding the allotted time is issued an initial ticket for \$20.00 for the first five business days. After the five allotted days have passed, the infraction is increased to \$35.00.

The City also prohibits "space jumping" which is defined as: *any person moving any vehicle from any designated zone parking space to any other space within a similarly designated zone⁴.*

⁴ SEC. 20-5109. PARKING TIME LIMITS/RESTRICTIONS ESTABLISHED: (d) Moving of vehicles prohibited. It shall be unlawful for any person to move any vehicle from any designated zone parking space to any other space within a

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Offenders of space jumping will be issued a ticket of \$20.00⁵. In order to enforce this, Parking Enforcement must track individual cars throughout the day by logging

A review of the City of Moline Municipal Code suggests that parking regulations described therein do not reflect the current uses of on- and off-street facilities.

Signage

Sign types and sign placement locations vary widely within the city-owned facilities. The most typical sign shows 3 to 4 sections of information. From top to bottom, signs typically include: Lot Name/Letter, Time restriction or “Leased,” period of enforcement, “Public parking after 5:00pm.” There are several decorative letter “P” signs and posts, mostly placed along 15th Street at Lots W, the Heritage Ramp, and at the entrances to the John Deere Commons Lots. Leased spaces are designated by lot, small signs indicating that a row of spaces are leased, and individual stall signs. A compendium of all signs for each lot is included in the reference materials for this memo.

iWireless Center and Large Event Parking Management

The iWireless Center is a major asset to Moline Centre, drawing event attendees from throughout the region to a variety of events ranging from QC Mallards games to major musical performances to the Ringling Bros. Circus. The arena is located on the Mississippi River, north of River Drive with easy access to the Great River Trail, Centre Station, and steps from 5th Avenue retail and dining destinations in Moline Center.

A basic understanding of the special event operations, listed below, was based on stakeholder information and the iWireless 2014-2015 event scheduling:

- The iWireless Center seats approximately 11,000 people.
- The arena’s adjacent parking lot has approximately 900 spaces. For many events a portion of these spaces are occupied by performer trailers and event staging, which requires access to the rear entrance to the arena.
- Access to both the iWireless parking lot and the arena’s main entrance are from River Drive.
- Last year there were a total of 72 total scheduled events.

similarly designated zone between the hours of 8:00 a.m. and 5:00 p.m.; Saturdays, Sundays, and legal holidays excepted. Upon observance of or detection of such offense in accordance with any provision of this Section 20-5109, the operator of the vehicle shall be issued a notice of violation and shall pay a fee of fifteen dollars (\$15.00). (Ord. No. 3001-2006; Sec. 20-5109 repealed; new Sec. 20-5109 enacted; 01/17/06)

⁵ City of Moline Code of Ordinance SEC 20-5109 out of date. Update fees listed within SEC. 20-5119. PENALTIES FOR VIOLATIONS OF THIS ARTICLE.

(b) Non-metered spaces. For illegally parking in violation of any of the subsections of Section 20-5109, the violation fee shall be twenty dollars (\$20.00) if paid within five (5) days (excluding Saturday, Sunday, or

- 19 of the events last year had over 5,000 attendees.
- 53 (78%) events were scheduled to begin after 6pm.
- 71 (99%) events lasted longer than 3 hours.

Assuming a conservative industry standard of 3 seats per parking space for the design of parking lots for major event venues, the iWireless Center would require 3-4,000 parking spaces to support the largest events. The iWireless Center relies on local public and private parking facilities to support major events. This additional traffic generated by these large events, and non-centralized parking options for event attendees cause confusion and congestion in Moline Centre before and after major events.

According to stakeholder interviews, once the iWireless parking lot is full, event traffic is directed to the Red Cross and John Deere employee lots, approximately 200 spaces, directly across River Drive. In addition, iWireless directs traffic to the Heritage and Midtowne ramps. Reference materials include the parking map posted on the iWireless ‘Directions and Parking’ website which shows the information event attendees are provided for parking in downtown. Typically, 9 inbound and 9 outbound Police Officers are stationed to direct traffic for major events. These officers are requested monthly by iWireless. **Map 7**, also in reference materials, shows the typical fill order of parking facilities for major events as it is understood from observation and stakeholder interviews.

New Parking Supply – Multi-Modal Station

The City of Moline, in partnership with Renew Moline, the Quad Cities Rail Coalition, QC Chamber of Commerce, and MetroLINK have initiated construction of The Q, a new multi-modal station with passenger rail platform. The station will be located at the northeast corner of 12th Street and 4th Avenue. A new parking facility supporting the hotel component of the development will occupy a lot at the northwest corner of 12th Street and 4th Avenue. Parking to support the Amtrak Station will be located on the south side of 4th Avenue and the north side of 5th Avenue east of 12th Street. The parking facilities will not serve as general public parking. Impacts on parking and circulation around the Multi-modal Station will be further studied in the Planning and Transportation Update to the Comprehensive Plan / Riverfront Sub-Area Plan.

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Issues Identified

Based on our evaluation of the existing parking conditions and stakeholder interviews, the project team identified the following primary issues related to parking:

Parking is difficult to identify and regulations are confusing for users

As a small urban area, drivers will make the assumption that they will be able to find parking close to their destination in downtown. Drivers circulating in downtown looking for parking need to be able to quickly and easily identify an available lot and how to access it from their vehicle.

Upon arriving downtown, drivers looking for parking in Moline are confronted with inconsistent signage, and unclear regulations. Some facilities are signed facing main roads, some at the lot entrances, and some within the lots themselves. Different colors and sign types are used to describe a single parking lot use. In many lots there are mixes of leased spaces, signed as both rows and individual stalls, and public parking spaces. Some signs are posted with 3 or 4 subsections of information referring to restrictions on different times of day, days of the week, enforcement types, and lot uses. On most signs, the messages are displayed in a text size that would be challenging to read from a passing vehicle. Signage for a lot doesn't serve drivers if they are unable to safely stop and turn into the lot once they identify the entrance. Stakeholder interviews, confirmed by site visits to Moline Centre, suggested that the available parking locations are difficult to access, and their regulations are unclear and inconsistent with information provided online by the City of Moline.

Recommendation 1.0: Simplify the system

- 1.1 Simplify the system to improve the user experience
- 1.2 Simplify signage to make the system more intuitive for the user
- 1.3 Provide accurate and up-to-date information to all users
- 1.4 Reconsider the structure of leased parking downtown

Enforcement efforts are not sufficiently resourced to ensure available spaces where they are needed most.

Moline's parking enforcement resources do not meet the current demand on the enforcement-only parking system. In many cities, paid on-street meters and pay-to-park lots ensure regular parking turnover. The function of enforcement in this context is to distribute citations to those individuals who have remained in a space beyond the time they have paid for. Parking turnover is encouraged by the requirement of the driver to pay for a space, and the risk of being ticketed if they do not.

Without a time-related payment system, the City's only method to encourage turnover is through enforcement. Drivers are more likely to risk citations, and leave a car parked over the posted time limit if they do not see enforcement as a real threat. In order for an enforcement-only system to operate efficiently, the ticketing of cars who abuse the time-restrictions must be there.

Moline currently employs one enforcement officer to enforce regulations for 1,500 stalls within lots and ramps with differing time restrictions, leased spaces, and 500 on-street parking spaces across an area of approximately 100 acres in Moline Centre. The task of providing an enforcement level adequate enough to encourage the desired turnover in the downtown area is too much for a single person. As Moline is an enforcement-only system, making enforcement operate as efficiently as possible is key.

Recommendation 2.0: Maximize enforcement

- 2.1 Focus on making existing enforcement more efficient.
- 2.2 Establish enforcement priorities
- 2.3 Pilot Automated Enforcement with a phased approach
- 2.4 Address the issue of 'lot-jumping' with a carrot/stick approach
- 2.5 Use data to inform decision making

Insufficient large event management negatively impacts downtown parking and circulation

The issue of parking operations and traffic management surrounding large iWireless events was voiced by many stakeholders at the outset of this study. The primary concern varied from each stakeholder perspective on this issue. General concerns from the City's perspective surround the impact of large events on the circulation of traffic and availability of parking for local retail patrons during large events. The perspective from the iWireless representatives centered on the lack of available parking for event attendees within a reasonable walk to the arena.

At present the arena and the City are sending conflicting information regarding arena access and parking to event attendees. In addition, there appears to be no coordinated traffic management strategy. New development coming to the immediate area will exacerbate the existing issues. Congestion approaching the intersection at 12th Street and River Drive will likely be compounded by the construction and subsequent traffic generated by the future Multi-Modal Center and Hotel at 12th Street and 4th Avenue. In light of development plans for the area adjacent to the iWireless center, the time is right for the City and iWireless to address traffic management and parking in the area and devise a collaborative strategy for better management of traffic circulation and parking in the area.

Recommendation 3.0: Establish an event parking strategy

- 3.1 Establish a parking and traffic management strategy for large events
- 3.2 Establish consistent communication between all stakeholders
- 3.3 Seek out all additional resources available on an as-needed basis.

Detailed Recommendations

1.0 Recommendations: Simplify the system

Rec 1.1 Simplify the system to improve user experience

The City's parking options should be easy to find and identify as public parking, and regulations must send a clear message as to each lot's purpose and regulations. Before signs can be simplified, the function of each lot must be simplified. Wherever possible, lots should carry a single regulation type without mixing leased or employee parking. To the extent possible, lots

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adjacent to each other should carry similar time restrictions. The system should be logical from the driver's point of view; mixing time restrictions for adjacent lots may seem frustrating or arbitrary for the user. To benefit users and businesses the most, time-restrictions and enforcement should focus on lots closest to the retail core along 5th Avenue, and to on-street parking to encourage the highest rate of turnover in the most desirable locations.

Strategy: Establish four simplified lot uses. Leased Parking, 2-Hour Public Parking, 4-Hour Parking (John Deere Commons Only), and 24-Hour Public Parking. Recommended lot use configuration is shown on the map below. Specific strategies for the reconfiguration of leased parking are described later in this section.



Lot Types

Leased Parking Lots: Leased parking should be consolidated to fewer lots, with individual stalls designated by permit number wherever leased parking remains mixed in public parking lots.

2-Hour Public Parking: These lots are concentrated in the most central area of downtown, and are intended to best serve local businesses most similar to on-street parking. The time limit reduction from 3-hours to 2-hours makes these spaces function like nearby on-street parking. Enforcement should be focused on these lots to encourage increased rates of turn-over.

4-Hour Public Parking: These lots are closest to activity generators in the area which want parking to serve and encourage longer visitor stays. However, a time-restriction ensures some space turn-over to retail and dining destinations nearby, and allows the City to better distinguish these parking lots from large event parking options.

24-Hour Public Parking: Mostly located slightly further from the center of downtown, and are a very low priority for enforcement.

Strategy: Ensure consistent 2-Hour time limit for all on-street parking. Most of the on-street parking in downtown is currently restricted to a two hour time limit. A field survey of the downtown area should be conducted to ensure that all 1-Hour and 3-Hour time restrictions are replaced. 15 Minute parking spaces should remain, as they serve a different user type in downtown.

Rec 1.2 Simplify signage to make the system more intuitive for the user

In order to simplify the system, signs must be more streamlined; their regulations more identifiable from the vantage point of passing drivers. Drivers need to answer three questions from their vehicle: (1) Is this lot public parking? (2) Where do I enter? (3) How long can I stay? Parking signage should highlight the most important information. Time restrictions and enforcement should be simple and easy to understand. The size, color, and physical location of signs identifying parking should provide a hierarchy of information for the user. A familiar color and symbol should highlight the primary entrance to the lot from a main street. Information seen in the physical environment should reflect, even in subtle ways, information about parking provided by the City and other entities in Moline. This information must also translate to digital resources used by drivers to navigate in downtown. More attention paid to visual cues to drivers will make the parking options more intuitive for the user.

Strategy: Streamline the text/message wherever possible

- The consistent "P" sign should be the largest and highest sign on each post, followed by the time restriction.
- Following lot time restrictions, the message "Enforced 8AM-5PM Weekdays" should be visible. This message implies that at all other times, the time-restrictions on the lot are not enforced.
- In the event that leased parking spaces appear in Public Parking lots, add the words "unless otherwise posted" before enforcement descriptions.

Strategy: Remove, cover, or replace all lot enforcement descriptions showing the "Free" terminology.

In most cities, a parking lot is "Free" unless a fee is posted or the spaces are otherwise designated as unavailable (private parking, customers only, etc). In Moline, the word "free" is used to describe the period of time a person can park in a public lot without risking being ticketed. This is one of the ways Moline's parking is confusing to the driver.

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Because of Moline's enforcement-based system, the most important message to communicate to the driver is the *time restriction*. The fact that the driver will not need to pay for their parking space is understood and unnecessary in the context. By conflating the idea of time-restricted parking with a pay-to-park system, drivers may not understand that the lot will be enforced in the absence of a payment system.

Strategy: Remove signs showing multiple uses in a single lot.

Remove all signs which describe different types of uses in a single lot. Because many of the lots in downtown Moline contain both leased and public parking spaces, it's necessary to simplify the message for users. Therefore, it is recommended that these leased spaces be consolidated where possible leaving a minimum number of leased spaces in public parking lots. Communicating multiple uses on a single sign requires additional text, making signs longer and more confusing for a passing driver. The leased spaces which remain in public lots must be signed as such at each stall to avoid confusion.

Strategy: Remove public parking signage directing people to the Heritage Lot; Remove Heritage Lot from all public parking map.

Although the Heritage Lot is centrally located, the lot's position behind a large office building and nearly 100% leased, stall-signed spaces, make it a very confusing place for casual visitors to park compared to the adjacent U-Lot(14) and W-lot(17). For this reason it is recommended that this lot be used exclusively for event parking, with temporary "Event Parking" signage installed as needed.

All other signage indicating public parking should be removed. Enforcement in this ramp should be limited only to ad-hoc lease-holder complaints.

Strategy: Use consistent symbol and color cues to make the system more intuitive for the user.

It takes both large scale changes, and fine-grain details to make a parking system easy to use. Decisions affecting the public face of the system must be coordinated to reinforce the information. A driver in downtown should be able to recall the most important feature with a split second comparison between signs they see on different blocks downtown. For example, the shape and color of the "P" sign will be more memorable to a drive-by observer than a lot name "M - Lot" or "I - Lot" on a smaller sign. These secondary organizational tools are:

1. Organize the parking information (map) into "Downtown Parking Zones." Lots within this area should carry the same signage type and time - limits.
2. Use consistent lot names, colors, and styles in all parking information available to users. This includes information on public parking, leased parking, and large event information.
3. The placement of parking signage should be consistent in all lots with the largest "P" identifier at the top of each sign.

4. Use consistent symbology for each enforcement type:
 - a. 2-Hour and 4-Hour Public Parking: Large format blue "P" installed at the top of each street-facing sign posted at the lot entrance.
 - b. 24-Hour Public Parking: Large white rectangle containing a green "P" installed at the top of each street-facing sign posted at the lot entrance.

Appendix PARKING MEMORANDUM

FIGURE 01: Proposed Simplified Sign Types and Locations

TIME-LIMITED PUBLIC PARKING



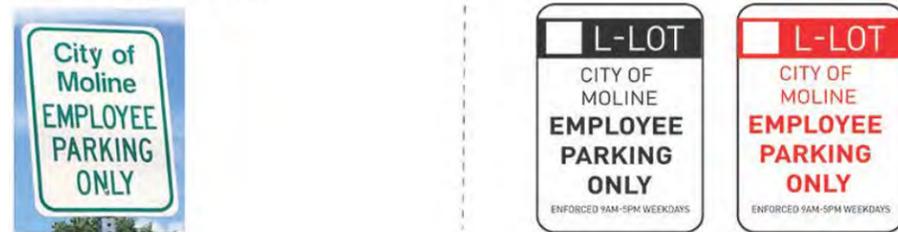
PUBLIC PARKING WITHOUT TIME RESTRICTION



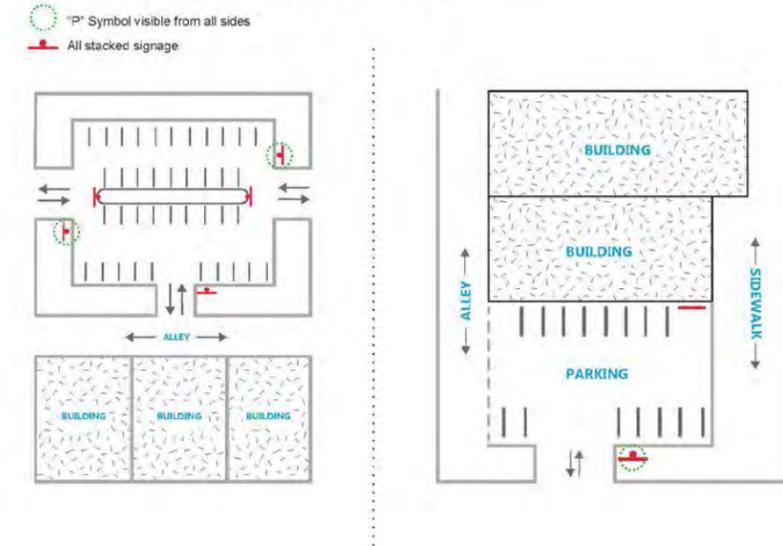
LEASED PARKING



DEDICATED LOTS



SIGN POST LOCATIONS AND DIRECTION



Rec 1.3 Provide accurate and up-to-date information to all users
When changing parking regulations, it is imperative that changes are reflected in all available information describing parking in downtown. This requires effort from the City to both create useful and universal materials, to distribute this information to others who may share information on parking, and to ensure that it is as easy as possible for local businesses and employers to share that information with others. Simplifying the City's parking system for the user starts with the parking map.

Strategy: Update parking map and information on the City's website. The City's new parking map should color code lots by time-restriction, highlighting lots which are available only after 5:00pm as a secondary symbology.

Strategy: Engage businesses to help spread consistent information about parking in downtown. Send an email out to all businesses and companies working in downtown requesting that they update their parking information; provide a link to the new parking map in several file types.

Strategy: Feature parking information along with any information about event in downtown.

Strategy: Update information about parking in all digital tools available. (Google Maps, Renew Moline Website, iWireless website, JD Museum, Yelp, etc.)

Appendix PARKING MEMORANDUM



Rec 1.4 Reconsider the structure of leased parking downtown

In order to accomplish the previous recommendation of simplifying the purposes of parking lots and ramps downtown, it will be necessary to review the structure and locations of leased parking in Moline Centre. This can be accomplished through the relocation of some leased spaces, changing signage to clarify the lot and stall uses, making information about leased parking more available, and simplifying the application process for those interested in leasing a space.

Re-organize leased parking to increase efficiency...
...For the user:

Strategy: Better market leased parking to local businesses, employees, and residents.

Strategy: Establish a means to apply for leased spaces online.

Strategy: Update informational materials. Hire a marketing firm to develop City parking map and leased parking marketing informational brochure and handout. These materials should highlight the convenient lot locations and low monthly price, enforcement guarantees, and dedicated permit numbers.

Strategy: Distribute information about leased parking with all parking citations written in the downtown area.

...For the City:

Strategy: Increase/decrease fine for parking citations to meet or exceed lowest lease parking price to incentivize change of behavior.

Strategy: Concentrate leased parking in fewer lots.

Strategy: Sign individual leased stalls by permit number. Consider distributing matching permit stickers for display in vehicle for more efficient enforcement.

Strategy: Establish a contact at larger area employers without dedicated parking facilities to assist in distributing new information on leased parking.



2.0 Recommendation: Maximize Enforcement

Changes such as those outlined above will not only provide clarification for the user on how, when, and for how long they may park in Downtown without the risk of citation, they also make operation of the system easier for the City. Simplifying the function of certain lots will allow for more efficient enforcement and data collection, which will better inform the City's policy and development decisions.

The recommendations and strategies offered below are intended to increase the efficiency of the existing parking enforcement. It is also recommended that the City hire an additional enforcement officer to help manage the task of parking enforcement in Moline Center.

2.1 Focus on making existing enforcement more efficient

In order for Moline's enforcement only system to operate efficiently with the existing enforcement staff - the approach must to be 1) focused in a smaller area to allow the parking enforcement officer to encourage turnover in the most important locations, and 2) be re-structured so that the enforcement effort can be a shared between the parking enforcement officer, the Moline Police Department, and City staff.

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Strategy: Increase enforcement

There is no perfect equation describing how many enforcement officers are needed per acre of parking, or how many lots or spaces can be covered by an individual in a set amount of time. These efficiencies are affected by many factors including the officer's mode of travel and tools used for data collection or issuing citations, as much as they are affected by the number of spaces, distance between or ease of access to the lots of interest.

- Hire an additional parking enforcement officer.
- Focus enforcement effort on a smaller area.

Strategy: Update enforcement equipment

Providing adequate tools to the existing parking enforcement officer is the single most important step the City can take toward enforcement efficiency in Moline. Significant advances have been made in parking technology in the past several years. The City should explore options for handheld devices which offer handheld license plate scanners and GPS data collection to increase efficiency.

Current Model: Autocite by Duncan Solutions. Model number: 88342

Strategy: Consider alternative modes of transportation for parking enforcement.

During fair-weather seasons, consider the use of bikes, segways, electric smart-cars or golf-carts to increase the speed at which parking enforcement can circulate downtown. This operational change serves a second important purpose increasing the visibility of enforcement to people driving and walking around downtown. Seeing an enforcement officer circulating several times per day will increase the psychological presence of enforcement without additional officers.

Strategy: Find opportunities for other City entities to help manage parking enforcement

- Establish a means for PD⁶ to assist in monitoring and responding to leased parking issues.
 - Establish a complaint number to contact the Police Department⁶ directly.
 - Post this contact information clearly on the City's parking information web-page.
 - Provide updated information to all individuals and companies holding parking leases informing them of a specific contact number to call in the event that they arrive at their space and find it occupied.
 - Leased parking spaces should come with a promise that if a lessee arrives to their promised space and finds that it is occupied - they can call the Parking Issues number and an enforcement officer will arrive to ticket, and subsequently tow the offending vehicle within 20 minutes, and 2 hours respectively. Ensure that the license plate number of the lease

⁶ PD, or Police Department described here serves as one potential partnership entity in parking enforcement. Alternative partnership opportunities within City Staff should be explored.

holder will not be ticketed for the next 24-hours by offering a daily parking pass and communicating the license information to parking enforcement on duty.

- Share the responsibility of tracking 24-Hour Parking lots.
 - Ensure that the City's 24-Hour parking ordinance stipulates that a parker cannot stay more than 24-Hours at a time. Seek potential partner to share responsibility of monitoring overnight parking violators. For example, the Police Department on evening patrol in downtown could visit each of the 24-hour lots to record license plate numbers of cars parked in the lot after midnight. These license plates should be recorded and shared with Parking Enforcement for survey the following day. Issue only warnings within the first 2 months of signage changes.

2.2 Establish enforcement priorities

Strategy: Establish priority areas, outline enforcement strategy, and provide best practices for reporting field information to better inform policy changes.

Check-in meetings should be conducted occasionally to review the success or failure of the new enforcement approach. As driver behavior adjusts to the new parking regulations and enforcement approach, enforcement will likely need to be re-focused around new issues as they present themselves. Continue to focus parking enforcement only on those spaces in the highest demand locations on-street, and lots with the lowest time-limits.

Strategy: Establish a quarterly Parking Enforcement meeting with Office of Planning and Development and Police Department or entity responsible for traffic management to discuss upcoming events and development impact on parking management.

2.3 Pilot Automated Enforcement with a phased approach

Based on their size, proximity to downtown commercial destinations, and relatively high utilization rate, the City's U and W lots have been identified as the highest priority for parking enforcement. To increase the efficiency of enforcement in the U and W lots - the City should consider installing a modified Pay-and-Display system, distributing free, time-stamped tickets for drivers to display on their dashboards upon entering the lot.

Typical Pay-and-Display systems are costly due to the integrated credit card payment system. Here the recommendation is to acquire a ticketing system more akin to those at the entrance of pay-at-exit parking ramps. The ticket issued by the machine would be time-stamped with the driver's time of entry, and would be placed time-stamp up on the dashboard. Enforcement would then be able to quickly check each time-stamp for cars who have stayed beyond the two hour limit. It is possible that a person may return to their car to replace their ticket, but with a shorter time-restriction and swift enforcement drivers will be disinclined to do this.

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This ticket-based system would enable parking enforcement to track each car from its true time of entry, instead of from the moment the enforcement officer is able to begin tracking. This ticketing system also serves to further increase the visibility of parking enforcement in the center of downtown. This new approach should be phased in, with the pilot starting in Lot 14(U). The effects of the system, its success, and further expansion should be assessed each year.

Strategy: Automated Enforcement Pilot Phasing

Phase 1: Acquire and install ticketing system at the 15th Street entrance of Lot 14(U).

Restrict entrance to 15th Street, exit to alley.

Phase 2: Install ticketing systems at both entrances to Lot 17(W).

Phase 3: Lot 16/18 (JD Commons Parking) with longer parking time limit to encourage longer stays at nearby activity centers.

2.4 Address the issue of 'lot-jumping' with a carrot/stick approach

'Lot-jumping' offenses occur when a driver parked in a time-limited lot moves their vehicle to another stall in the same lot, or to another lot with the same time-restriction as an effort to keep their car in an area longer than the time allowed and avoid being ticketed. Lot jumping can happen intentionally (employees of local businesses abusing short term parking to store their cars for the entire workday) or unintentionally (a driver does not understand that moving from one lot to another of the same time-restriction is not allowed).

In an enforcement-based parking system such as Moline's, lot jumping has no silver-bullet solution, but can be addressed over time by making it easier to understand the system and take advantage of parking alternatives, meanwhile raising the risk for abusers to make the consequences of breaking the rules more severe.

---Scenario---

An employee of a local business parks in a 3-Hour Free lot in the morning (9:00am), and easily moves his car to a new stall on their lunch hour (12:30pm) with a relatively low risk of being ticketed through the end of the workday (5:00pm). By limiting the time to 2-hours, the risk of ticketing is higher, and the option of lot jumping is less convenient, as the driver would need to move his car 3 or more times to ensure he would not receive a ticket (stick). As time-restrictions on local lots are changed, the City distributes information to all local businesses offering 50% off the first quarter when they sign up for a leased parking space within the first month of new parking regulations, and now the driver can sign up and pay through the City's website (carrot).

The issue of lot jumping could also be addressed through small functional changes to the proposed dashboard display ticketing system. The system would have to be programmed in such a way that drivers would be required to input their license plate number upon entering the lot. This system could then automatically notify parking enforcement of expired tickets issued by license number, as well as refusing to issue additional tickets to license plates over the 2-hour time limit.

Strategy: Limit to 2-hour instead of 3-hour parking maximum time-limit in central parking lots 14(U), 17(W), 8(M), and the ground floor of the Midtowne Ramp.

Strategy: Increase enforcement on lots already identified as having high incidence of 'lot-jumping', focusing first on lots in the central 2-hour zone. Use parking enforcement experience to date to focus initial effort and review data showing citation lot location to inform future focus areas.

Strategy: Increase the fine for 'lot-jumping'. Price of ticket should be equal to or more than the lowest monthly leased parking space fee.

Strategy: Provide more information on leasing with all citations issued. As all informational materials for parking are updated, be sure to develop a ¼ sheet leased parking informational flyer to include with all 'lot-jumping' citations issued.

Strategy: During the period of transition for parking regulations, consider a temporary fee-waiver or discount incentive for leased parking.

Strategy: Clarify language in City of Moline 'Lot Jumping' Ordinance. Provide a link to this ordinance on the City parking website.

Strategy: Consider establishing escalating fees for repeat offenders.

2.5 Use data to inform decision-making

As changes are made to the existing parking system, it is important to establish a mechanism for benchmarking the success or failure of new initiatives. Data collected through issuing citations, and hours of field experience are extremely valuable assets.

Strategy: Consistently review the data collected. One day, or half-day per month should be set aside for parking enforcement to review the tickets issued and to identify and document citation hot-spots. This information should be formalized in a monthly memo to the Planning and Development Department.

Strategy: Conduct bi-annual parking counts and recording utilization.

Strategy: Establish a record of repeated 'lot-jumping' offenders. Use this information to inform graduated citations.

3.0 Recommendation: Establish an event parking strategy

The strain on the streets and parking facilities in downtown is a burden that must be shared by both iWireless and the City of Moline. The first step toward better traffic management is improving communication within management, between management and operations, and finally to downtown visitors. Facilities identified for event parking, the parking lot fill order, traffic

Appendix PARKING MEMORANDUM



direction, and signage and information distributed to event attendees must align for a traffic management and parking strategy to benefit all stakeholders. This new parking strategy should concentrate overflow parking in one area, and communicate options and directions to drivers further in advance of the iWireless Center.

iWireless is a regional attraction which has the potential to benefit all of the Quad Cities by attracting new visitors to the area's local businesses, hotels, and attractions. The City should look for assistance from the Quad Cities Convention and Visitors Bureau and QC Chamber of Commerce to acquire additional resources and man-power necessary for the largest events.

3.1 Establish a parking and traffic management strategy for large events

Strategy: Work with iWireless to establish a new parking and traffic management strategy for major events⁷

- Identify consistent lots and fill order. Concentrate these lots in one area.
- Identify the positions and roles of each officer stationed. Train officers on new fill procedures and real-time communication strategy.
- Identify the positions and messages of all temporary signage. Position signage beginning at I-74 exit ramps to 7th Avenue and River Drive.
- Identify the direction and preferred entrance of all overflow parking lots.
- Revise all public parking information to align with new strategies.

Strategy: Better utilize the Heritage Ramp (.2mile/4min walk to iWireless entrance) Lot 17(W) (.3 mile/6min) and gravel lot at 4th Avenue and 16th Street for large event parking.

3.1 Establish consistent communication between all stakeholders

Strategy: Establish a monthly communication schedule between City, iWireless, and PD for major events. Use these meetings to clarify needs, and assess the success of the new parking strategy.

Strategy: Conduct PD training on new fill procedures. Ensure that all officers directing traffic can communicate in real-time.

3.3 Seek out all additional resources available on an as-needed basis.

Strategy: Request traffic management support resources of QC Convention and Visitors Bureau and Quad City neighbors. Resources such as portable traffic sign trailers and the production of large banners and sandwich boards.

Strategy: Seek use of employee parking lots along 4th Avenue for the largest events. Establish a use-agreement for this purpose. If Riverstone and John Deere's IT

Department are hesitant to agree to sharing lots, consider planning an evening, programmed event on the private parking lot to demonstrate the minimal impact of after-hours repurposing.



⁷ Defined as >3,000 attendees or the number of attendees that could be parked in the existing iWireless parking facility using typical stadium parking multiplier (.3)

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Reference Material

Map 1: All Downtown Parking Facilities by Ownership



Memorandum - Reference Materials
 Planning and Transportation Update to the 2014 Comprehensive Plan / Riverfront Sub-Area Plan

Map 2: On-Street Parking within Downtown Study Area



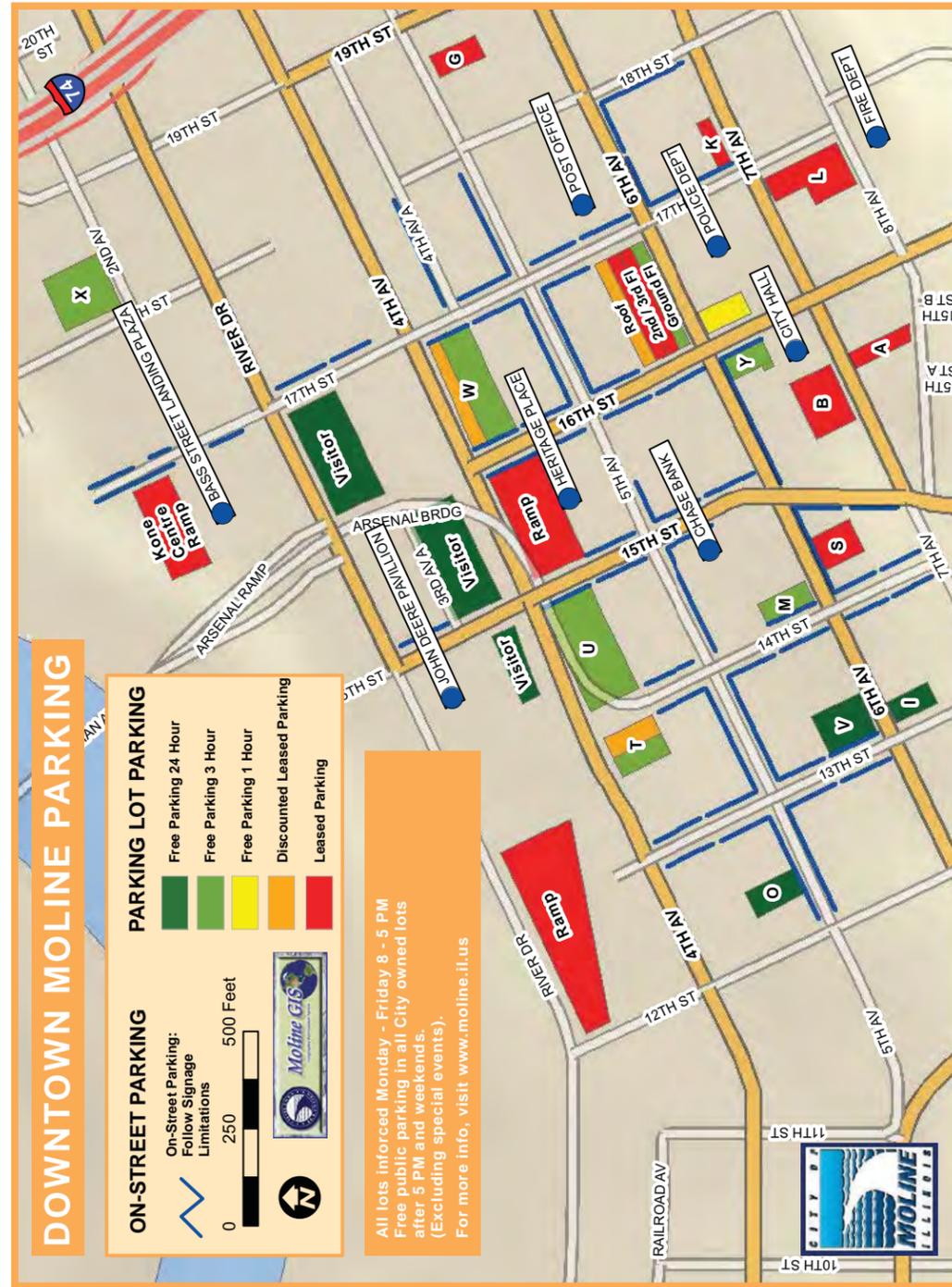
Memorandum - Reference Materials
 Planning and Transportation Update to the 2014 Comprehensive Plan / Riverfront Sub-Area Plan

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Reference Material

Map 3: On-Street Parking within Downtown Study Area



Memorandum - Reference Materials
 Planning and Transportation Update to the 2014 Comprehensive Plan / Riverfront Sub-Area Plan

Map 4: City-Owned Lots / Ramps



Memorandum - Reference Materials
 Planning and Transportation Update to the 2014 Comprehensive Plan / Riverfront Sub-Area Plan

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Reference Material

Map 5: Daytime and Evening Parking Facility Utilization based on SSE Data



Note: Lots 220 & 221 are privately owned lots. These lots were included due to their size and proximity to City-Owned facilities.



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Memorandum - Reference Materials
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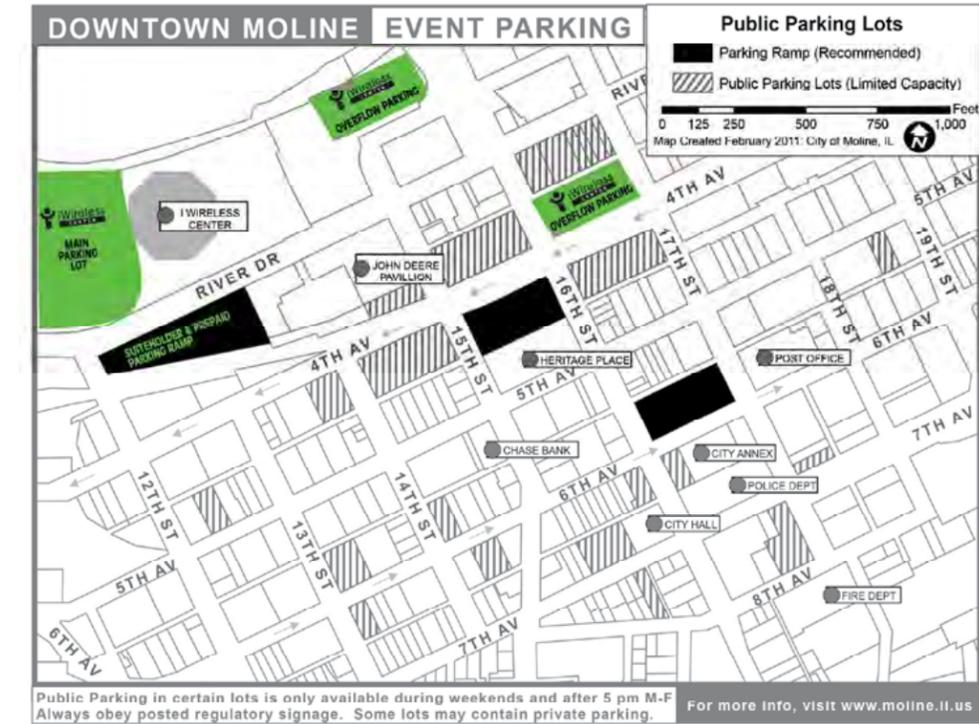
Reference Material

Map 6: Leased Parking by Lot Use Type



Memorandum - Reference Materials
Planning and Transportation Update to the 2014 Comprehensive Plan / Riverfront Sub-Area Plan

Map 7: iWireless Center - Existing parking lot/ramp fill order as described by stakeholders



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Reference Material

Map 8: Proposed Lot Uses



Memorandum - Reference Materials
Planning and Transportation Update to the 2014 Comprehensive Plan / Riverfront Sub-Area Plan

Map 9: Proposed Lease Consolidation Strategy



Memorandum - Reference Materials
Planning and Transportation Update to the 2014 Comprehensive Plan / Riverfront Sub-Area Plan

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Reference Material

Map 10: iWireless Center Proposed Parking Lot/Ramp Fill Strategy



Memorandum - Reference Materials
 Planning and Transportation Update to the 2014 Comprehensive Plan / Riverfront Sub-Area Plan

Appendix MARKET ANALYSIS NOTES



Moline Market Analysis Notes – September 2015

Residential (General Rental)

- The addition of residential units will need to drive development downtown. Market rate residential development is a crucial factor in the buildup of Downtown Moline.
- There is concern that new construction rentals can't be underwritten because rents that high can't be justified. Others are concerned that it's difficult to do good quality rehab rentals without the state historic preservation credit that has been so important in Davenport. Discounted land makes it easier, but some feel it can still be difficult.
- Apartments could be added in the form of smaller 12-unit rehabbed buildings along 6th Avenue.
- Townhomes for rent or sale may also be possible along the Mississippi River and east of I-74 once the new alignment is complete.
- Most of Moline's rental properties have used Low-Income Housing Tax Credits.
- There have been recent achievements.

Residential (Skinner Block Lofts)

- Nearly completed leasing of the rehabilitated Skinner Block Lofts (with more pending). Of the 12 units, 11 are full, leasing within 3 months. These are the current top of Moline's d.t. rental market except for The Mills.

Type	Net Rents	Size	Gross Rent/SF
1 BR/1 BA (2)	\$1,100	1,100	\$1.00
2 BR/2 BA (6)	\$1,350	1,200 (approx.)	\$1.13
3 BR/2 BA (4)	\$1,650	1,800	\$0.92

Residential (Riverbend Commons)

Growing occupancy at The Mills at Riverbend Commons

- 90 units, but they are leased by bed. 45 x 2 beds, 45 x 4 beds = 270 possible residents
- Opened in Oct. 2015, so missed school year
 - August 2014-2015 – 40% occu.
 - May-August 2015 – 100% occu. (5-year corporate intern contracts – Deere, Alcoa, Kone)
 - August 2015-2016 – 70-80% occu.
- WIU expects 3,500 undergrads by 2020 and 5,000 overall including graduate students and commuters
- Currently also has Blackhawk College and Trinity Nursing students, possibly others.
- Expected to be largely WIU students within 2-3 years
- Plans for 150 apartments in Phase 2
- Student renters at The Mills are paying \$1.43/SF, after current incentives are applied. Skinner Block Lofts renters are paying \$0.92-\$1.13/SF for very large units. These represent the current top of the rental market in Downtown Moline.
- A Cushman Wakefield market study estimated demand for 500 more apartments across the Quad Cities, excluding exogenous demand from workers moving closer to jobs and from students. This represents a great opportunity; however, it is less clear how many of these units can be supported within Downtown Moline.

Residential (Chase Redevelopment)

- Apartments are planned for the Chase property
 - One source could be 6-month training periods at the Arsenal.
 - We were told 24-25 apartments; Christine mentioned possibly 35 from her conversation with them. We were told 8 of these will be corporate housing; Christine was told 15-20.
 - 1 and 2 BR apts. with 600-850 SF are planned to rent for \$1.50/SF.

Moline Market Analysis Notes – September 2015

Residential (Downtown Davenport)

- Downtown Davenport is very competitive for rents, even as Downtown Moline's reputation has improved considerably as an entertainment location.
 - The rents below apply to Front Door Iowa's high quality buildings in downtown Davenport: Market Lofts, Renwick, Hotel Blackhawk, and Forrest Block, all rehabbed within approximately the last 5 years:

Type	Avg. Gross Rents (incl. all utilities)	Size	Gross Rent/SF
Studio	\$950+	600 SF	\$1.58
1 BR/1 BA	\$1,225+	675 SF	\$1.81
2 BR/1 BA	\$1,500+	875 SF	\$1.71
3 BR/2 BA	\$2,000+	1,600 SF	\$1.25

- These have upgraded appliances, open floor plans, dishwashers, washers and dryers in unit.
- Front Door Iowa plans to do more like this.
- These are full with 10-15 person wait lists for each building.
- Rents for Union Arcade (Davenport building renovated in 2015) are a bit less than these for smaller units and comparable for 2 BRs.
- Renters in downtown Davenport tend to be young professionals from the Rock Island Arsenal and other larger corporations and students from the Palmer College of Chiropractic, etc.
- The large number of loft buildings in Davenport were developed using Iowa's state historic tax credit. Moline (and most of Illinois) has no alternative. This has been a major disadvantage.

Residential (Sale)

- For-sale housing will follow market-rate rental.
- We heard the \$250,000 range may be supportable. We also were told market-rate townhomes might be possible in the current bridge area selling for \$160,000-\$175,000.
 - These prices may not include the land cost.

Office

- The office market is soft in Moline, including in the downtown.
 - This is also the case elsewhere in the Quad Cities, although Iowa's more favorable corporate and property tax situations and its comparative stability at the state level have reportedly made it more attractive to companies.
- Several vacant office spaces in Moline include some along River Drive in the city's best quality office buildings (Kone Centre, Riverstation, Caxton)
 - Asking rents for these are \$13-14/SF, not including additional expenses, which run \$2.50-\$8.50/SF.
 - These are consistent with the "low net" rental rates for new construction and existing Class A offices across the Quad Cities as a whole, as reported by Ruhl Commercial Company.
 - Vacancy rates are much higher for Class A and Class C office space in Illinois than in Iowa, but they are similar for Class B offices.
- These existing spaces with 3,000-4,000 SF or less are attractive to companies already located in Moline and the broader Quad Cities, including service providers, small businesses and large firms seeking overflow space.
 - Nonetheless, the market is not deep enough at this time to fill a great deal of space.
- There is weak demand for multi-tenant office development. Any significant office development will be driven by large single users.

MARKET ANALYSIS NOTES



Moline Market Analysis Notes – September 2015

- The Quad Cities have little in the way of large corporate offices with 10,000-30,000 SF on a single floor. While the new Kone tower has space available on this scale, it is reportedly priced well above what the market can currently support (\$25-\$30/SF).
- As a larger number of people are attracted to live in Downtown Moline and more amenities are added, companies already in the Quad Cities and the city will be more likely to fill existing office space and support a modest amount of additional small offices, including medical offices. Preferably, these can be located in adaptively reused older buildings to hold down rents.

Retail (General)

- Restaurants are faring well in Downtown Moline and are attracting residents, workers, and visitors from the city and neighboring jurisdictions.
- More are in the works in the vicinity of John Deere Commons and the Western Illinois University areas, with at least one under construction.
- Given the proximity of students and workers paired with the entertainment district-quality of the downtown core, more restaurants should be supportable.
- Increasing the number of specialty retailers in Downtown Moline would create a more concentrated draw for shoppers to like to park once and walk or to stroll from nearby hotels. Rents can't be too high in order to facilitate this.
- Ruhl Commercial reports downtown net rental rates for retail space of \$6-\$12/SF across all the Quad Cities.
 - These are reasonable, as suggested by the estimated vacancy of 5% – a low rate.
- Large format chain retailers would be difficult to attract downtown and would be less appropriate for the district.
 - There is vacant land and available existing space to the south along John Deere Road, where large retailers will more likely locate.
- As more people live in Downtown Moline, there will be a need for additional convenience retail, “staple” shopping and personal services.
 - Visibility, accessibility and reasonable pricing will be very important to support this.

Retail (WIU Area)

- Riverbend Commons
 - Discussions with a grocer to occupy 40,000 SF at Riverbend Commons are far along. This would be part of Phase 2.
 - The current prospect would stock a variety of standards, along with ethnic foods and a sizeable prepared food component. This would be appropriate for the lunchtime and student-oriented location.
 - However, access and wayfinding may present challenges. Those west of the highway will need to be attracted and guided to the store, and frequent train crossings present a real barrier to shoppers with limited time.
 - Phase 1 current tenants are Sriracha Thai Bistro & Oyster Bar (7,500 SF) and Happy Joe's Pizza (almost 5,000 SF). These will open Fall 2016. Both have 10 year leases.
 - 1,150-1,200 SF available at \$15-21/SF (triple net) with \$7 pass-throughs, 5 year lease term
 - They have talked about and considered a chiropractor, bubble tea, gym, dry cleaning, but no deals yet.
- A convenience store in the ground floor of the very successful Moline Enterprise Lofts closed.
 - Edge location deterred foot traffic
 - Lack of same-side, quick parking deterred car trips.
 - Nonetheless, other businesses have since shown interest in the vacant space.
- Existing specialty retailers have stood the test of time, drawing on Quad Cities residents and visitors.

Moline Market Analysis Notes – September 2015

- Their business is adequate, but not brisk.
- They compete with the Village of East Davenport and Le Claire, Iowa. It is possible that landlords in these competing locales hold rents down to maintain occupancy.
 - These areas are comparatively dense with such retailers, which makes them attractive for day trips.

Retail (Q Multimodal Area)

- The extended stay hotel to be built at the multimodal center is planned to include some retail, eating and entertainment also open to non-guests. There will be some opportunity beyond this for spillover development, but it could be limited without the addition of more residences nearby.
 - All retail will be third party operator. Looking at café/coffee, restaurant, entertainment like small bowling alley, pool and/or video golf in basement.
 - 100 room Starwood Element Moline extended stay hotel, to open 2017.
 - All will be suites – studio, 1 or 2 BR, all with efficiency kitchens – refrigerator, stove, microwave.
 - Developer Amin Group also owns the John Deere Commons Radisson, which will be getting a facelift.
- There is also the opportunity to build a small retail strip adjacent to the existing Community Health Center. Locating a pharmacy here would complement the Center, which has none.

Planned Development

- East Moline – Fountainhead
 - 158 acre planned mixed-use residential and retail development along the river.
 - Ultimately over 344 units of residential being considered.
 - As of late August 2015, were still securing at least some of the financing.
 - Phase 1 to consist of 2 connect Hyatt hotels (Hyatt House and Hyatt Place) and an 88-unit apartment building.
 - Hyatt's contract says this needs to be complete by Fall 2016. Unknown if this applies to the residential
- Bettendorf
 - 42-unit townhouse development near getting permits on 53rd Avenue
 - 2 small banks and a gas station downtown
 - Say no office currently b/c there is a glut of office in the Quad Cities.
- Davenport
 - 241 units that appear to be market-rate residential.
 - Multiple other commercial, institutional and mixed-use buildings.



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